

# Agenda

## Town of Gibsons

### Planning & Development Committee

**September 17, 2019 at 3:00pm**

Council Chambers  
Town Hall, 474 South Fletcher Road, Gibsons  
604-886-2274 - [www.gibsons.ca](http://www.gibsons.ca)

***Council***



Mayor Bill Beamish  
Councillor David Croal  
Councillor Annemarie De Andrade  
Councillor Aleria Ladwig  
Councillor Stafford Lumley



Planning & Development  
Committee  
**AGENDA OF**  
September 17, 2019  
Council Chambers, 3:00pm  
474 South Fletcher Road, Gibsons, BC

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1. **CALL TO ORDER**

2. **APPROVAL OF THE AGENDA**

3. **DELEGATIONS**

- 3.1 **Ruth Simons - Howe Sound Biosphere Region Initiative Society** Page(s)  
*Regarding requesting endorsement of nomination to UNESCO Biosphere Reserve.* 5 - 12

4. **REPORTS**

- 4.1 **Director of Planning - Application for Gospel Rock Village Form and Character Development Permit (DPA No. 10) – DP-2018-24 (Apartments), DP-2018-25 (Townhouses), DP-2018-26 (Hotel) and Parking Variance DVP-2019-09** Page(s)  
13 - 130

RECOMMENDATION(S)

THAT the report titled Application for Gospel Rock Village Form and Character Development Permit (DPA No. 10) – DP-2018-24 (Apartments), DP-2018-25 (Townhouses), DP-2018-26 (Hotel) and Parking Variance DVP-2019-09 be received;

AND THAT the Planning and Development Committee requests Greenlane to:

- a. retain natural vegetation (such as Arbutus and Douglas Fir trees) adjacent to park and public spaces through use of restrictive covenants where feasible;

AND THAT the Planning and Development Committee supports the issuance of Gospel Rock Village Form and Character Development Permit (DPA No. 10) –DP-2018-24 (Apartments), DP-2018-25 (Townhouses), DP-2018-26 (Hotel);



AND FURTHER THAT the Planning and Development Committee supports the parking variance requested to reduce the 257 on-site vehicle parking space requirement to 232 on-site vehicle parking spaces, with 4 car share vehicles and parking spaces, and two 15-min limit parking stalls on the plaza, subject to:

- a. Greenlane providing four dedicated on-site car share parking spaces and four vehicles for the car share parking spaces;
- b. A letter of credit for \$50,000 to be provided to the Town in respect of the provision of each shared vehicle, with the letter of credit:
  - i. reduced to \$20,000 after provision of the following:
    - i. a copy of an executed legal agreement between the property owner and a car-sharing service providing for the operation of the shared vehicle at the shared vehicle parking space;
    - ii. confirmation in writing from the car-sharing service that funds sufficient to acquire the shared vehicle are in hand or that the acquisition has been otherwise completed; and
    - iii. a shared vehicle parking space is clearly accessible to members of the car-sharing service at all times, is individually and legibly marked for exclusive use of a shared vehicle;
  - ii. returned in full upon written confirmation by the car-sharing service that it has operated a shared vehicle at the shared vehicle parking space for a minimum of six years after an occupancy permit has been issued for the Building in respect of which the shared vehicle was provided.

4.2 **Director of Planning - Development Permit for Form and Character (DPA 4) for 749 School Road**

Page(s)  
131 -  
189

**RECOMMENDATION(S)**

THAT the report titled Development Permit for Form and Character (DPA 4) for 749 School Road be received;

AND THAT the Planning and Development Committee recommends issuance of DP-2019-12 for 749 School Road, subject to the adoption of amendment bylaws 985-23, 2019 and 1065-49, 2019.

**5. INQUIRIES**

**6. NEXT MEETING**

- 6.1 *The next Regular Planning & Development Committee meeting to be held on Tuesday, October 1, 2019 in the Town Hall Council Chambers at 3:00pm.*

**7. ADJOURNMENT**



August 6, 2019

Town of Gibsons Municipality

Dear Mayor Beamish and Council:

Re: Endorsement of the nomination for Atl'ka7tsem/Howe Sound Biosphere Reserve  
Nomination to UNESCO for Biosphere Reserve designation

We are excited to be nearing completion of our nomination document for the Alt'ka7tsem/Howe Sound Biosphere Reserve and expect to submit to the Canadian Commission for UNESCO for their review and support by the end of September. It has been a two-year process to complete the document which will be approximately 230 pages and we are on the home stretch.

We have relied to date on the strong support from all our local governments for the Biosphere nomination. We are now asking representatives of each local government to endorse the nomination. The endorsement does not legally or financially bind the Municipality but is an indication that the Town of Gibsons Municipality supports the objectives of the Alt'ka7tsem /Howe Sound Biosphere Reserve.

Quotation from Mayor Josie Osborne, Tofino – UNESCO Clayoquot Sound Biosphere Reserve  
*"Being declared an UNESCO Biosphere Reserve brings a greater sense of responsibility to living within our means in this world."*

In February 2017, Town of Gibsons Council passed a resolution of support in principle of the Atl'ka7tsem/Howe Sound Biosphere:

WHEREAS, the Town of Gibsons is a signatory to the Howe Sound Community Forum's Principles for Cooperation;  
THEREFORE, conditional on the support of the Squamish First Nation, the Town of Gibsons support in principle the ongoing initiative to nominate Howe Sound as a UNESCO Biosphere Region.  
Carried

In July 2019 the Sunshine Coast Regional District endorsed the nomination.

We are now asking the Mayor or representative to be a signatory from Town of Gibsons Municipality for section 5 of the document:

5.4 Signed by the authority/authorities, elected local government recognized authority or spokesperson representative of the communities located in the transition area(s).

Full name and title: \_\_\_\_\_

Date: \_\_\_\_\_

Address, email, phone number: \_\_\_\_\_

A support letter in addition to the signature of endorsement is also welcome. We will need a signature on our final document before the end of September.

We are always seeking opportunities to inform more people about the intentions and aspirations of the designation and welcome any opportunities to further inform Council members and residents.

Thank you and I look forward to your questions and response.

Sincerely,

Ruth Simons,  
Lead,  
Howe Sound Biosphere Region Initiative Society  
[www.howesoundbri.org](http://www.howesoundbri.org)  
c/o Box 465  
Lions Bay, B.C.  
V0N 2E0 604 921-6564

## Sunshine Coast Regional District

1975 Field Road  
Sechelt, British Columbia  
Canada V0N 3A1

P 604.885.6800  
F 604.885.7909  
Toll free 1.800.687.5753

info@scrd.ca  
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July 29, 2019

Canadian Commission for UNESCO  
Howe Sound Biosphere Region Initiative  
c/o Ruth Simons, Lead  
Box 465  
Lions Bay, BC V0N 2E0

Dear Ms. Simons:

**RE: Endorsement of the Nomination for an Atl'ka7tsem/Howe Sound Biosphere Reserve**

The Sunshine Coast Regional District (SCRD) is pleased and excited to support the nomination of Atl'ka7tsem/Howe Sound for UNESCO Biosphere Reserve designation.

The SCRD recognizes the value of the landscapes and healthy recovery of the marine life in Howe Sound, particularly the uniqueness of the Howe Sound glass sponge reefs. The opportunity to showcase, protect and cooperatively manage the unique natural and cultural assets is of immense interest to the Regional District.

We are supportive of the objectives of the UNESCO Man and the Biosphere program and the opportunity for the Howe Sound Biosphere Initiative to be an active member of an international network seeking to find solutions to significant environmental issues. Fostering new or strengthened relationships with other governments, Skwxwú7mesh Nation and the community are aligned with SCRD's strategic goals. The possibilities extending from these relationships include, to name but a few, growing the economy, enabling volunteerism, and advancing reconciliation. Now more than ever, these are critical needs for achieving ways for people and nature to thrive together.

A robust understanding of values in Atl'ka7tsem/Howe Sound, as developed through an ecosystem-based approach to management and conservation, would add value to SCRD's land use planning processes, such as through the development of our Official Community Plans or growth strategies, while enhancing regional opportunities for coordinated approaches to land use management. Such integration could support sustainable development and action on climate change while inspiring residents, visitors and all Canadians to live sustainably.

SCRD optimistically looks forward to the establishment of the Atl'ka7tsem/Howe Sound UNESCO Biosphere Reserve.

Yours truly,

**SUNSHINE COAST REGIONAL DISTRICT**

Lori Pratt  
SCRD Board Chair

**ELECTORAL AREAS:** A - Egmont, Pender Harbour B - Halfmoon Bay D - Roberts Creek E - Elphinstone F - West Howe Sound  
**MUNICIPALITIES:** District of Sechelt / Sechelt Indian Government District / Town of Gibsons





## Initiative for UNESCO Biosphere Reserve Designation for Atl'ka7tsem/Howe Sound – September, 2019

UNESCO Director-General, Audrey Azoulay said, *“There is a pressing need to take action for biodiversity, for our shared environmental heritage. After diagnosing the issue at stake, highlighted by the recent report of the Intergovernmental Science-Policy Platform on Biodiversity and Ecosystem Services (IPBES), the vitality of the World Network of Biosphere Reserves gives us cause for hope. Each UNESCO biosphere reserve is an open sky laboratory for sustainable development, for concrete and lasting solutions, for innovation and good practices. They seal a new alliance between the world of science and youth, between humans and the environment.”*

UNESCO Biosphere reserves seek to reconcile human activity with the conservation of biodiversity through the sustainable use of natural resources. This reflects UNESCO's key objective of fostering innovative sustainable development practices and combatting the loss of biodiversity by accompanying communities and Member States in their work to understand, appreciate and safeguard the living environment of our planet.

Main objectives of UNESCO Man and the Biosphere Program:

1. Biodiversity Conservation
2. Sustainable Development
3. Reconciliation
4. Logistic Support

Main objectives for Atl'ka7tsem/Howe Sound Biosphere Region Management Plan:

1. For there to be an overarching holistic land and marine use plan for the Atl'ka7tsem/Howe Sound region that is mutually recognized and respected by First Nations, civil society, stakeholders and all levels of government.
2. Biodiversity Conservation within the region is understood and preserved for future generations
3. People, culture and humanity thrive in harmony with nature.

Our **Vision:** To see all communities within the Atl'ka7tsem/Howe Sound Biosphere Region (AHSBR) living sustainably within a healthy ecosystem that has a diversified economy and vibrant cultures as well as ongoing advantages associated with UNESCO designation.

**Our Mission:** To work collaboratively toward the Vision by providing logistic support for research, education and training that not only promotes biodiversity conservation within both water-based and land-based environments but also promotes sustainable development of the economy with high priority on social and cultural values as well as environmental values.

**Objectives:** HSBRS is working on three key objectives in its efforts to see the region become an exemplary model of ecosystem-based management that sensitively strikes a balance between its environmental priorities and its social, cultural and economic priorities:

1. To advance **biodiversity conservation** in the region's marine and terrestrial ecosystems.
2. To advance **sustainable development** in line with the UNESCO's Man and the Biosphere Program priorities and the UN's Sustainable Development Goals.
3. To advance **Reconciliation** in recognition of UNDRIP, the UN Declaration on the Rights of Indigenous Peoples that enshrines the rights of the world's indigenous peoples in terms of their survival, dignity and well-being.

**Proposed governance structure:**

Atl'ka7tsem/Howe Sound Biosphere Region Initiative Society will be a registered charitable organization committed to creating and operating in a culture of "Ethical Space" as defined by the Canadian Biosphere Reserves Association (CBRA):

*"It is the natural confluence of Indigenous and non-Indigenous worldviews and cultures. It is created through relationship building and shared governance. At a societal level, it is a vision of a future where these worldviews come together, and where both are valued equally."*

**Board of Directors:** 8-10 member Board is accountable to UNESCO Man and the Biosphere Program and Canadian Biosphere Reserve Network and for strategic planning and direction of AHSBR. The board will be Co-Chaired by one being elected by members, the other reserved for a Squamish Nation member. Board members have a deep understanding and commitment to the MAB program and UNESCO. Members represent First Nations, academia, industry, academia, science, NGO's, civil society, business. Board members are spokespersons for the Society, contributing time and resources.

**Roundtable Group:** 10-15 participants from the Region representing local government, Squamish Nation, Provincial government, Fisheries and Oceans Canada, industries, Social services, Science, Conservation organizations, Tourism association. Meeting a minimum of twice a year, the Roundtable group act as advisors and **build consensus in the region towards the objectives and will be guided by the Terms of Reference.**

**Staff and Volunteers:** The Executive Director (ED) reports to the Board of Directors. The ED holds the strategic and operational responsibilities required to achieve ASHBR's mission. Additional staff will be hired as projects and funding allows. Volunteers will be a critical part of the AHSRBR's day-to-day functioning and overall success.



**Proposed Funding:**

Long term sustainable funding for staffing and overheads will be derived from a Howe Sound Biosphere Region Endowment Fund. Other funding will be sources from consulting fees, crowd source funding for specific projects and grants from various sources.

**1. What does the Biosphere organization do?****A strategic Communications and Engagement Hub:****Communication:**

- serve as a clearinghouse for information and knowledge including information gathering, research database maintenance and widespread dissemination through the Biosphere Region website and social media pages in order to actively promote its use for informed decision-making;
- create and make public an annual report on Biosphere Region performance including progress on all strategic goals;
- Advance education by disseminating conservation information and research results from the region to science organizations and educational institutions, including work on regional climate change issues;
- Support and increase understanding of the *Sḵwx̱wú7mesh snichim* language, cultural and history; and
- Build awareness about the UNESCO World Network of Biosphere Regions, the UNESCO Man and the Biosphere Program, the UN's Sustainable Development Goals and the UN Declaration on the Rights of Indigenous Peoples (UNDRIP).

**Engagement**

- Participate in an advisory capacity on working groups that include non-profits and registered charities with a shared purpose of advancing conservation in the Atł'ka7tsem/Howe Sound Biosphere Region, enhancing co-ordination and efficiency wherever possible;
- Plan, coordinate and host public events and multi-stakeholder forums with *Sḵwx̱wú7mesh* and other First Nation groups to address matters related to Reconciliation
- Plan, coordinate and host public events and multi-stakeholder forums to address issues and advance strategic objectives within the region.
- Facilitate involvement of volunteers in research projects, event organization and educational materials development; and
- Develop institutional capacity within the organization and the Biosphere Region to meet its goals and objectives, including workshops and networking events related to the UN's

Sustainable Development Goals, highlighting the value of an ecosystem-based approach to management that balances ecological, economic and social objectives leading to enhanced sustainable development.

**Current Projects of Howe Sound Biosphere Region Initiative Society:**

- Post winter storm clean up and best practices for adapting to Climate Change along shorelines
- Ocean Watch: Participate on the Task Force while advancing various strategic action items from Howe Sound Edition. Consulting to Ocean Wise on Phase 2 edition
- Contributed to Squamish Nation Cumulative Effects Monitoring report
- Coordinating, planning and providing logistic support for Howe Sound Community Forums
- UN Sustainable Development Goals workshops – subject to receipt of Federal Grant for four projects aimed at expanding knowledge on Canada's 2030 SDG Agenda.
- Marine Reference Guide – advisory role on Steering committee
- Working with Capilano University, Quest University, SFU and other academics to increase opportunities of potential value in the design of new programs focusing on environmental and social sustainability using Howe Sound as a case study.
- Engaging with BC Parks and community organizations to support and advance conservation and information within existing Howe Sound parks.
- Collaborating with numerous NGO's in the region in support of mutual objectives and projects. i.e. advancing of marine protected areas, monitoring of biodiversity, Indigenous Youth Guardian project.
- Advancing understanding of Howe Sound Cumulative Effects Assessment reports and supporting implementation into decision making.
- Nomination to UNESCO for Biosphere Reserve status.



# STAFF REPORT

**TO:** Planning & Development Committee      **MEETING DATE:** September 17, 2019

**FROM:** Lesley-Anne Staats, Director of Planning      **FILE NO:** 3220-Chaster-Block 7  
Odete Pinho, Agora Consulting

**SUBJECT:** Application for Gospel Rock Village Form and Character Development Permit (DPA No. 10) – DP-2018-24 (Apartments), DP-2018-25 (Townhouses), DP-2018-26 (Hotel) and Parking Variance DVP-2019-09

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## RECOMMENDATIONS

1. THAT the report titled Application for Gospel Rock Village Form and Character Development Permit (DPA No. 10) – DP-2018-24 (Apartments), DP-2018-25 (Townhouses), DP-2018-26 (Hotel) and Parking Variance DVP-2019-09 be received;
2. AND THAT the Planning and Development Committee requests Greenlane to:
  - a. retain natural vegetation (such as Arbutus and Douglas Fir trees) adjacent to park and public spaces through use of restrictive covenants where feasible;
3. AND THAT the Planning and Development Committee supports the issuance of Gospel Rock Village Form and Character Development Permit (DPA No. 10) – DP-2018-24 (Apartments), DP-2018-25 (Townhouses), DP-2018-26 (Hotel);
4. AND FURTHER THAT the Planning and Development Committee supports the parking variance requested to reduce the 257 on-site vehicle parking space requirement to 232 on-site vehicle parking spaces, with 4 car share vehicles and parking spaces, and two 15-min limit parking stalls on the plaza, subject to:
  - a. Greenlane providing four dedicated on-site car share parking spaces and four vehicles for the car share parking spaces;
  - b. A letter of credit for \$50,000 to be provided to the Town in respect of the provision of each shared vehicle, with the letter of credit:
    - i. reduced to \$20,000 after provision of the following:
      1. a copy of an executed legal agreement between the property owner and a car-sharing service providing for the operation of the shared vehicle at the shared vehicle parking space;

2. **confirmation in writing from the car-sharing service that funds sufficient to acquire the shared vehicle are in hand or that the acquisition has been otherwise completed; and**
  3. **a shared vehicle parking space is clearly accessible to members of the car-sharing service at all times, is individually and legibly marked for exclusive use of a shared vehicle;**
- ii. **returned in full upon written confirmation by the car-sharing service that it has operated a shared vehicle at the shared vehicle parking space for a minimum of six years after an occupancy permit has been issued for the Building in respect of which the shared vehicle was provided;**

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#### **BACKGROUND / PURPOSE**

The Gospel Rock Village development proposal on Block 7, Chaster Road was initiated in May 2017 by Modus Planning, Design and Engagement, on behalf of Greenlane Homes to request the development of a Gospel Rock Village comprising of a mix of commercial and 360 residential units in the form of apartments, townhouses, and single-family dwellings on an approximately 47-acre parcel. The proposal also includes a park dedication and protected conservation area on 23 acres with a 'green lane' created to tie the neighborhood and park together.

As a condition of adopting the zoning amendment (ZA-2017-03), the Town developed Form and Character Development Permit Area guidelines for multi-family residential and commercial uses, per Official Community Plan (OCP) Policy 3.4.11 in the Gospel Rock Neighbourhood Plan. The Form and Character Development Permit Area No. 10 (DPA No. 10) guidelines were adopted on July 24, 2018. On November 26, 2018, the applicant, Greenlane, applied for a Development Permit under the new DPA No. 10 designation. Architectural and landscape designs have been updated since application submission in November 2018. The current DPA 10 designs are dated August 19, 2019.

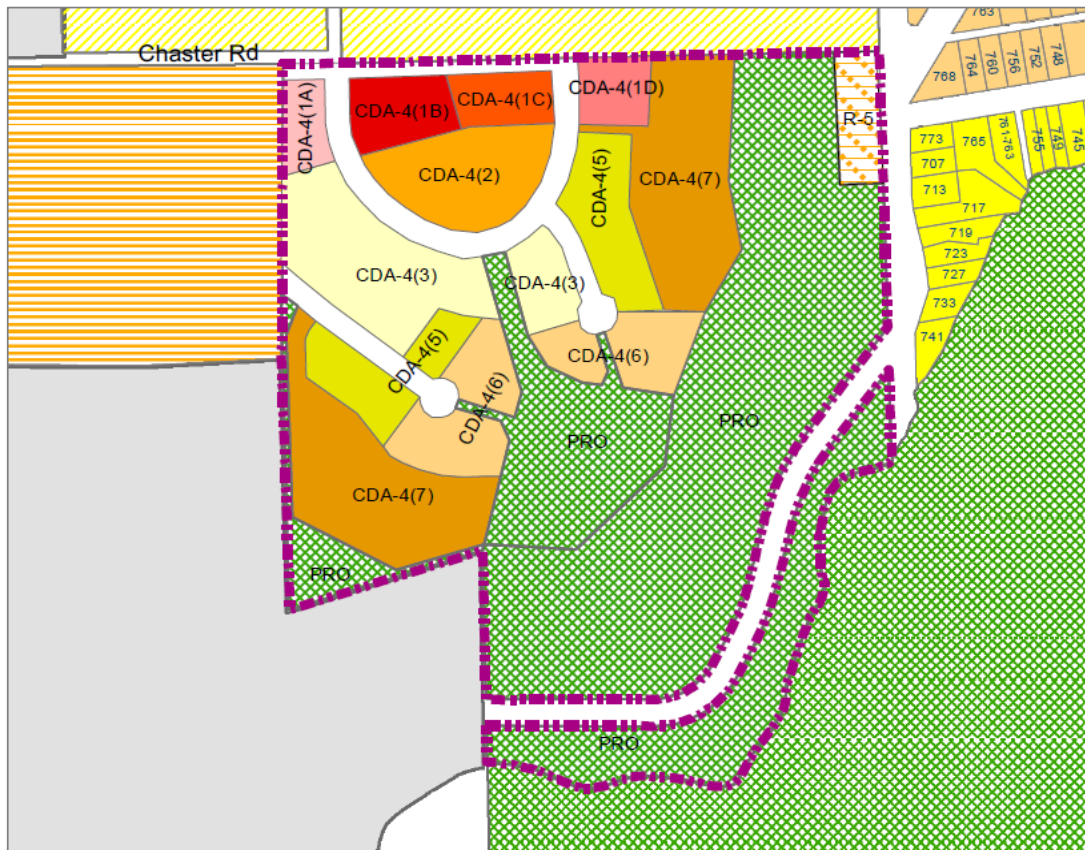


Figure 1: Block 7 site with various subzones within the Comprehensive Development Area Zone 4 (CDA-4)

This report provides the Town's Planning and Development Committee with an analysis of the development using the DPA No.10 guidelines. The proposed building and landscape design submission for Gospel Rock Village has been submitted by the applicant's architect, JYW Architecture Inc and landscape architect Perry and Associates (Attachment A). DPA No. 10 provides building and landscape details on how the site will ultimately be constructed in a manner that is consistent with the objectives of the OCP guidelines. Once the Development Permit is granted, future construction on the site would be required to substantially conform with the designs and conditions contained in the development permit.

This report summarizes the application under DPA No. 10 for Gospel Rock Village form and character guidelines exclusively. Other required development permits (DPA 1 Geotechnical Hazards, DPA 2 Environmentally Sensitive Areas, DPA 9 Aquifer) and pre-development review processes will be addressed by staff or in future reports to Council, as needed. In addition, this report summarizes proposed on-site parking and a requested parking variance.

The purpose of this report is to provide an overview of the application and obtain a recommendation(s) from the Planning and Development Committee for Council's consideration on a decision.

## **SUMMARY**

This report summarizes the proposed DPA No 10 for Gospel Rock Village form and character. This Committee's consideration is requested in reviewing if the proposed architecture and landscape plans are in alignment with the OCP's DPA No. 10 form and character guidelines for Gospel Rock Village. The intent of the guidelines is to "ensure high quality, pedestrian-oriented development within the Gospel Rock Neighborhood and enhance the appearance of development for the benefit of visitors, residents and businesses." The proposed Gospel Rock Village development under review includes a 50-room hotel, 49-unit apartment, 48-unit townhouse and 60 lot subdivision for future single-family residential homes.

## **DISCUSSION**

### ***Referrals***

This application was referred to the Gibsons and District Volunteer Fire Department and the Town's Infrastructure Services department and Building Official. No concerns were raised during the referrals. During the Building Permit review process, referrals will be provided again with detailed building plans for further opportunities to comment.

### ***Geotechnical Review – Form and Character Implications***

Kontur Geotechnical Consultants prepared a preliminary geotechnical assessment for the proposed Gospel Rock Village development on December 27, 2018. In addition, Kontour prepared a Geotechnical Exploration report, dated January 31, 2019. The preliminary assessment finds there is a moderate to high risk of rock falls, topples and slides in the study area. 12 proposed residential lots will require a combination of restrictive covenants, to maintain minimum distances from top (crest) or bottom (toe) of any steep slope or bluff. In addition, these lots would also require interventions such as retaining walls and slope stabilizing mitigation options. Depending on site specific geotechnical assessment, the 12 lots may be restricted in buildable area and trigger a requirement for council consideration of development variance permits (for building siting, height, protective structures). This is further discussed below in, Zoning Bylaw Conformance.

The restrictive covenants will be registered on the Titles of the affected properties, as required for the subdivision approval process.

### ***Environmental Assessment Review – Form and Character Implications***

Diamond Head Consultants prepared an environmental assessment for the Gospel Rock Village development, dated April 29, 2019. Diamond Head prepared a separate environmental assessment for the 6-lot subdivision, dated April 23, 2019. The two environmental assessment reports indicate that there are 8 proposed lots located within environmentally sensitive areas

(ESA) that will be impacted. Lots A1 to A6, located in the proposed 6-lot subdivision on the northeast corner of Block 7, are located in a Douglas fir arbutus dryland forest, with steep slopes along the western half of proposed lots that contain scattered bedrock outcrops. The 6-lot subdivision is recommended to have a 14m setback and 'natural state' no vegetation disturbance covenant off the western end of the lots, with exclusion fencing installed at the 14m setback to protect the ESA from development and invasive species spread. Only the roadside east end of lots should be developed. The six-lot subdivision is also identified in the Kontur preliminary geotechnical assessment report as being at risk of rock falls, hazards at toe of slope, and requiring site specific geotechnical assessment and mitigation measures to reduce risks. At this time, it is not known if environmental requirements for natural state /no disturbance covenant and geotechnical slope stabilizing mitigation requirements will be compatible.

Lots 14 and 15 have a natural slope break, which is identified in both the environmental assessment as requiring protection and in the geotechnical report as requiring site-specific assessment and possible mitigation measures. Finally, Lot 3 and some areas in the northwestern edge of the site, which are to be protected under a natural state covenant, can be restored and enhanced (removing invasive species and planting native species) to help compensate for loss of environmentally sensitive areas on the site.

Planning staff have asked the developer to review geotechnical and environmental consultants recommendations and to address the compatibility of protective measures. Planning staff have also asked the developer to prepare an invasive species risk management plan in the park dedication area (49% of property), which will further support conservation, enhancement and compensation for environmentally sensitive areas loss.

Staff note that the identified hazard areas and ESA's are not within the village centre, and would not impact the form and character of the development applications within DPA No. 10.

***Development Permit No. 10 Gospel Rock Village Form and Character Guidelines***

The Planning and Development Committee's role is to review the fit of the project with the DP guidelines outlined in the OCP's DPA No.10 for Gospel Rock Village guidelines and provide a recommendation to Council on a decision. The intent of the guidelines is to "ensure high quality, pedestrian-oriented development within the Gospel Rock Neighborhood and enhance the appearance of development for the benefit of visitors, residents and businesses."

Attachment A includes the Gospel Rock Village Design and Landscape package, and a written summary on the design rationale prepared by JYW Architecture and Perry + Associates. Tables 1-9 below highlights how the proposed design addresses the applicable form and character guidelines.



**Figure 1:** Overview image of Gospel Rock development site (rendering provided by Greenlane), showing development context with 'village plaza' hotel, apartment, townhouses and commercial uses, within Crescent Road. Surrounding the village centre are future single-family development properties (60 in total). 49% of the site is to be retained in a natural state (as parkland).





**Figure 2:** Gospel Rock Site Plan – Overview of Proposed Development Plan

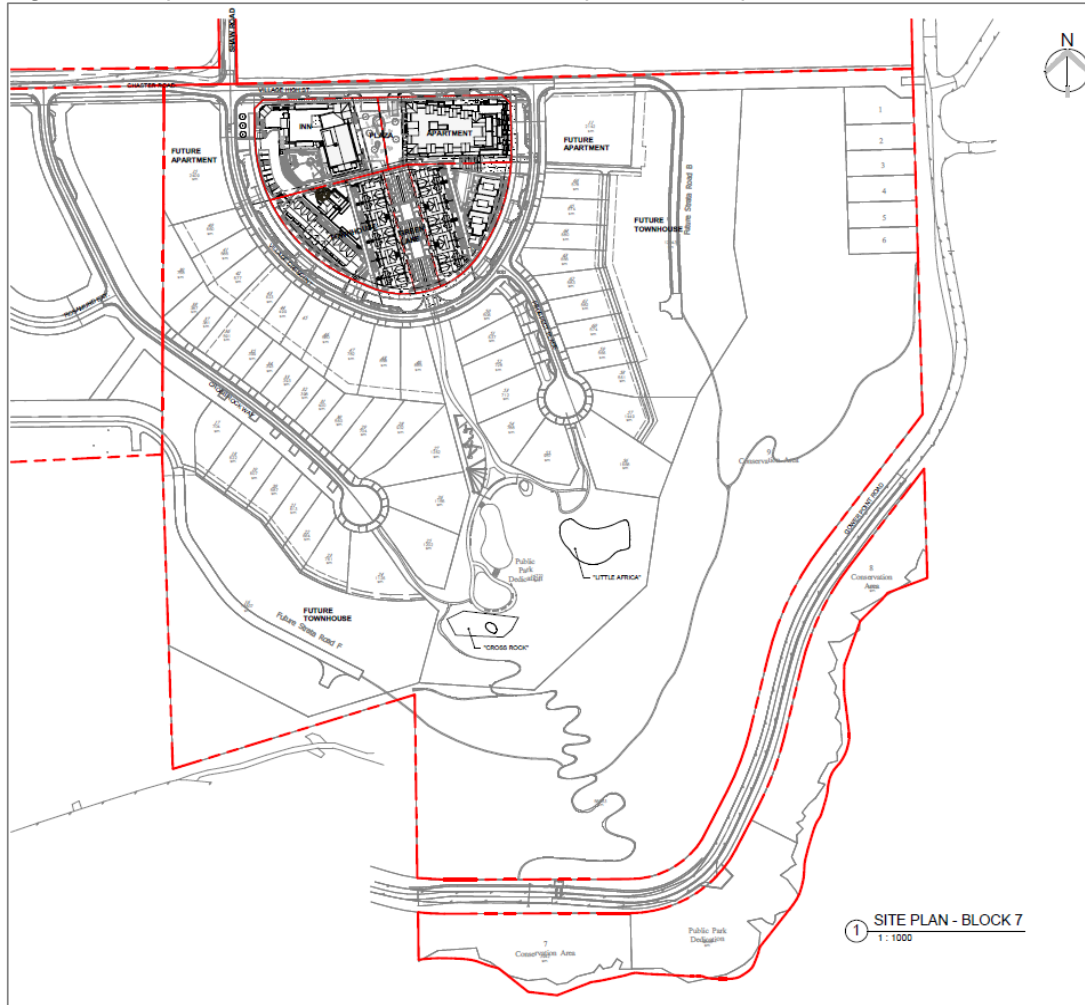


Table 1: General Form and Character

DPA No. 10 Guidelines (Gospel Rock Village)	Staff Comments	Addresses Criteria (Y/N)
<b>General Form and Character of Development</b> <i>To create a high quality, visually appealing development with a safe, comfortable pedestrian environment, the following guidelines shall apply:</i>		
Where feasible, minimizing site grading and retaining natural vegetation is encouraged.	Finished grades do not exceed maximum 10% slope permitted by Town bylaws. To meet this requirement, there will be substantial areas of cut and fill. The proposed cut/fill aims to reduce materials moved off site, however the village area will not retain any natural vegetation due to extent of re-grading proposed and to provide underground parking. Staff note that the majority of retaining natural vegetation will occur outside of the village centre, in the park and conservation covenant area.	Y/N
Significant landscaping adjacent to public roadways and integrated within the site, use of both architectural and landscape features to provide a “gateway” or distinct entrance.	Plaza entrance to site is a distinct entrance to development and transition to greenway/ parks area. Pedestrian gateways and public art are proposed at entrances to townhouses. Public roads will have landscape plantings and trees.	Y
The form and character should support and enhance the Gibsons small-town character, including a mix of building masses and open spaces reflective of more incremental development.	Transition from existing Elphinstone rural area to proposed development will be abrupt. From rural residential to urban resort destination. Includes 4-5 storey hotel/commercial with internal reduction in building density and height with townhouses and single family residential. Buildings have West Coast design.	N
Provide a clear distinction between private and public realms through subtle cues in materials, pathway edges, grade and/or landscape design elements.	Changes in patio paving and landscape structures mark transitions from townhouse to public. Feature stone walls, planter seating and landscape elements throughout create transitions between uses.	Y

DPA No. 10 Guidelines (Gospel Rock Village)	Staff Comments	Addresses Criteria (Y/N)
<b>General Form and Character of Development</b> <i>To create a high quality, visually appealing development with a safe, comfortable pedestrian environment, the following guidelines shall apply:</i>		
Each dwelling unit in a residential or mixed-use project shall incorporate direct access to a usable private outdoor space such as a patio, balcony, and/or upper-level terrace.	All apartment, townhouse, hotel rooms have usable outdoor spaces including: patios, balcony or terrace spaces.	Y
West Coast seaside design features should be incorporated in the design, including:		
<ul style="list-style-type: none"> <li>Emphasis on natural light through the significant glazing and orientation of buildings to views and/or other significant natural features.</li> </ul>	All spaces have generous windows and natural light.	Y
<ul style="list-style-type: none"> <li>Consideration of indoor/outdoor relationships and flexibility within open-plan layouts.</li> </ul>	Each unit includes private outdoor space (patio, balcony, terrace).	Y
<ul style="list-style-type: none"> <li>The use of natural building materials and exposed timber (structural) features.</li> </ul>	Primary exterior material is fibre cement hardi siding (natural tones) with brick, stone accents at base. Timber wood glulam posts (cedar tone) are features for each building.	Y
<ul style="list-style-type: none"> <li>Use natural colours with a variety of complementary colours used as accents to promote visual interest.</li> </ul>	Exterior materials use natural colours in grey and brown palette. Diverse application of accent colours	Y
Low Impact Development techniques for stormwater management is encouraged – including swales or other landscape features that alleviate impacts of storm runoff from impervious surfaces, including roofs and surface parking areas; green roofs and permeable parking areas are encouraged	Greenlane includes a planted rain garden to collect water from the adjacent landscape and hard surfacing. The design intends to collect sediment and mitigate peak flows of rainwater. Stormwater from roof leaders and hardscape is designed to be directed to onsite stormwater features for treatment prior to discharge to the municipal systems. Further detailed design will be provided at building permit stage.	Y

DPA No. 10 Guidelines (Gospel Rock Village)	Staff Comments	Addresses Criteria (Y/N)
<b>General Form and Character of Development</b> <i>To create a high quality, visually appealing development with a safe, comfortable pedestrian environment, the following guidelines shall apply:</i>		
Design lighting to minimize light spill, glare and sky glow by using non-glare full cut-off fixtures.	Schematic lighting plan includes post lights in plaza, bollard lights along greenlane, wall/step lights at stairs and entrances to townhouses. Specifications at Building permit stage.	Y
Recycling facilities are required for all commercial, multi-family residential, and mixed-use buildings	Garbage and recycling space is allocated in parking level P2 for hotel/ commercial, P2 for apartment building and at two locations for townhouses.	Y
To the extent possible, locally sourced materials should be used to reduce transportation impacts and reflect the local climate, light, history, and culture. Additionally, the following materials are encouraged:	Exterior materials include metal roof, fiber cement siding, stone, brick and wood glulam posts, which are durable and age well in the coastal climate. JYWA responded "This will be incorporated into the detailed building design"	?
<ul style="list-style-type: none"> <li>Recycled materials or materials with a high-recycled content.</li> </ul>	Detailed at building permit stage.	?
<ul style="list-style-type: none"> <li>Concrete with at least 25% fly ash or slag.</li> </ul>	Detailed at building permit stage	?
<ul style="list-style-type: none"> <li>Wood products certified CSA Sustainable Forest Management Standard or equivalent.</li> </ul>	Detailed at building permit stage	?
<ul style="list-style-type: none"> <li>Interior finishes and installation methods with low toxic emissions.</li> </ul>	Detailed at building permit stage	?
To the extent possible, buildings should be designed to reduce energy usage by incorporating standards such as solar ready and LEED.	DP application states that project will "be designed to meet the new Building Code BCBC 2018" and Step Code requirements. An energy modeler will be retained to provide options for confirmation that the buildings will be adequately energy efficient.	Y

DPA No. 10 Guidelines (Gospel Rock Village)	Staff Comments	Addresses Criteria (Y/N)
<b>General Form and Character of Development</b> <i>To create a high quality, visually appealing development with a safe, comfortable pedestrian environment, the following guidelines shall apply:</i>		
Opportunities for the inclusion of public art should be explored in public space, especially plazas and other public open space to enhance the overall open space network.	Landscape plan shows 5 locations for “Potential Art Location” including at park greenway, apartment east entrance, west entrance to hotel, in plaza, corner of Village Crescent and Village High St. Applicant explanation states “art pieces and placement should respect the land, local geography and natural history of the space. Inclusion of indigenous artists and stakeholders should be explored.”	Y
<ul style="list-style-type: none"> <li>Opportunities to display Skwxwú7mesh art, language and culture in public space are especially encouraged</li> </ul>	Town staff wish to be involved in public art selection process for public spaces. Applicant states “the developer is open to partnership with the Town of Gibsons and First Nations”	?

**Figure 2:** Image of Village Plaza, showing pedestrian oriented entrance. Plaza and townhouses shown centre of image and hotel with patio restaurant shown on right.



Table 2: Building Form, Scale, and Massing

DPA No. 10 Guidelines (Gospel Rock Village)	Staff Comments	Addresses Criteria (Y/N)
<b>Building Form, Scale, and Massing</b> <i>To encourage varied building forms and to avoid creation of a commercial strip image, the following guidelines respecting massing and scale shall apply:</i>		
Varied building forms will be encouraged.	Building forms are varied. They use optical articulation with use of decks/ balconies.	Y
Large areas of blank wall are not acceptable on a face with a pedestrian or residential area orientation.	No buildings have large areas of blank walls.	Y
Long, unbroken building lines and rooflines should be avoided.	No buildings have long unbroken building lines and rooflines.	Y
Wall lines should be off-set and modulated to create visual interest.	Wall lines are modulated and articulated with use of decks/ patios.	Y
Pitching, stepping down, and/or varying rooflines should be incorporated to vary height in the roofscapes of buildings.	Varying rooflines are incorporated in design.	Y
On sloped sites, building forms should step down with the natural grade of the site to avoid imposing masses on the downhill slopes.	Building form is stepped down, with taller buildings at top of hill and reducing in height down the slopes.	Y
Where commercial development incorporates a residential or multi-unit use on upper floors, a graded transition in the building height is desired to ensure adjacent properties are not faced with a massive wall.	The hotel/inn ground floor restaurant, retail, and spa transition with hotel rooms above. There are no blank walls	Y
Longer frontages of townhomes will articulate "clusters" of not more than three homes in order to avoid repetitive forms.	Townhomes are designed with clusters of up to 11 homes, different colour variations give appearance of clusters of 3.	Y
Building form and character will create visual interest through the subtle articulation of massing, roof forms and changes in materials and colours.	Exterior building materials are proposed to alternate darker/ lighter colours every three homes, to break repetitive forms and add to articulation.	Y



DPA No. 10 Guidelines (Gospel Rock Village)	Staff Comments	Addresses Criteria (Y/N)
<b>Building Form, Scale, and Massing</b> <i>To encourage varied building forms and to avoid creation of a commercial strip image, the following guidelines respecting massing and scale shall apply:</i>		
Street-facing units and units fronting a public space / green street should utilize a layering of elements – including but not limited to street-facing stairs, stoops, porches, patios, and landscaping to transition between private-use and the public realm:	Street facing town units (apartment and townhomes) fronting public space do include stairs, patios, and landscape transitions.	Y
<ul style="list-style-type: none"> <li>Ground floor units should be elevated between 0.5-1 metre above the street. If the ground floor is not elevated, other means of separation and/or buffering for privacy should be utilized.</li> </ul>	Ground floor units are grade separated from street.	Y
<ul style="list-style-type: none"> <li>Gates, railings, walls and landscaping providing a buffer from the street and creating a clear distinction between the public and private realm shall be provided.</li> </ul>	In all units, gates, railings and landscape plantings create distinction between public and private realm, except ground level east and west apartment units. Buffer planting is incorporated.	Y
<ul style="list-style-type: none"> <li>Landscape buffer elements should be 0.9-1.2 metres tall.</li> </ul>	Consistent with landscape plan.	Y

**Figure 3:** Image of hotel, commercial area and townhouses.



Table 3: Siting of Building and Structures

DPA No. 10 Guidelines (Gospel Rock Village)	Staff Comments	Addresses Criteria (Y/N)
<b>Siting of Building and Structures</b> <i>To maintain a strong street presence while also framing the interior open space (Village Green and Greenlane), the following guidelines respecting siting of buildings shall apply:</i>		
Buildings should be sited with the entrance to the buildings facing the public street and the internal open space (Village Green and/or Greenlane) to activate public spaces and signal access and priority for pedestrians.	Hotel, apartment building and townhomes have entrances facing public pedestrian street.	Y
Buildings situated along the Village portion of the Greenlane are intended to represent a higher degree of formal design to help frame the Village Greenlane and signal a transition from the Lookout Park to the Village.	Building and landscape design frame the Village Greenlane. Rainwater trench in centre of Greenlane creates a landscape transition toward the lookout park.	Y
Natural landscapes, including significant trees, should be incorporated into site development when feasible.	Village grading will not retain natural landscapes and trees. Remainder of site (49%) will be kept in natural state.	Y/N

Table 4: Residential / Commercial Buffers

DPA No. 10 Guidelines (Gospel Rock Village)	Staff Comments	Addresses Criteria (Y/N)
<b>Residential / Commercial Buffers</b> <i>Effective transitions between commercial areas and adjacent residential properties can be achieved by:</i>		
<ul style="list-style-type: none"> <li>Fencing, combined with a broad area of landscape plantings (tree, shrubs).</li> </ul>	Fencing and extensive landscape plantings in plan.	Y
<ul style="list-style-type: none"> <li>Dense shrubbery or hedges capable of impeding travel through to adjacent properties, and which is not visually transparent to adjacent properties</li> </ul>	Transitions between public and private realm proposed with extensive landscaping.	Y
<ul style="list-style-type: none"> <li>Creating a transition zone to complement the scale of the development, not less than 3.0 metres in width.</li> </ul>	A 2m pathway with additional double row tree planting creates transition from hotel and apartment to townhouses.	Y



DPA No. 10 Guidelines (Gospel Rock Village)	Staff Comments	Addresses Criteria (Y/N)
<b>Residential / Commercial Buffers</b> <i>Effective transitions between commercial areas and adjacent residential properties can be achieved by:</i>		
<ul style="list-style-type: none"> <li>Minimizing the potential for overlook to neighbouring windows and private spaces.</li> </ul>	Site and building design minimize overlook between neighbours, except residential units fronting plaza and hotel.	Y
<ul style="list-style-type: none"> <li>Careful positioning of lighting, parking, and access points to minimize impacts on adjacent properties.</li> </ul>	Underground parking with two entries at hotel and apartment distributes traffic. Townhouses also have two options for entry/exit. Additional lighting information to be reviewed at detailed design stage.	Y

Table 5: Amenity Space / Private Areas (for multi-unit residential buildings)

DPA No. 10 Guidelines (Gospel Rock Village)	Staff Comments	Addresses Criteria (Y/N)
<b>Amenity Space / Private Areas (for multi-unit residential buildings)</b> <i>The location and size of outdoor spaces such as patios and balconies have a considerable effect on the sense of privacy. New residential buildings should be designed to provide privacy for each resident; through windows, private outdoor spaces or balconies, or through changes in grade or elevation as follows:</i>		
<ul style="list-style-type: none"> <li>An outdoor living space/patio terrace of minimum 2.3 metres depth for townhouses and minimum size of 10 m<sup>2</sup> (110 ft<sup>2</sup>) is recommended.</li> </ul>	Confirmed patios of townhouses. Outdoor living space recommendation met for most units.	Y
<ul style="list-style-type: none"> <li>Where yards are accommodated, a minimum 5 metres depth and a minimum area of 30 m<sup>2</sup> (325 ft<sup>2</sup>) is recommended.</li> </ul>	Townhouses have yard or deck with minimum 5m depth.	Y
<ul style="list-style-type: none"> <li>Apartments should have a minimum 1.5 metres depth terrace or balcony, sufficiently large to create a usable outdoor "room". Balconies should be at least half enclosed in order to give the occupant privacy, security and weather protection.</li> </ul>	Confirmed size of apartment balconies.	Y

DPA No. 10 Guidelines (Gospel Rock Village)	Staff Comments	Addresses Criteria (Y/N)
<b>Amenity Space / Private Areas (for multi-unit residential buildings)</b> <i>The location and size of outdoor spaces such as patios and balconies have a considerable effect on the sense of privacy. New residential buildings should be designed to provide privacy for each resident; through windows, private outdoor spaces or balconies, or through changes in grade or elevation as follows:</i>		
<ul style="list-style-type: none"> <li>Changes in grade can provide for private areas between street edges and the development units.</li> </ul>	Landscape plantings and features screen for privacy of units and street edges.	Y

Table 6: Pedestrian Environment

DPA No. 10 Guidelines (Gospel Rock Village)	Staff Comments	Addresses Criteria (Y/N)
<b>Pedestrian Environment</b> <i>The design of Gospel Rock Village is intended to prioritize the pedestrian within the central open spaces whereby vehicle access is largely accommodated at the outer edge of the Neighbourhood. The following guidelines further define building design responses to support a pedestrian-friendly environment:</i>		
Buildings shall emphasize a human-scale and pedestrian orientation within the Neighbourhood by presenting 1-2-storey façade elements to engage pedestrian edges. Single storey elements are preferred. This may be achieved by:	Hotel and apartment building are 4 storey tall. The larger corner building mass, use of patios/decks and darker building materials at base of buildings, create appearance that above 1 <sup>st</sup> floor the building façade is setback.	Y
<ul style="list-style-type: none"> <li>well-marked building entrances</li> </ul>	There are visually clear entrances	Y
<ul style="list-style-type: none"> <li>architectural features</li> </ul>	Corners are heavier massing	Y
<ul style="list-style-type: none"> <li>an emphasis on the fenestration (the arrangement and positioning of windows)</li> </ul>	Large window, taller ceilings at ground floor	Y
<ul style="list-style-type: none"> <li>weather protection through the use of awnings, arcades and canopies along commercial storefronts and building entries. Weather protection must be integral to the building form, and the materials and colors used should be consistent with the façade</li> </ul>	Extensive use of awnings for deck coverage and architectural features. Entrance to hotel with extended canopy. Entrance to apartment and ground level units include weather protection.	Y

DPA No. 10 Guidelines (Gospel Rock Village)	Staff Comments	Addresses Criteria (Y/N)
<b>Pedestrian Environment</b> <i>The design of Gospel Rock Village is intended to prioritize the pedestrian within the central open spaces whereby vehicle access is largely accommodated at the outer edge of the Neighbourhood. The following guidelines further define building design responses to support a pedestrian-friendly environment:</i>		
Pedestrian facilities shall link to form an interconnected network of sidewalks and trails, linking individual buildings to semi-public and public open space and parks.	The plaza, green lane and extensive sidewalk network connect pedestrians within the development and to open space/ parks	Y
Buildings and open spaces should be wheelchair accessible.	Site grades of plaza to hotel and apartment building entrances are wheelchair accessible. Movement to/around townhouses have stairs.	Y with limitations
To provide small commercial frontages, commercial retail bays should be no wider than 10 metres in order to create a fine-grained pattern of shops. A maximum spacing of 8 metres for entrances is desired along the key pedestrian-oriented high streets. A larger retailer may combine bays internally; however, the external bay articulation should be maintained.	Half of hotel building frontage on plaza is for restaurant seating, which would complement an active pedestrian plaza experience. Retail along north frontage of Village High Street shows 5 retail entrances that meet frontage spacing guideline. One larger retail entrance at northwest corner (Village High St and Village Crescent/Shaw Road extension) shown, which does not have additional retail bays, but may have internal division for smaller shops. Two storey retail space and can have internal stairs.	Y/N
Commercial retail buildings shall be oriented towards, and shall meet the sidewalk at grade, and are encouraged to be built to the property line so that a continuous commercial street frontage and positive street definition are maintained.	Grade level entry from plaza to retail and restaurant. Grade level entry from Village High Street and Village Crescent, meeting zoning setback requirements.	Y
Commercial retail frontages should engage and enliven the pedestrian realm through visual access (maximized glazing) and by way of distinguished display materials, signage, and lighting.	Retail frontages have large windows, taller 4.2m ceilings and space for retail signage, as well as post lighting in plaza.	Y

Table 7: Landscaping

DPA No. 10 Guidelines (Gospel Rock Village)	Staff Comments	Addresses Criteria (Y/N)
<b>Landscaping</b> <i>Landscaping of commercial and multi-family areas in the Gospel Rock Village is key to situating development within its landscape context. Site planning and landscaping contribute to neighbourhood character and aesthetics, livability, and environmental sustainability. Site planning should strive to minimize building coverage, preserve natural features and minimize rainwater run-off. For all developments:</i>		
A detailed landscape plan shall be prepared by a BC Registered Landscape Architect.	Landscape plan prepared by Perry and Associates Inc., dated Nov 23, 2018 and updated August 19, 2019.	Y
Trees, shrubs and ground covers should be drought tolerant, low maintenance varieties suitable to the local growing conditions and climate, and complimentary to the native dryland forest ecosystem.	On site plants include native and 'adapted' species, selected for low water requirements.	Y
Native plant materials are preferred and shall include a mix of coniferous and deciduous species. Invasive species are prohibited.	Several native plant materials selected. No invasive species in plant list.	Y
Landscape structures – such as of arbors, archways, or pergolas – are encouraged to transition between indoor and outdoors places. An encroachment permit may be supported for trellises encroaching up to 1.8 metres onto the sidewalk provided they are not less than 2.75 metres above the sidewalk and there is no conflict with street trees or streetscape elements, such as lighting and signage.	"Pedestrian Gateway Features" are referenced in two locations entering townhouses (no details provided). Three "feature stone walls", a "feature step wall" and one "trellis" indicated at pedestrian connection points.	Y
The retention of natural tree stands is encouraged.	Landscape plan for development does not refer to retention of natural trees. Site grading will not permit retention of tree stands within the village centre development area. Approximately 49% of the 47 acre site is zoned as Parks, Recreation and Open space.	Y

Table 8: Parking

DPA No. 10 Guidelines (Gospel Rock Village)	Staff Comments	Addresses Criteria (Y/N)
<b>Parking</b> <i>Parking in the Gospel Rock Village DPA shall comply with the following guidelines:</i>		
Off-street parking shall be provided underground with the exception of a small number of short-term convenience surface parking stalls.	Majority of parking is underground parking structures beneath new buildings with direct elevator access to residential and commercial spaces. Some surface parking is along main public streets for visitors to community and parks. Parking does not meet zoning requirements which will need to be addressed prior to issuance of a building permit.	Y
Parkade entrances and associated components (such as doorways, ramps, etc.), whether accessed from the street or lane, should be architecturally integrated into the building so as to minimize its exposure.	Hotel, apartment and townhouse buildings have two parkade entrances each. Entrances do appear integrated into building appearance, at this time. Further details on parkade gate materials to be provided at building permit stage.	Y
Where off-street parking is provided, it should not visually dominate a development. Parking areas should incorporate significant landscaped areas within the lot. No more than 10 parking stalls in a row should be allowed without a landscaping break.	Off-street parking is broken up with fewer than 10 stalls in each location and with vegetative street tree plantings and swales.	Y
Where surface parking is required, provide pedestrian amenities (such as benches, shelters, alcoves, seating, walls, trellises and arbors) as a buffer between the sidewalk and the building or parking edge and pathways to the adjacent businesses.	Surface parking near green lane southern entry has 'entry seat wall' on either side of pathway. One trellis structure noted in west courtyard entrance to townhouses. Public plaza includes 3 benches and moveable furnishings.	Y
Electric Vehicle charging stations shall be provided.	Electric vehicle charging stations will be provided in all three parkades.	Y

DPA No. 10 Guidelines (Gospel Rock Village)	Staff Comments	Addresses Criteria (Y/N)
<b>Parking</b>		
<i>Parking in the Gospel Rock Village DPA shall comply with the following guidelines:</i>		
Low, dense screening of street fronting stalls is required. This can be achieved through the use of landscape materials or a combination of landscape features such as arbors or trellises which run the length of the parking area.	Street fronting stalls have street trees and planted swales.	Y
Support service facilities and structures such as loading bays, refuse containers, storage areas, and utility services should be located and screened with walls, fencing, hedging, planting, other screening materials or a combination of these materials to minimize visibility from public areas.	All garbage, recycling, and utility services are underground and will not be visible from public areas.	Y

Table 9: Signage

DPA No. 10 Guidelines (Gospel Rock Village)	Staff Comments	Addresses Criteria (Y/N)
<b>Signage</b>		
<i>Signage in the Gospel Rock Neighbourhood DPA shall comply with the following guidelines:</i>		
All signs should be architecturally integrated within the overall design of buildings and landscape and may require modification of corporate or franchise design elements:	Required to submit signage details at time of building permit. Overall strategy to align signage with architecture of Gospel Rock design materials, form and style.	Y
<ul style="list-style-type: none"> <li>Freestanding signs should be restricted to a maximum height of 4.5 metres above grade.</li> </ul>	Signage shown on suspended panels above retail stores.	Y
<ul style="list-style-type: none"> <li>Freestanding signs should reflect a West Coast character by using elements of wood and/or stone.</li> </ul>	General comments made about consistent architectural language and west coast character.	Y
Multi-unit buildings are encouraged to have an attractive, simple, single entry sign rather than multi-tenant signs which create a cluttered appearance.	Signage details to be provided at time of building permit.	?
Changeable illuminated copy signs shall not be permitted.	Signage details to be provided at time of building permit.	?

DPA No. 10 Guidelines (Gospel Rock Village)	Staff Comments	Addresses Criteria (Y/N)
<b>Signage</b> <i>Signage in the Gospel Rock Neighbourhood DPA shall comply with the following guidelines:</i>		
Internally illuminated (backlit box) signs shall not be permitted.	Signage details to be provided at time of building permit.	?
External lighting for fascia and wall signs should be directed downward which may be achieved with the use of goose neck style lighting fixtures. An illumination design needs to ensure all wiring and conduits are concealed.	Signage details to be provided at time of building permit.	?

**Figure 4:** Example of signage proposed for Hotel and suspended panel signage above retail entrances.



### Zoning Bylaw Conformance

The Zoning Bylaw changes in October 2018 introduced site specific land use requirements for this project under the new CDA-4 zone. The table below highlights the values for key requirements. See figure 2 for Block 7 site subzones referred to in the zoning summary table within the Comprehensive Development Area Zone 4 (CDA-4)

Table 10: CDA-4 Zoning Bylaw limits and proposed

Topic	Zoning Bylaw Limits (CDA-4)	Shown in Designs
<b>Floor Area Limits</b>	<p><u>Subarea 1</u> – 1A, 1B, 1C and 1D recognized as ‘village centre’ with permitted mix of apartment, commercial and tourist accommodation uses. Maximum gross floor area for service commercial and retail uses, except for a restaurant use associated with principal tourist accommodation use in subarea 1B, shall be 464m<sup>2</sup>.</p> <p><u>Subarea 2</u> – Multifamily Residential zone permitting townhouses. Maximum number of units shall be 150. residential development outside of the ‘village centre’, primarily modeled after the RM-2 zone. Building floor area of third storey not exceed 70% of the second storey building floor area.</p> <p><u>Subareas 3 and 4</u> – single and two family residential – 23.8 dwelling units per hectare (9.7 dwelling units per acre)</p> <p><u>Subareas 5, 6, 7</u> – residential development outside of ‘village centre’ primarily modeled after single family R-2, R-3, RM-2.</p>	<p><u>Subarea 1:</u> Subarea 1B gross floor area retail is shown at 460m<sup>2</sup> (4951 ft<sup>2</sup>)</p> <p><u>Subarea 2:</u> Townhouses are shown with reduced third storey building floor area.</p> <p><u>Subareas 3 &amp; 4:</u> meets maximum allowable units per hectare.</p> <p><u>Subareas 5,6,7:</u> do not have floor area limits.</p>
<b>Height</b>	<p><u>Subarea 1</u> – 4 storeys 15 m (49 ft), except a principal building in Subarea 1B may have a fifth storey, not exceeding 55% of the floor area of the storey beneath. Step lower 5 storey portion at a 1:2 ratio beginning at the lesser of 4 storeys and 15m (49ft).</p> <p><u>Subarea 2</u> – Maximum height of buildings is the lesser of 3 storeys and 11.5m (38ft)</p> <p><u>Subareas 3, 4, 5, 6, 7</u> – Single family residential – not exceed height of 8.0m (26.2 ft).</p>	<p>Extensive site grading cut/fill will affect reference for natural grade. Building height defined as “the vertical distance from average grade to mean the elevation between average eave level and the ridge top of the sloped roof.”</p>
<b>Setbacks</b>	<p><u>Subarea 1</u> - Setbacks all property lines min. 3m (9.8ft)</p> <p><u>Subarea 2</u> – Front lot line setback is 3.0m (9.8ft)</p> <p><u>Subareas 3 and 4</u> – Front lot line setback is 3.0m (9.8ft); rear 5.0m (16.4 ft); interior side lot line 1.2m (4.0ft) and exterior side lot line 2.4m (8.0ft) at street.</p> <p><u>Subareas 5, 6, 7</u> – Single family residential – Minimum front lot line setback 3.0m (9.8ft). Rear 7.5m (24.6ft), interior side lot line 1.5m (4.9ft), exterior side lot line 3.0m (9.8ft)</p>	<p><u>Subarea 1:</u> site plans show building setbacks not less than 3m.</p> <p><u>Subarea 2:</u> confirmed.</p> <p><u>Subareas 3, 4, 5, 6 &amp; 7:</u> setbacks checked at time of building permit.</p>



### **Parking**

The Zoning Bylaw designations for this site refers to the regular parking requirements listed in Part 6 of the Zoning Bylaw, and shared parking may be applied to Subareas 1 and 2.

The current proposal does not meet Part 6 of the Zoning Bylaw and will require a variance for any requested reduction in parking requirements. The table below highlights the proposed parking and requirements, and what is shown on the plans.

Table 11: Parking Requirements per Part 6 Zoning Bylaw No. 1065

<b>Topic</b>	<b>Part 6 Parking Requirements</b>		<b>Shown in Designs</b>
<b>Vehicle Parking</b>	Hotel (50 rooms) and Restaurant (100 seats)	76 stalls required	66 proposed (-19 stalls)
	Commercial/ retail (460 m <sup>2</sup> )	11– up to 25% shared parking permitted	Proposing shared parking – up to 25% permitted
	Apartment (49 units)	74 stalls required	70 proposed (-4 stalls)
	Townhouses (48 units)	96 stalls required	96 proposed
	<b>VEHICLE PARKING SUMMARY</b>	<b>257 stalls required</b>	<b>232 proposed (-25 stalls) Parking variance requested</b>
<b>Visitor Parking</b>	Visitor parking required for townhouse or apartment use. 15% must be available for visitors		Included in vehicle parking count
<b>Spaces for Disabled Persons</b>	Where 100 or more spaces, 2 plus one space for every 50 or a portion thereof exceeding 100		Included in vehicle parking count
<b>Bicycle Parking</b>	Hotel & Retail Class 1	14 required	14 provided
	Hotel & Retail Class 2	15 required	20 provided
	Apartment Class 1	62 required	62 provided
	Apartment Class 2	10 required	10 provided
	Townhouse Class 1	60 required	65 provided
	Townhouse Class 2	10 required	10 provided
	<b>BICYCLE PARKING SUMMARY</b>	<b>136 Class 1 35 Class 2</b>	<b>141 provided (+5 Class 1) 40 provided (+5 Class 2)</b>

A parking study was completed by WSP on April 2, 2019, with an update memo submitted July 17, 2019. Proposed on site vehicle parking is 232 parking stalls, which is 25 stalls short of the parking bylaw requirement of 257 vehicle parking spaces. The applicant is requesting a parking variance for the 25 spaces.

To address the shortage in on-site vehicle parking, the applicant proposes to provide the following to support a request for parking variance:

- Providing on site secure cycling facilities (which meet and exceed required bike parking)
- Shared parking (shared between hotel, restaurant and commercial uses, up to 25%)
- Providing 4 car sharing vehicles and parking stalls (to be purchased by the developer and managed by Coast Car Co-op. The Coast Car Co-Op has expressed willingness to contribute to the car sharing program at Gospel Rock Village).
- 2 dedicated parking stalls with 15-minute limit at the plaza entrance

The rationale and strategy for parking reduction is included in Attachment B – “Gospel Rock Village Shared-Use Parking Strategy” prepared by WSP, dated July 17, 2019.

The developer is proposing to provide 232 on-site vehicle parking stalls, plus 4 car sharing spots and two 15-minute limit parking stalls on the plaza resulting in a total of 238 parking spots provided for the Gospel Rock Village development. Should Council support the requested variance, registration would be on title of the properties to ensure delivery of car share program committed.

### **Council Policy 3.9 – Evaluating Variances**

In March 1997, Council passed a policy to consider variance of standards specified in the Zoning Bylaw or Subdivision & Development Bylaw, where it can be demonstrated by the applicant that:

- (1) The request is not within the jurisdiction of the Board of Variance;**

*Staff note that the request is not within the jurisdiction of the BOV.*

- (2) The same request has not been previously denied by the Board of Variance;**

*As above, this has not been considered nor denied by the BOV.*

- (3) The variance will not result in significant negative impacts on neighbouring properties;**

*The variance will reduce parking and should not impact neighbouring properties, as car share vehicles, shared parking spaces between commercial uses, and additional bicycle parking is proposed to off-set parking impacts.*

- (4) There is a demonstrated need for the variance in order to permit reasonable use of the property;**

*The parking study completed by WSP, dated July 17, 2019, indicates a need for a parking reduction, to encourage transportation alternatives to the automobile for a compact and pedestrian-friendly development, and is supported by the parking professional.*

- (5) The overall intent of the original bylaw requirement or standard is not compromised;**

*The intent of the parking requirements is to ensure sufficient parking on site. The variance reduces the required parking, and compensates the impacts with 4 car-share vehicles, additional bicycle parking, and shared parking spaces between commercial uses. The parking study concludes that the proposed parking is expected to meet the parking demand during peak hours.*

- (6) The variance does not appear to establish a precedent for other properties, but responds to a site-specific situation or difficulty;**

*This application is for a site-specific development of a neighbourhood village centre. This is a new development intended to be designed as a high-density, compact, pedestrian-friendly development and encourage non-auto modes of transportation.*

- (7) As per the Municipal Act provisions, does not result in a change in land use or an increase in permitted density;**

*A parking reduction will not change the land use or density.*

- (8) The variance results in suitable development that is an asset and compatible in the context of surrounding uses;**

*Staff considers that the variance would result in a suitable development for the village centre of the Gospel Rock Village. The development is new in it's surrounding context and uses with the newly adopted CDA-4 zone.*

- (9) In the case of variance of specific development standards for off-site servicing, that any additional costs that may be incurred by the municipality or future land owners are considered.**

N/A

Staff recommends that the PDC supports the requested parking variance to reduce the 257 on-site parking space requirement to 232 on-site vehicle parking spaces, with 4 car share vehicles and parking spaces, and two 15-min limit parking stalls on the plaza, subject to:

- a) Greenlane providing four dedicated on-site car share parking spaces and four vehicles for the car share parking spaces;
- b) A letter of credit for \$50,000 to be provided to the Town in respect of the provision of each shared vehicle, with the letter of credit:
  - a. reduced to \$20,000 after provision of the following:
    - i. a copy of an executed legal agreement between the property owner and a car-sharing service providing for the operation of the shared vehicle at the shared vehicle parking space;

- ii. confirmation in writing from the car-sharing service that funds sufficient to acquire the shared vehicle are in hand or that the acquisition has been otherwise completed; and
  - iii. a shared vehicle parking space is clearly accessible to members of the car-sharing service at all times, is individually and legibly marked for exclusive use of a shared vehicle;
- b. returned in full upon written confirmation by the car-sharing service that it has operated a shared vehicle at the shared vehicle parking space for a minimum of six years after an occupancy permit has been issued for the Building in respect of which the shared vehicle was provided.

#### **POLICY / PLAN IMPLICATIONS**

##### **Strategic Plan Implications**

N/A – the Strategic Plan is under review.

##### **Financial Plan Implications**

Financial implications of the Gospel Rock Village development include the collection of Development Cost Charges (DCCs) and the expansion of the property tax base in the Town. Offsetting the expanded tax base is the additional cost for the maintenance and operation of Town services in a new neighbourhood.

##### **Official Community Plan**

The Gospel Rock Village proposal is closely aligned with the Gospel Rock Neighbourhood Plan in the OCP. This report analyses where the form and character meets the criteria under the DPA No. 10 guidelines, and where it does not. Through this analysis, staff considers that the overall design is mostly aligned with the guidelines, except for a few key components:

- Where feasible, minimizing site grading and retaining natural vegetation is encouraged – village grading will not retain natural landscape and vegetation.
- The form and character should support and enhance the Gibsons small-town character – the proposal provides for a west coast mountain experience.
- To the extent possible, locally sourced materials should be used to reduce transportation impacts and reflect the local climate, light, history and culture – this is unknown due to the timing of construction and availability of products at that time.
- Public art details are unknown at this time.
- Natural landscapes, including significant trees, should be incorporated into site development when feasible – staff have had discussions with the developer on

protecting some remaining arbutus trees, and the developer supports a covenant on rear yards fronting the park to protect significant trees. However, this is mostly in the single-family dwelling areas, outside of the scope of this Development Permit review.

- Signage details are unknown at this time. Future sign permits may be obtained at time of building permit.

### **NEXT STEPS**

In advance of proceeding with this development, Greenlane Homes Ltd. is required to obtain several development permits. Relevant Development Permit requirements that apply to this site include:

- DPA No.1 Geotechnical Hazards – to be reviewed and authorized by staff, prior to approval of subdivision;
- DPA No. 2 Environmentally Sensitive Areas – to be reviewed and authorized by staff, prior to approval of subdivision;
- DPA No. 9 Gibsons Aquifer – to be reviewed and authorized by staff, prior to Building Permit, if excavation exceeds 1.5 m in depth on 6 lots at northeast corner of Block 7;
- DPA No. 10 Gospel Rock Village Form and Character – reviewed by staff and Planning and Development Committee, and to be authorized by Council, per this report.

In addition to Development permits, the applicant is requesting Council review and consideration of a requested parking variance to allow a reduction in onsite parking.

A recommendation from the Planning and Development Committee will be forwarded to Council for consideration. Should Council agree with the PDC's recommendation, it may pass a resolution to that effect. Alternatively, Council may pass another resolution.

Upon receipt of a Council resolution authorizing the issuance of the Development Permit, the developer will continue to complete the subdivision requirements before obtaining a Building Permit to commence construction.

### **RECOMMENDATIONS / ALTERNATIVES**

Staff recommendations are listed on page 1. Alternatively, the Planning and Development Committee may pass a recommendation to support the issuance of the Development Permit as presented:

*AND THAT the Planning and Development Committee supports DP-2018-24, DP-2018-25, and DP-2018-26 for the Gospel Rock Village as proposed;*

Or the PDC may request further changes to the proposed form and character:

*THAT the applicant for Gospel Rock Village be requested to revise \_\_\_\_\_ [the form and character] in order to address \_\_\_\_\_.*

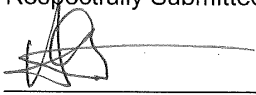
Or the PDC may not support the requested parking variance.

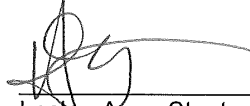
**Attachments**

- Attachment A – Gospel Rock Village – Village Centre Development Permit Application Plans, August 19, 2019.
- Attachment B – Gospel Rock Village Shared-Use Parking Strategy, WSP, Updated July 17, 2019.

Respectfully Submitted,

for

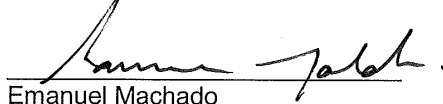
  
Odet Pinho, MCIP, RPP  
Agora Planning Consultant



Lesley-Anne Staats, MCIP, RPP  
Director of Planning

**CHIEF ADMINISTRATIVE OFFICER'S COMMENTS:**

I have reviewed the report and support the recommendation(s).

  
Emanuel Machado  
Chief Administrative Officer





**GOSPEL ROCK VILLAGE  
VILLAGE CENTRE DEVELOPMENT PERMITS APPLICATION  
AUG 19, 2019**





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DESIGN RATIONALE/RESPONSE TO DESIGN GUIDELINES

16.11 Gospel Rock Village Development Permit Area No. 10

Updated Aug 19, 19

GUIDELINES

- 1. Development Permits issued in this area shall be in accordance with the following guidelines:
- 2. General Form and Character of Development

JYWA Response: The heart of Gospel Rock Village (Phase 1 Development) is the main Plaza (Village Green), which is surrounded by commercial and residential space (restaurant, Hotel, retail, live/work spaces and Apartment/Townhouses). It has a “Village-like” setting because of its natural surroundings and, logistically, its distance from the centre of the Town of Gibsons. This Village setting suggests a more destination/”resort-like” character. This is because the developer made a deliberate decision to concentrate on the allowed density of the property to the centre of the Village to allow more of the natural forest to be preserved as permanent greenspace surrounding the development. This concentration of density (three to four-storey massing) will gradually transition to single-family buildings (60 in total) to the edge of the Village, and will be buffered by the existing forest. This will minimize any negative impact on viewing and shadowing to the surrounding existing residential neighbourhood. It is the developer’s intent that this development should “grow” naturally from its local character. The building style will be mainly referenced by the local “West Coast style,” including form and character, use of building material, as well as the colour selection. The developer is also trying to maximize the use of local resources, including material and labour, design, management, marketing, etc. The goal is to have the local community embrace this new development as a natural extension of the beautiful Town of Gibsons, while rejuvenating the community with new members who will enjoy its natural setting and embrace the community life.

- 3. To create a high quality, visually appealing development with a safe, comfortable pedestrian environment, the following guidelines shall apply:
    - Where feasible, minimizing site grading and retaining natural vegetation is encouraged.
- JYWA Response: Changes to the existing grades are minimal; where there are public roads and greenspaces, final finish grades are kept at the maximum slope (at 10%) allowed by the standards of the Town of Gibsons.
- Significant landscaping adjacent to public roadways and integrated within the site, use of both architectural and landscape features to provide a “gateway” or distinct entrance.

JYWA & P+A  
Response:

The landscape design enhances the public greenspace design, which follows the Greenspace concepts of the planning approach set by the Rezoning parameters.

Starting from the “Village Green,” a Plaza will be surrounded by a Café/Restaurant on the ground floor of the Hotel on the west, and work/live units on the lower level of the Apartment building on the east. The Plaza’s activities will begin from a rock outcrop retained from the existing land form as the focal point and spread out to the Plaza, among the freely-placed seasonal chairs, and to a Café with outdoor seating. An artwork piece will be located at the south end of the Plaza, which transitions to the “Green Lane” - a wider, open space formed by two and one-half storeys of Townhouse Buildings.

The Green Lane will incorporate a large central lawn boulevard that will double as a fire-fighting access. The lawn will designed to withstand firefighter loading requirements with the use of special pavers. The central lawn will be flanked by pathways, columnar trees and landscape planting to soften the edge closest to the townhouses.

The Green Lane has been further broken down into two “Courtyards,” surrounded by larger trees, feature planting and benches to encourage social interaction between neighbours.

Continuing south, the ‘Green Lane’ extends into the Park space connecting residents to “Little Africa” and “Cross Rock” - a carefully preserved natural park space composed largely of currently existing landscape features. Public trails begin from this point and will lead pedestrians further to oceanfront greenspaces.

An east and west pedestrian connection has also been provided that extends through the site allowing for mid development access to help increase pedestrian accessibility and connectivity. These pathways will be characterized by feature planting, wayfinding and potential public art opportunities to signify the public nature of the path system at the connection to Village Crescent.

Alternate pedestrian entrances that lead into the East and West Courtyard areas will be framed with landscape trellis features to act as a gateway between the street and the internal courtyard. These trellis nodes will include resident amenities such as notice/info boards, mailboxes and bike racks.

- ***The form and character should support and enhance the Gibsons small-town character, including a mix of building mass and open spaces reflective of more incremental development.***

JYWA Response: Architectural design of the buildings is aimed at reducing the building masses. Articulating architectural details with a “West Coast” style will emphasize the character and location of the building environment. Timber construction details are used extensively to build the elevations for each building; variations to the scale and size of these timber elements reflect an intimate scale for the Townhouses, mixed-use for Apartment Buildings, and a public scale for the Hotel/Commercial Building. Layers of different building materials (stone, brick, etc.) and a variation of subtle colour differences help to break building massing, yet are consistent enough to bring together a coherent picture of a West Coast-style Village Centre. This design and the rich texture of building fabric will help to emphasize the “small town” character of this new community. We feel that this approach will also help to set the “right tone” for future development of the remaining areas within the Gospel Rock Village.

- ***Provide a clear distinction between private and public realms through subtle cues in materials, pathway edges, grade and/or landscape design elements.***

JYWA & P+A Response: A subtle hierarchical order of timber construction elements on elevations of the Hotel, Apartment and Townhouses will establish a distinctive character for each building. The Buildings also frame open public greenspaces. The character of the landscape design further enhances these subtle differences in the character of each greenspace. Variations of the exterior colours are selected based on the coastal landscape, and the exterior building materials enhance the “small town” character of the Village Centre area.

The transitions between private and public realm will be distinguished in the landscape in a variety of ways. At the property line, paving materials will change from City Standard paving types to unit paving and saw-cut concrete. Careful attention will be given to the detailing of the transition of these materials so that they are subtle and harmonious.

Landscape structures such as trellises, feature walls and signage will further assist in delineating the private and public realm. As design refinement progresses, P+A will look for every opportunity to provide additional subtle cues through planting and material selection.

- ***Each dwelling unit in a residential or mixed-use project shall incorporate direct access to a usable private outdoor space such as a patio, balcony, and/or upper level terrace.***

JYWA Response: All units have access to usable private outdoor space, such as a patio, balcony or terrace. Most of the units also have additional access to semi-public greenspace; e.g., the Commons and the Green Lane. These greenspaces lead gradually to the Park - a much more open and natural

landscape, such as “Little Africa” and “Cross Rock” - and to oceanfront beaches.

- ***West Coast seaside design features should be incorporated into the design, including:***

- ***The emphasis on natural light through the significant glazing and orientation of buildings to views and/or other significant natural features.***

JYWA & P+A Response: The view from each dwelling unit is maximized to take advantage of the natural setting. For example, north-facing units in the Apartment building are stepped back to allow east-facing views. The Townhouse Buildings follow the natural grades; i.e., higher on the Green Lane and lower on Village Crescent, so that all buildings benefit from the surrounding views.

A generous central lawn boulevard and narrow columnar trees have been chosen to maximize the amount of sunlight in the Green Lane.

- ***Consideration of indoor/outdoor relationships and flexibility within open-plan layouts.***

JYWA Response: Private outdoor space (patio, balcony, terrace) is provided to each unit. The connection between indoor to outdoor space in each is carefully considered to allow for the extension of indoor to outdoor living.

- ***The use of natural building materials and exposed timber (structural) features.***

JYWA Response: Mass timber elements form the main character of each building. The timber elements of each building are unique in design for that building. For example, the timber elements of the Hotel’s gables are emphasized over the public space of the Hotel, such as the Fitness Centre and Multipurpose Hall at the Hotel’s roof level. The Apartment’s gable elements are of a smaller scale for each unit, which offers a reduced roof shape when viewed from a distance. The same Townhouse timber elements are used at the entrances to the individual units, with further refined details.

- ***The use of natural colours with a variety of complementary colours used as accents to promote visual interest.***

JYWA Response: The colour selections were influenced by the West Coast landscape: different shades of water and sky; beaches; coastal forests; etc. Each building has its own combination of material and colour to support the uniqueness of its design.

- ***Low Impact Development techniques for stormwater management is encouraged – including swales or other landscape features that alleviate impact of storm runoff from impervious surfaces, including roofs and surface parking areas; green roofs and permeable parking areas are encouraged.***

P+A and Webster Response: Stormwater management is incorporated in the Civil/Subdivision package and integrated into the Landscape design. More details will be provided for the Building Permit.

Onsite stormwater will be collected from landscape paving areas and directed to soft landscaping and lawn areas via bio-swales where possible.

Bio-retention methods will also be employed through the use of a rain garden at the south end of the Green Lane. The rain garden will collect water from the adjacent landscape and hard surfacing in the Green Lane, collect sediment and mitigate peak flows.

All additional uncaptured stormwater from roof leaders and hardscape will be directed to/through onsite stormwater features for treatment prior to discharge to the municipal system.

- o **Design lighting to minimize light spill, glare and sky glow by using non-glare full cutoff fixtures.**

JYWA Response: The use of downlights for the street lighting and other Village Green feature lighting is considered in the design. Specifications will be provided for the Building Permit. In general terms our lighting designs strive to provide allowable Lighting Power Densities below the ASHREA required LPD’s of the current BCBC. In fact, our LPD’s typically exceed the mandated LEED values as well.

- o **Recycling facilities are required for all commercial, multi-family residential, and mixed-use buildings.**

JYWA Response: A Garbage and Recycling Room is provided in all buildings.

- o **To the extent possible, locally sourced materials should be used to reduce transportation impacts and reflect the local climate, light, history, and culture. Additionally, the following materials are encouraged:**

- **Recycled materials with a high-recycled content.**
- **Concrete with at least 25% fly ash or slag.**
- **Wood products certified CSA Sustainable Forest Management Standard or equivalent.**
- **Interior finishes and installation methods with low toxic emissions.**

JYWA Response: This will be incorporated into the detailed building design. Specifications for the listed materials will be provided in the Building Permit Application and will meet all requirements.

- o **To the extent possible, buildings should be designed to reduce energy usage by incorporating standards such as solar ready and LEED.**

JYWA Response: The buildings will be designed to meet the new Building Code BCBC 2018 with current energy-saving design features and will adhere to all energy-saving guidelines established by the Building Code 2018. An energy modeller will be retained to provide options for confirmation that the buildings will be adequately energy efficient. An energy modeller can model options for providing an improved building envelope, improved glazing as well as various mechanical upgrades to show conformance and/or “improved performance” of BCBC 2018 and Step Code requirements.

- o **Opportunities for the inclusion of public art should be explored in public space, especially plazas and other public open space to enhance the overall open space network.**

P+ A Response: There are five potential locations for artwork that have been identified on the site plan located at major pedestrian and neighbourhood axis and viewpoints. These locations are meant to be schematic only and would be adjusted to suit the artist’s vision and specific requirements to accurately accentuate the artwork piece:

- o Village High Street and Village Crescent: This intersection will serve as the primary access point to the development and will host prominently as a gateway and arrival space.

Public art is considered in this space to help reinforce the prominence of the corner as it will be viewed by those entering the neighbourhood.

- o Village Green Art Location: Centrally located to be viewed from all points in the Plaza as well as Village High Street and the Green Lane. The Village Green art object could potentially act as an organizational device in the Plaza; i.e., as a prospect or in a more subtle presentation; i.e., furnishing, functional art, lighting, etc. Connections to the surrounding artist studios and the function of the Plaza as a meeting space and multi-use area would be encouraged.

- o East and West Pedestrian Connections @ Village Crescent:

These locations could hold a wayfinding function denoting the entrance point to the east/west pedestrian path connection to the Green Lane spine.

Both of these art locations are characterized by the mass of the building and a grade change directly adjacent to them. An artwork could be incorporated into signage and wayfinding and as a counterpoint to the massing.

- o South Green Lane + Park Access:

This special node is located along the central Green Lane path at the main access to the public Park. Art should consider gateway elements and theming to reflect the Park and nature beyond. There is also potential to have this art-piece relate to the Village Green artwork as they could visually tie to one another

Overall, the art pieces and placement should respect the land, local geography and natural history of the space. Inclusion of Indigenous artists and stakeholders should be explored. Refer to Landscape Plan L1.0 for the proposed art locations.

- **Opportunities to display Sᓵwᓴwú7mesh art, language and culture in public space are especially encouraged.**

JYWA Response: The Developer is open to partnership with the Town of Gibsons and First Nation groups to engage the First Nation community for their meaningful input. The Team will work with the First Nation group and local artists to facilitate the selection of artwork and placement in open public spaces. Locations for the public art have been identified on the Landscape Drawings.

**Building Form, Scale, and Massing**

To encourage varied building forms and to avoid creation of a commercial strip image, the following guidelines respecting massing and scale shall apply:

- **Varied building forms will be encouraged.**

JYWA Response: The Village Centre has three different types of buildings: Hotel, Apartment Building and Townhouses - all connected by underground parking.

Hotel: will be framed out with covered timber balconies, the size of each to be determined by the number of Hotel rooms. Recessed amenity spaces located on the fifth (5th) level will have an overhead sloped roof to capture the best view for hotel guests and neighbourhood residents. A ground floor café/restaurant will open to and animate the Plaza. Included are a few small retail spaces starting from the corner of Shaw Road and Chaster Road where there will be a major intersection for the whole Gospel Rock neighbourhood. Retail spaces continue on Village High Street to the Plaza and will be connected by a covered pedestrian walkway, which is necessary in our climate.

Apartment Building: has multiple pitched roofs over the fourth (4th) floor roof of the east portion of building. The building is stepped downward to 4-storey, then to 2-storey Townhomes on Village Crescent; four live/work units with distinctive façades are located on the Plaza side. These units may be preferred by artists or small business owners.

Townhouses: are divided into four buildings, connected by u/g parking. Building A has three clusters, each having three sloped-roof Townhomes that are stepped to follow the street grading, and one two-storey unit at the end, which has a roofline similar to a single-family house in order to reduce the building scale. Buildings B and C are on both sides of the Green Lane. Each building consists of two clusters of three-storey Townhouses with sloped roofs and dormers over a pair of Townhouses; and, a two-storey pair of recessed, sloped roof Townhouses in between, with gable ends facing the street to break down the building mass.

Similar to Building A, the last unit facing Village Crescent has bay windows and balconies, traditionally used for a single-family house. Building D has

three clusters of flat roofed, two-storey Townhouses that are stepped to follow the street grading. The units have different widths for the frontages, which gives a natural break between the building mass. The Elevator to Pavilion F is located at the end of Building D, facing Village Crescent. This is a one-storey building with two single sloped roofs over the elevator shaft and stairs. The roof shape of Pavilion F is very similar to the other freestanding elevator pavilion, Pavilion E in the East Village Common.

- **Large areas of blank wall are not acceptable on a face with a pedestrian or residential area orientation.**

JYWA Response: All building façades have been treated with architectural elements such as windows, bay windows, balconies, material changes and reveals, and some wall art.

- **Long, unbroken building lines and rooflines should be avoided.**

JYWA Response: See response above regarding Building Form  
All building façades have been treated with architectural elements such as windows, bay windows, balconies, material changes and reveals, and some wall art

- **Wall lines should be offset and modulated to create visual interest.**

JYWA Response: Similar to the treatment of the building form and roof lines, consideration has been given to each building for the wall lines and façade articulation:

Hotel: Has a strong one-storey high brick arcade surrounding the ground floor which supports the timber-framed balconies of the hotel rooms above. The timber frames are layered over the building in a light misty grey tone that is often found in the West Coast sky. The stone on the columns at the base of the brick arcade is carried through the Village High Street side to the façade below the Plaza grade on the underground parking wall. The 5<sup>th</sup> floor walls are recessed from the floor below. The bluish grey is reminiscent of the reflection of the ocean under a blue sky.

Apartment: has a sandy clay-toned coloured brick base on the Plaza side which is carried around the Apartment building above the dark brown coloured brick podium. The podium consists of 2-storey Townhomes on East Village Crescent and the exposed underground parking façade on Village High Street. The Townhome façade’s light misty grey colour is broken up by the same dark brown brick as the background colour of the timber balcony. The rhythm of the Townhomes hints of the upcoming Townhomes on the same side of Village Crescent.

The Townhouse façade design is coherent with the building forms. The variation of material and colour enhances the volumetric determination of the building forms: for Building A, the projected bays are accented with a light tone base colour, yet the accented coloured bays alternate between the clusters of Townhouses. For Buildings C and B, cedar-toned walls are recessed as a background colour to divide the building into



segments, and the tone of each pair of Townhouses varies according to its typology. The different frontage widths of Building C’s units are consistent with the material and colour variations. This rhythm of light walls against recessed dark walls offers continuity in the streetscape of East Village Crescent.

- **Pitching, stepping down, and/or varying rooflines should be incorporated to vary height in the roofscapes of buildings**

JYWA Response: See response above regarding Building Form

- **On sloped sites, building forms should step down with the natural grade of the site to avoid imposing masses on the downhill slopes.**

JYWA Response: All buildings are designed to follow the natural street grading of the hilly site in order to reduce the building mass:

Hotel: has four storeys on the Plaza side with stepped levels down to 2 storeys at the intersection of Shaw and Chaster, where the Village entrance is.

Apartment Building: on the other side of Village Crescent, the Apartment is stepped from four storeys on the Plaza to four storeys, then down to two storeys at the east of Village Crescent, following the natural descending grades.

Townhouses: are designed to follow the natural grades in both east/west and north/south directions. Buildings A and D are positioned lower than Buildings B and C, and are stepped to follow the street grading. Buildings B and C are situated on the natural hilltop where the Green Lane is located. The garden suites are positioned below the Townhomes to bridge the grade differences between the buildings and enable a more levelled East Courtyard and West Courtyard.

- **Where commercial development incorporates a residential or multi-unit use on upper floors, a graded transition in the building height is desired to ensure adjacent properties are not faced with a massive wall.**

JYWA Response: Hotel: contains some commercial use, such as a Café/Restaurant and small retail units. They are all located on the ground and street level. Hotel rooms are located above and are well-separated from commercial activities. Larger walls are treated with openings, reveals and material/colour variations. Other residential apartments are located on the opposite side of the Plaza.

- **Longer frontages of townhomes will articulate “clusters” of not more than three homes in order to avoid repetitive forms.**

JYWA Response: See responses above regarding “Building forms” and “Wall lines”: Townhouse Building A has been divided into three “clusters” of three Townhomes each. Building C and Building B are composed of two Townhomes for each “cluster”. Building D’s unit is designed with a different frontage for each home. Therefore, each is unique to the other.

- **Building form and character will create visual interest through the subtle articulation of massing, roof forms and changes in materials and colours.**

JYWA Response: See responses above regarding “Building forms” and “Wall lines”

- **Street-facing units and units fronting a public space/green street should utilize a layering of elements – including but not limited to street-facing stairs, stoops, porches, patios, and landscaping to transition between private-use and the public realm:**

- **ground floor units should be elevated between 0.5-1 metre above the street. If the ground floor is not elevated, other means of separation and/or buffering for privacy should be utilized**

JYWA Response: The majority of ground floor units are elevated from the sidewalk of the adjacent street. Due to the sloped site, the ground floor elevation of each unit to the sidewalk is different; some are less than 0.5 metres, and some are higher than 1 metre. However, all ground floor units are well-screened from the street by a landscape buffer for privacy purposes.

- **gates, railings, walls and landscaping providing a buffer from the street and creating a clear distinction between the public and private realm shall be provided**

P+A Response: All Townhouses facing the street and/or courtyards have a low fence and planting material as a privacy screen.

- **Landscape buffer elements should be 0.9 – 1.2 metres tall.**

P+A Response: All private courtyard fences are 1 to 1.2 metres, and planting material is selected to be a similar height.

▪

**Siting of Building and Structures**

To maintain a strong street presence while also framing the interior open space (Village Green and Greenlane), the following guidelines respecting siting of buildings shall apply:

- **Buildings should be sited with the entrance to the buildings facing the public street and the internal open space (Village Green and/or Greenlane) to activate public spaces and signal access and priority for pedestrians.**

JYWA Response: Both main entrances to the Hotel and the Apartment Building are located on the Village Green. All Townhouses are accessed from either Village Crescent or the Green Lane.

- **Buildings situated along the Village portion of the Greenlane are intended to represent a higher degree of formal design to help frame the Village Greenlane and signal a transition from the Lookout Park to the Village.**

JYWA Response: Townhouse Buildings B and C are symmetrically situated on both sides of the Green Lane. The feeling of formality is evoked by the symmetry in contrast with the beautiful natural landscape. This balanced composition presents a formal transition from the Village Green overlooking the Park in the foreground.

- **Natural landscapes, including significant trees, should be incorporated into site development when feasible.**

P+A Response: We intend to retain natural land forms in the development area. The site development will include Outlook Park where native plants will be used in all replanting efforts. Natural vegetation will be retained where possible and additional native plants will be added as needed. The remainder of the site will be kept in its natural state.

Residential/Commercial Buffers

Effective transitions between commercial areas and adjacent residential properties can be achieved by:

- **Fencing, combined with a broad area of landscape plantings (trees, shrubs).**  
P+A Response: See response above regarding “Street-facing units.”
- **Dense shrubbery or hedges capable of impeding travel through to adjacent properties, and which is not visually transparent to adjacent properties.**  
P+A Response: Buffer planting including evergreen hedging will be used between residential properties in addition to landscape fencing, gates and privacy screens.  
  
Landscape plant massing will be used to soften building edges and corners and assist with residents’ privacy.
- **Creating a transition zone to complement the scale of the development, not less than 3.0 metres in width.**  
JYWA Response: All buildings are designed with a minimum three-metre setback from the property lines. Landscape buffers are provided.
- **Minimizing the potential for overlook to neighbouring windows and private spaces.**  
JYWA & P+A Response: The Apartment Building and Townhouse buildings are located with ample distance between each other. The minimum distance between residential windows is nine (9) metres.  
  
A mix of coniferous and deciduous trees will be used throughout the landscape to further assist with screening and overlook.
- **Careful positioning of lighting, parking and access points to minimize impact on adjacent properties.**  
JYWA Response: Each Townhouse has its own entrance and is separated from the other by a fence and plant material. Light fixtures are located on the underside of the entrance porch roof. Specifications for the light fixtures will be provided for the BP.

Amenity Space/Private Areas (for Multi-Unit Residential Buildings)

The location and size of outdoor spaces such as patios and balconies have a considerable effect on the sense of privacy. New residential buildings should be designed to provide privacy for each resident; through windows, private outdoor spaces of balconies, or through changes in grade for elevation as follows:

- **An outdoor living space/patio terrace of a minimum 2.3 metres depth for townhouses and minimum size of 10 m² (110 ft.²) is recommended.**

JYWA Response: All Townhomes have a front yard and a backyard patio. Many of them have additional roof decks and balconies.

- **Where yards are accommodated, a minimum 5 metres depth and a minimum area of 30 m² (325 ft.²) is recommended.**

JYWA Response: All Townhouses have a yard, or a deck where sloped terrain is accommodated by a minimum five (5) metre depth.

- **Apartments should have a minimum 1.5 metres depth terrace or balcony, sufficiently large to create a usable outdoor “room.” Balconies should be at least half enclosed in order to give the occupant privacy, security and weather protection.**

JYWA Response: All Apartment units have a balcony with a minimum one and one-half (1.5) metre depth. Some of them have an additional patio and/or deck.

- **Changes in grade can provide for private areas between street edges and the development units.**

JYWA Response: Provided. Refer to response above.

Pedestrian Environment

The design of Gospel Rock Village is intended to prioritize the pedestrian within the central open spaces whereby vehicle access is largely accommodated at the outer edge of the Neighbourhood. The following guidelines further define building design responses to support a pedestrian-friendly environment:

- **Buildings shall emphasize a human-scale and pedestrian orientation with the Neighbourhood by presenting 1 – 2 storey façade elements to engage pedestrian edges. Single-storey elements are preferred. This may be achieved by:**

▪ **well-marked building entrances**

JYWA Response: The main entrance to the Apartment Building is located on the Village Green/Plaza. Each Townhouse and Garden Suite has its own entrance facing the Street and the Green Lane.

▪ **architectural features**

JYWA Response: The Apartment building has recognizable two-storey live/work units facing the Plaza and two-storey Townhomes facing the Village Crescent; the remaining units are recessed one to two storeys from the ground, depending upon the site grade. The third floor of each Townhouse is recessed, with a one-storey porch or canopy at the street level. All Townhouse end units facing the street are specifically designed to reduce the building mass to two storeys, with features similar to those of a single-family house.

▪ **an emphasis on the fenestration (the arrangement and positioning of windows)**

JYWA Response: The fenestration has been deliberately positioned in the Townhouses to provide a pedestrian-friendly experience and privacy at the same time.

- **weather protection through the use of awnings, arcades and canopies along commercial storefronts and building entries. Weather protection must be integral to the building form, and the materials and colours used should be consistent with the façade.**

JYWA Response: The brick Arcade around the Hotel’s ground floor commercial space provides the perfect weather protection and the base for timber-framed balconies above.

- **Pedestrian facilities shall link to form an interconnected network of sidewalks and trails, linking individual buildings to semi-public and public open space and parks.**

JYWA Response: The Village Centre’s primary pedestrian network includes the periphery sidewalks along Village Crescent, Village High Street and the Village Green to the Green Lane. An internal east/west pedestrian path is provided mid-block, linking West Village Crescent to the east by passing Village Green and the Green Lane. This path connects people from public street to the semi-public space, the Commons. This network is also connected to the larger pedestrian network, as planned for the Block 7 and Gospel Rock neighbourhood.

- **Buildings and open spaces should be wheelchair accessible.**

JYWA Response: All Village Centre buildings are accessible by wheelchair, as per the Building Code. Village Green, the Green Lane and the Commons are also accessible. The Hotel has one wheelchair-accessible room.

- **To provide small commercial frontages, commercial retail bays should be no wider than 10 metres in order to create a fine-grained pattern of shops. A maximum spacing of 8 metres for entrances is desired along the key pedestrian-oriented high streets. A larger retailer may combine bays internally; however, the external bay articulation should be maintained.**

JYWA Response: Retail spaces on Village High Street have multiple storefronts. The largest store facing the corner of Chaster and Shaw Roads is less than 5,000 sf. and is dividable.

- **Commercial retail buildings shall be oriented towards, and shall meet the sidewalk at grade, and are encouraged to be built to the property line so that a continuous commercial street frontage and positive street definition are maintained.**

JYWA Response: Refer to responses above. All retail spaces are built to meet the sidewalk or pedestrian walkway.

- **Commercial retail frontages should engage and enliven the pedestrian realm through visual access (maximized glazing) and by way of distinguished display materials, signage, and lighting.**

JYWA Response: Storefront display windows are provided for all retail spaces.

**Landscaping**

LaLandscaping of commercial and multi-family areas in the Gospel Rock Village is key to situating development within its landscape context. Site planning and landscaping contribute to neighbourhood character and aesthetics,

livability, and environmental sustainability. Site planning should strive to minimize building coverage, preserve natural features and minimize rainwater run-off.

For all developments:

- **A detailed landscape plan shall be prepared by a B.C. Registered Landscape Architect.**

JYWA Response: Yes. P+A Landscape Architecture has been engaged for this project.

- **Trees, shrubs and groundcovers should be drought-tolerant, low maintenance varieties suitable to the local growing conditions and climate, and complimentary to the native dryland forest ecosystem.**

P+A Response: Onsite Plant materials, including native and “adapted” species, will be selected for low-water requirements and drought tolerance and to complement the native dryland forest ecosystem.

- **Native plant materials are preferred and shall include a mix of coniferous and deciduous species. Invasive species are prohibited.**

P+A Response: We intend to use a robust palette of native and adapted plant species. P+A will apply a thoughtful approach to species selection to help minimize maintenance requirements and attraction of local wildlife (i.e., deer and bears).

- **Landscape Structures – such as of arbors, archways, or pergolas – are encouraged to transition between indoor and outdoor places. An encroachment permit may be supported for trellises encroaching up to 1.8 metres onto the sidewalk, provided they are not less than 2.75 metres above the sidewalk and there is no conflict with street trees or streetscape elements, such as lighting and signage.**

P+A Response: Landscape trellises are used as gateway features at pedestrian access locations into the Village Courtyard areas. It is not anticipated that they would be at the sidewalk but, rather, would be located within the 3-metre building setback, if possible.

There is an additional landscape structure in the West Courtyard to serve as an all-season meeting space and picnic area, complete with weather protection.

- **The retention of natural tree stands is encouraged.**

P+A Response: There are no onsite trees identified for retention as the site will be excavated to accommodate the Parkade. Existing trees will be retained offsite in the Park area and dedicated land.

**Parking**

Parking in the Gospel Rock Village DPA shall comply with the following guidelines:

- **Off-street parking shall be provided underground with the exception of a small number of short-term convenience surface parking stalls.**

JYWA Response: In order to make Gospel Rock Village a pedestrian-orientated community, off-street parking for this project is mostly provided by underground

parking structures beneath each of the new buildings, with direct access (via elevators) to residential and commercial spaces. Along the main public streets, surface parking stalls are provided with an integrated streetscape design, mainly for visitors to the community and the surrounding parks. As promised by the developer (Greenlane Homes Ltd.), existing greenspaces are for the people of the Town of Gibsons to enjoy. The visitor parking spaces are provided for that purpose.

Detailed parking stall calculations, based on the requirements of the Parking Bylaw of the Town of Gibsons and the recommended share-use strategy, will be presented by a traffic engineer’s report. The strategy is to balance the functional requirements of the community and minimize a ‘cut and fill’ to the existing site topography.

- **Parkade entrances and associated components (such as doorways, ramps, etc.), whether accessed from the street or lane, should be architecturally integrated into the building so as to minimize its exposure.**

JYWA Response: The Hotel, Apartment Building and Townhouses will share parking and other services’ access, and will have maximum reduced exposure to the street. The Hotel and Townhouses share one driveway crossing for underground parking and services. The Apartment Building will share its driveway crossing with the second entrance to the Townhouse Parkade. By combining the buildings’ street access, the street exposure will be effectively reduced.

- **Where off-street parking is provided, it should not visually dominate a development. Parking areas should incorporate significant landscaped areas within the lot. No more than 10 parking stalls in a row should be allowed without a landscaping break.**

JYWA Response: The majority of off-street Parking stalls are located in underground Parkades. The few surface parking spaces are buffered with landscape planting from the street.

- **Where surface parking is required, provide pedestrian amenities (such as benches, shelters, alcoves, seating, walls, trellises and arbors) as a buffer between the sidewalk and the building or parking edge and pathways to the adjacent businesses.**

JYWA & P+A Response: Surface parking stalls are buffered with planting material and grade differences.

- **Electric vehicle charging stations shall be provided.**

JYWA Response: Electric vehicle charging stations will be provided in all three Parkades.

- **Low, dense screening of street fronting stalls is required. This can be achieved through the use of landscape materials or a combination of landscape features such as arbors or trellises which run the length of the parking area.**

JYWA & P+A Response: See response above. Hardy drought-resistant planting will be provided to help screen parking from the street and sidewalk.

- **Support service facilities and structures such as loading bays, refuse containers, storage areas, and utility services should be located and screened with walls, fencing, hedging, planting, other screening materials or a combination of these materials to minimize visibility from public areas.**

JYWA Response: See responses above. Support service facilities such as: loading; garbage recycling, etc.; and the parking entrances are arranged in a very compact manner. There are only two service lanes for the entire Village Centre. All garbage/recycling rooms are in covered spaces connected to the Parkade, with internal access for the user. The lower Parking entrance to the Apartment Building is screened with trellises from the Courtyard. The other Townhouse Parkade entrance is hidden from the street by the grade difference.

Signage

Signage in the Gospel Rock Neighbourhood DPA shall comply with the following guidelines:

- **All signs should be architecturally integrated within the overall design of buildings and landscape and may require modification of corporate or franchise design elements:**
  - **Freestanding signs should be restricted to a maximum height of 4.5 metres above grade.**
  - **Freestanding signs should reflect a West Coast character by using elements of wood and/or stone.**
- **Multi-unit buildings are encouraged to have an attractive, simple, single entry sign rather than multi-tenant signs which create a cluttered appearance.**
- **Changeable illuminated copy signs shall not be permitted.**
- **Internally illuminated (backlit box) signs shall not be permitted.**
- **External lighting for fascia and wall signs should be directed downward and use goose neck style lighting fixtures. An illumination design needs to ensure all wiring and conduits are concealed.**

JYWA & P+A Response: Overall Strategy

The strategy for signage is to consider multiple functions of all signage to be utilized on site. Careful consideration of the built form of site signage will provide the opportunity to establish an architectural language that defines Gospel Rock Village. All signage should be tasteful, appropriately-scaled and strategically placed. Material and construction will reflect the quality and durability, and the form and style will be consistent with the new project brand.

Wayfinding

The signage will assist residents and visitors in effortlessly orientating themselves while navigating within the neighbourhood. Additionally, signage will help establish an effective transition between public and private space, helping to ensure managed access to the sensitive ecosystem of the Park and conservation area.

Interpretation and Safety Notice

Signage will also include interpretative components related to site



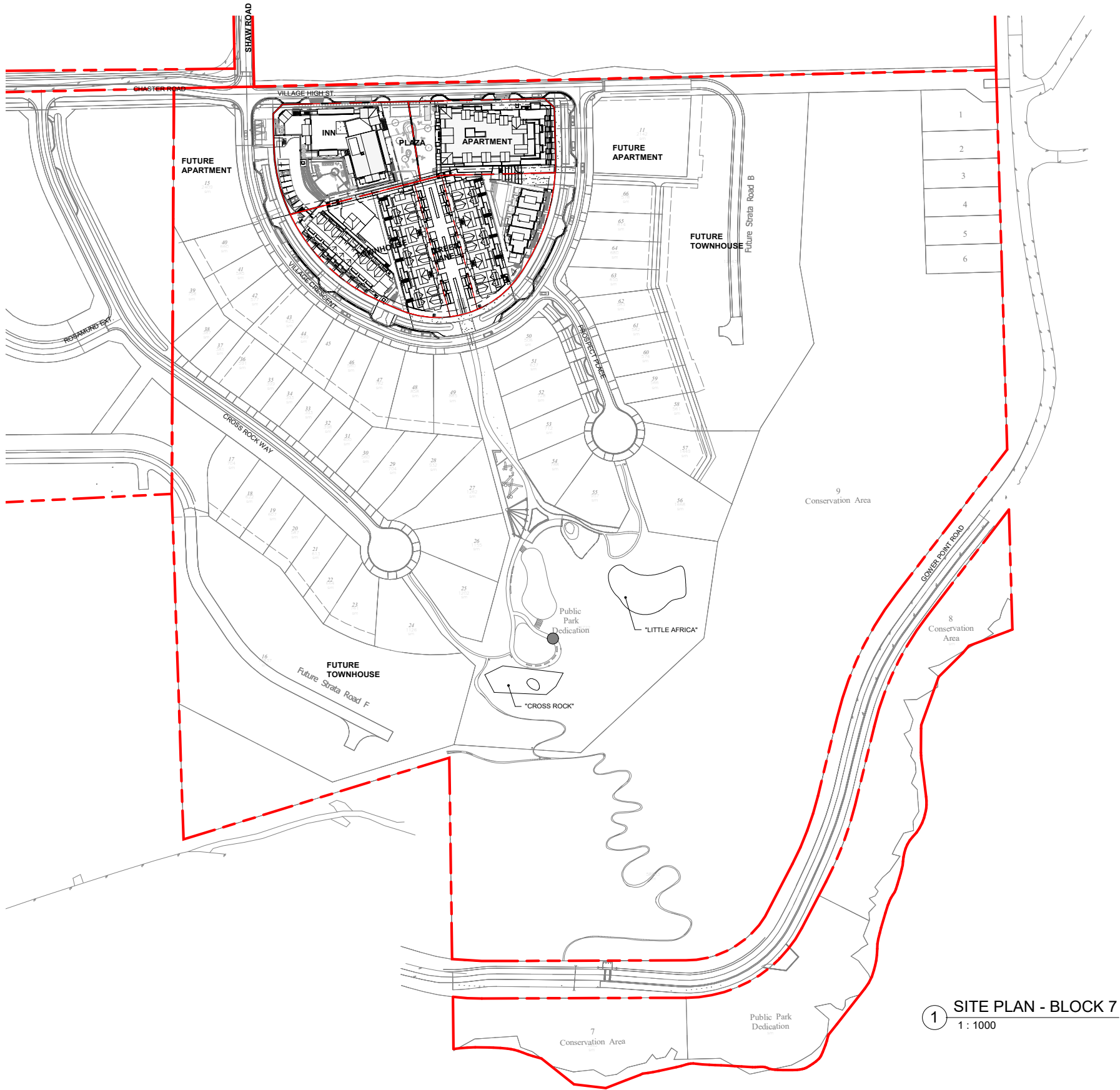
history, placemaking, acknowledgement and communication of the unique qualities and characteristics of Gospel Rock Village. The signage will be in various forms and placed in various locations. It will provide residents and visitors with a range of information in respect to both property access, as well as legal liability.

In the Village Centre building design, the corner retail store’s signage is over the main entrance canopy, as shown on the Drawing. Other retail stores and Café/Restaurant’s signs are on suspended panels in the Arcade. Additional retail signs will be located inside the Arcade above each door. Signage for the Hotel is located on the stair tower facing west, and over the entrance canopy on the Plaza.

Signage located within the Village Centre landscape will employ materials consistent with the West Coast character of the development. This will include the use of stone and wood where possible, supported by durable construction practices, and detailing that is scaled appropriately for the pedestrian realm.

Landscape signage will take the form of wayfinding and directional information signage that will be part of a complete package to ensure clarity and consistency of the signage aesthetics, materials and graphics throughout the project.

Opportunities to include artwork, cultural, historical and environmental interpretation and community information as well as coordination with the Town of Gibsons’ requirements will be considered during the signage package development.



Attachment A  
Page 12 of 80

**KEY PLAN**

**GOSPEL ROCK VILLAGE**  
BLOCK 7 SUBSANS BC  
PROJECT NUMBER: 1003

**SITE PLAN**

**A000**

No.	DATE/NO.	Description	Consultant
1	2018/173	ISSUED FOR CP	
2	2018/07/19	REVISED FOR CP SUBMISSION	

**JYW ARCHITECTURE INC.**  
211-211 Columbia Street, Vancouver, B.C. Canada, V6A 2R5  
Tel: 604 680 2568 Email: jyw@jyw.ca

VILLAGE CENTRE INN



DRAWING LIST	
A100	PROJECT INFORMATION
A101	SITE PLAN
A201	LEVEL B2 FLOOR PLAN
A202	LEVEL B1 FLOOR PLAN
A203	LEVEL 1 FLOOR PLAN
A204	LEVEL 2 FLOOR PLAN
A205	LEVEL 3 FLOOR PLAN
A206	LEVEL 4 FLOOR PLAN
A207	LEVEL 5 FLOOR PLAN
A400	COLOUR & MATERIAL INSPIRATION
A401	ELEVATIONS
A402	ELEVATIONS
A501	SECTIONS

PROJECT INFORMATION		Attachment A	
PROJECT NAME:		VILLAGE CENTRE INN	
PROJECT ADDRESS:		GIBSONS, B.C., CANADA	
LEGAL DESCRIPTION:		LOT 14 BLOCK 7 DISTRICT LOT 842 GROUP 1 NEW WESTMINSTER DISTRICT PLAN EPP90953	
ZONING:		CDA, 1-B	
GROSS SITE AREA:		3798 m²	
SITE COVERAGE:		89.5%	
BUILDING HEIGHT:		4 STOREYS or 15.0 m (49 ft); 5TH FLOOR AMENITY SPACE.	
SETBACK:			
NORTH SETBACK		3.0m	
SOUTH SETBACK		3.0m	
EAST SETBACK		3.0m	
WEST SETBACK		N/A	


BUILDING FLOOR AREA SUMMARY		
LEVEL B2		
LEVEL B2 CIRCULATION		121 m²
LEVEL B2 PARKING		1119 m²
LEVEL B2 RETAIL		136 m²
LEVEL B2 SERVICE		107 m²
		1483 m²
LEVEL B1		
LEVEL B1 CIRCULATION		119 m²
LEVEL B1 PARKING		1095 m²
LEVEL B1 SERVICE		181 m²
		1395 m²
LEVEL 1		
LEVEL 1 CIRCULATION		347 m²
LEVEL 1 COMMON AREA		48 m²
LEVEL 1 HOTEL ROOM		11 m²
LEVEL 1 OFFICE		39 m²
LEVEL 1 RESTAURANT		236 m²
LEVEL 1 RETAIL		324 m²
LEVEL 1 SERVICE		116 m²
LEVEL 1 SPA		90 m²
		1211 m²
LEVEL 2		
LEVEL 2 CIRCULATION		160 m²
LEVEL 2 HOTEL ROOM		689 m²
LEVEL 2 SERVICE		45 m²
		893 m²
LEVEL 3		
LEVEL 3 CIRCULATION		160 m²
LEVEL 3 HOTEL ROOM		767 m²
LEVEL 3 SERVICE		45 m²
		971 m²
LEVEL 4		
LEVEL 4 CIRCULATION		160 m²
LEVEL 4 HOTEL ROOM		768 m²
LEVEL 4 SERVICE		45 m²
		972 m²
LEVEL 5		
LEVEL 5 CIRCULATION		89 m²
LEVEL 5 COMMON AREA		51 m²
LEVEL 5 GYM		85 m²
LEVEL 5 MULTIPURPOSE ROOM		259 m²
LEVEL 5 SERVICE		4 m²
		487 m²
Roof		
Roof CIRCULATION		33 m²
Roof SERVICE		18 m²
		51 m²
TOTAL		7454 m²

ROOM SUMMARY					
Level	STANDARD ROOM	1 BR SUITE	2 BR SUITE	HC ROOM	
LEVEL 2	13	0	2	1	16
LEVEL 3	13	1	2	1	17
LEVEL 4	14	1	2	0	17
TOTAL	40	2	6	2	50

PARKING STATISTICS		
ZONING REQUIREMENT:		
1 STALL / SLEEPING UNIT (50)		
1 STALL / 8 SEATS OF ATTACHED RESTAURANT USE (13)		
1 STALL / 8 SEATS OF ATTACHED RES. SERVE ALCOHOL (13)		
1 STALL / 45 SQ.M. OF RETAIL USE (10)		
SMALL CAR RATIO (MAX.30%)		
TOTAL REQUIREMENT:		86
TOTAL PARKING PROVIDED		66
HC PARKING	2	3%
PARKING	49	74%
SMALL CAR	15	23%
TOTAL:		66

BICYCLE PARKING STATISTICS		
ZONING REQUIREMENT:		
TOURIST ACCOMMODATION: CLASS 1: 0.27 SPACES PER EA 100 SQ.M. (12)		
TOURIST ACCOMMODATION: CLASS 2: 0.27 SPACES PER EA 100 SQ.M. (12)		
RETAIL AND OFFICE USE: CLASS 1: 0.27 SPACES PER EA 100 SQ.M. (2)		
RETAIL AND OFFICE USE: CLASS 2: 0.4 SPACES PER EA 100 SQ.M. (3)		
TOTAL REQUIREMENT:		
CLASS 1: 14		
CLASS 2: 15		
TOTAL BICYCLE PARKING PROVIDED		
CLASS 1: 14		
CLASS 2: 20 (ON LANDSCAPE DRAWINGS)		
BICYCLE CLASS 1	LEVEL B2	4
BICYCLE CLASS 1	LEVEL B1	10
BICYCLE CLASS 1: 14		
BICYCLE CLASS 2	LEVEL B2	10
BICYCLE CLASS 2	LEVEL 1	10
BICYCLE CLASS 2: 20		

WATER CLOSET CALCULATION		
BUILDING CODE REQUIREMENT:		
ASSEMBLY OCCUPANCY:		
MULTI-PURPOSE ROOM OCCUPANCY LOAD: 273 (0.95 SQ.M. PER PERSON)		
RESTAURANT OCCUPANCY LOAD: 197 (1.2 SQ.M. PER PERSON)		
TOTAL ASSEMBLY OCCUPANCY LOAD: 470 (235 PER SEX)		
WATER CLOSET REQUIRED FOR MALES: 5		
WATER CLOSET REQUIRED FOR FEMALE: 9		
BUSINESS AND PERSONAL SERVICES OCCUPANCY:		
RETAIL OCCUPANCY LOAD: 125 (3.7 SQ.M. PER PERSON)		
OFFICE OCCUPANCY LOAD: 5 (9.3 SQ.M. PER PERSON)		
TOTAL BUSINESS AND PERSONAL SERVICES OCCUPANCY LOAD: 127 (64 PER SEX)		
WATER CLOSET REQUIRED FOR MALE: 4		
WATER CLOSET REQUIRED FOR FEMALE: 4		
TOTAL REQUIREMENT:		MALE: 9 FEMALE: 13
PROPOSED:		
FEMALE		14
MALE		7
UNIVERSAL		2



JTW ARCHITECTURE INC.  
211-011 Columbia Street, Vancouver, B.C., Canada, V6A 2R5  
Tel: 604 689 3368 Email: jtw@jtwinc.net

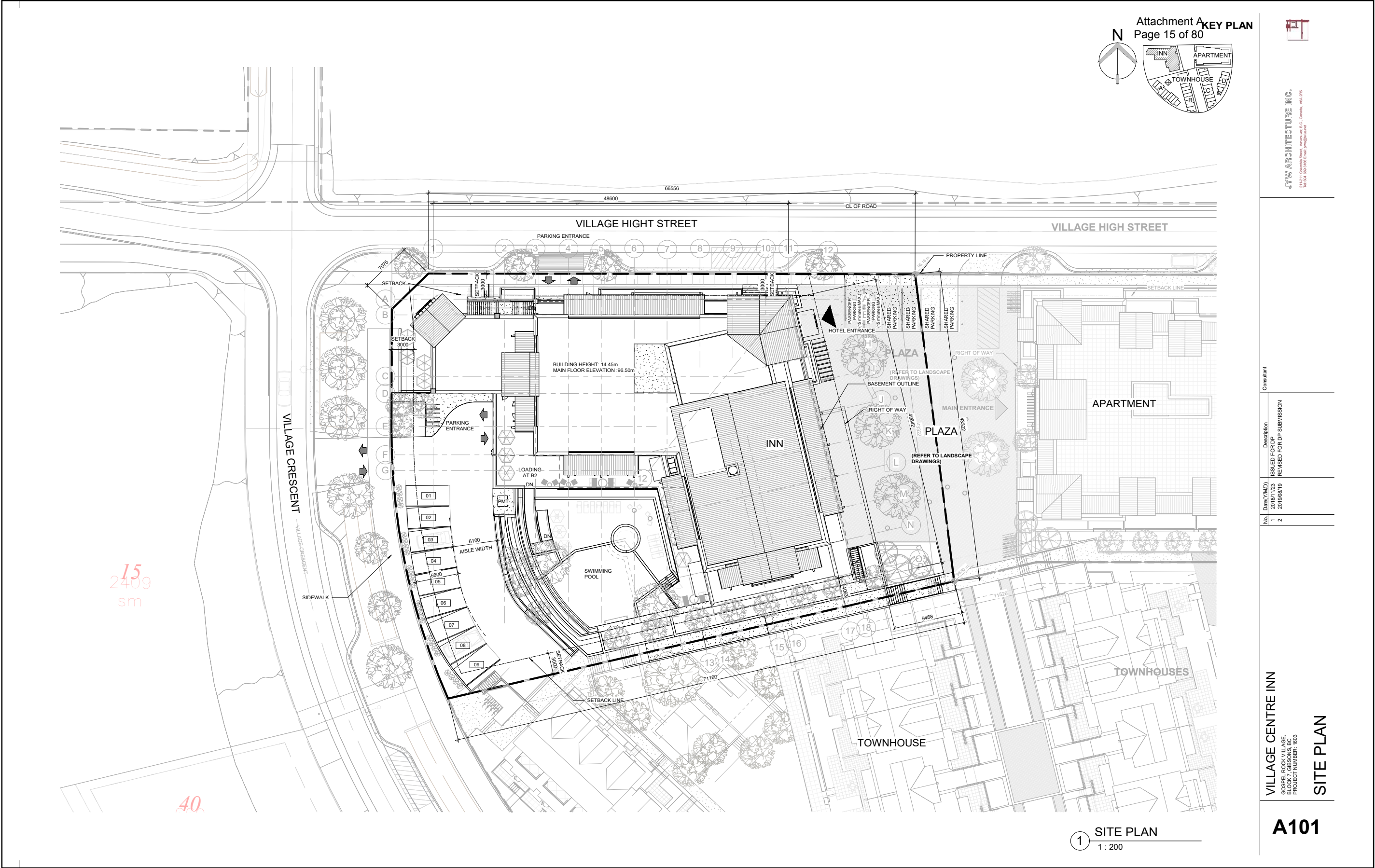
VILLAGE CENTRE INN

GOSPEL ROCK VILLAGE,  
1400 GOSPEL ROCK ROAD,  
VICTORIA, B.C. V8N 1A5  
PROJECT NUMBER: 1003

PROJECT INFORMATION

A100

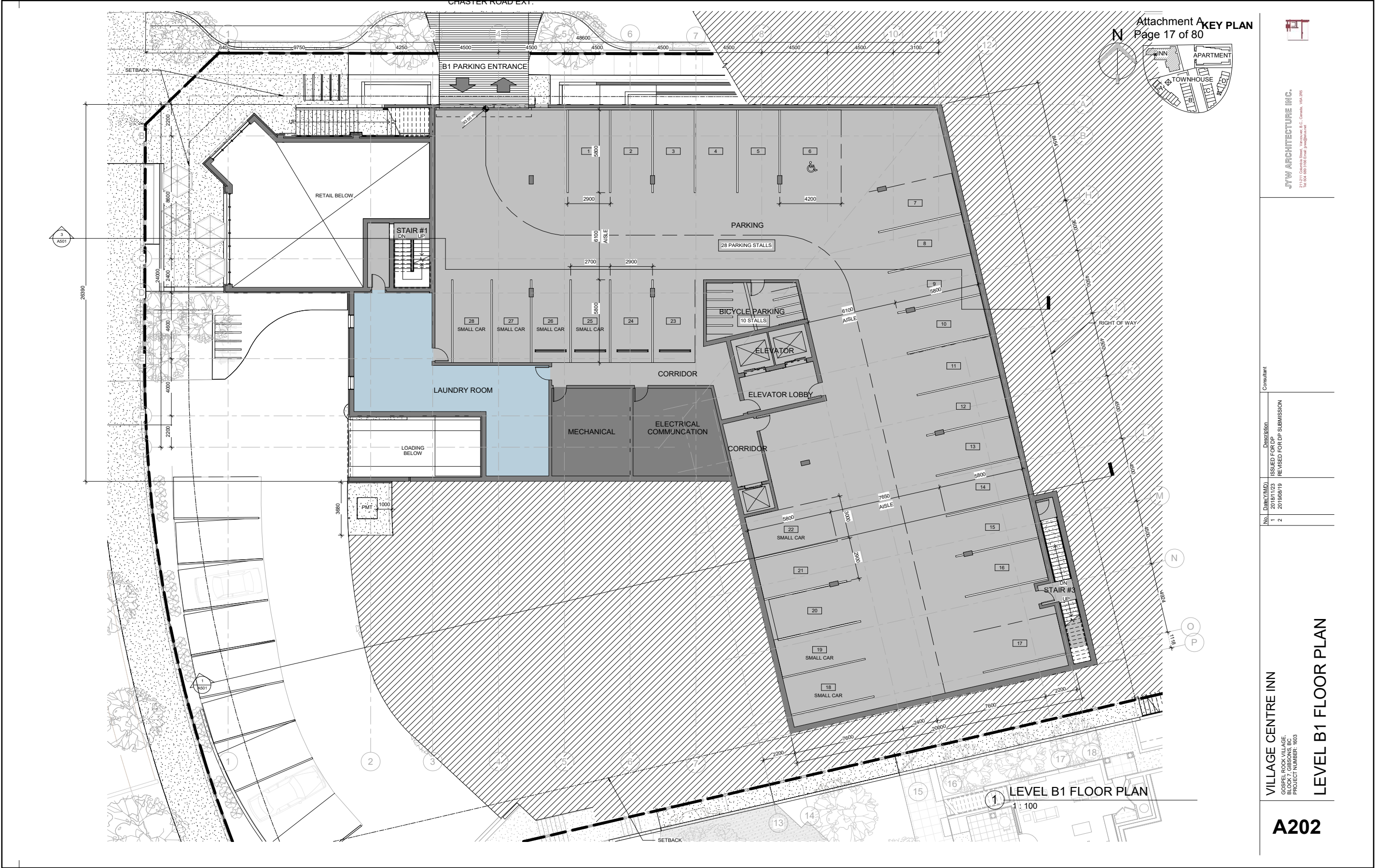










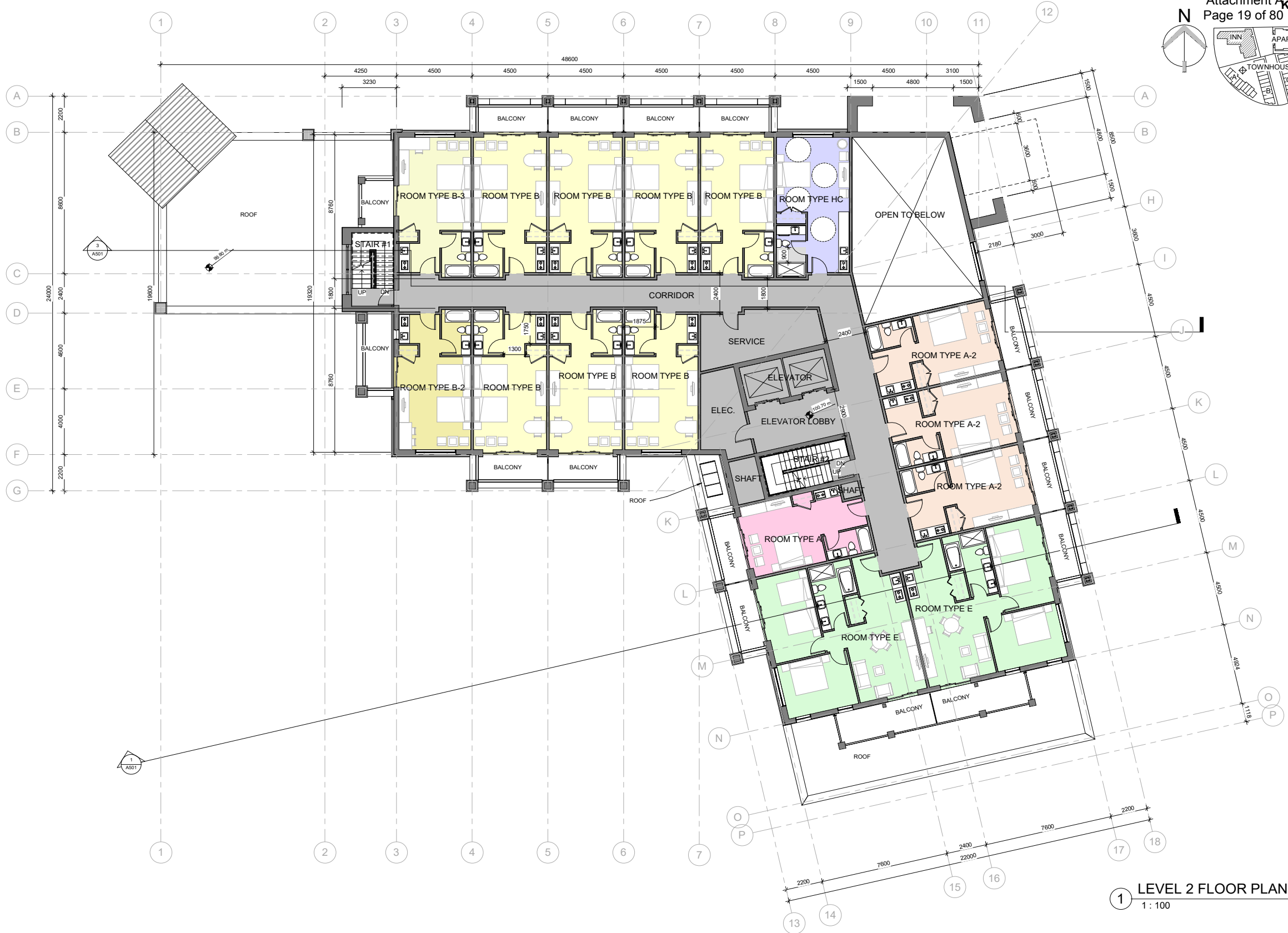


LEVEL B1 FLOOR PLAN









Attachment A  
Page 19 of 80

KEY PLAN

JYW ARCHITECTURE INC.  
211211 Columbia Street, Vancouver, B.C. Canada, V6A 2R5  
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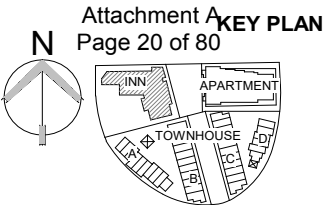
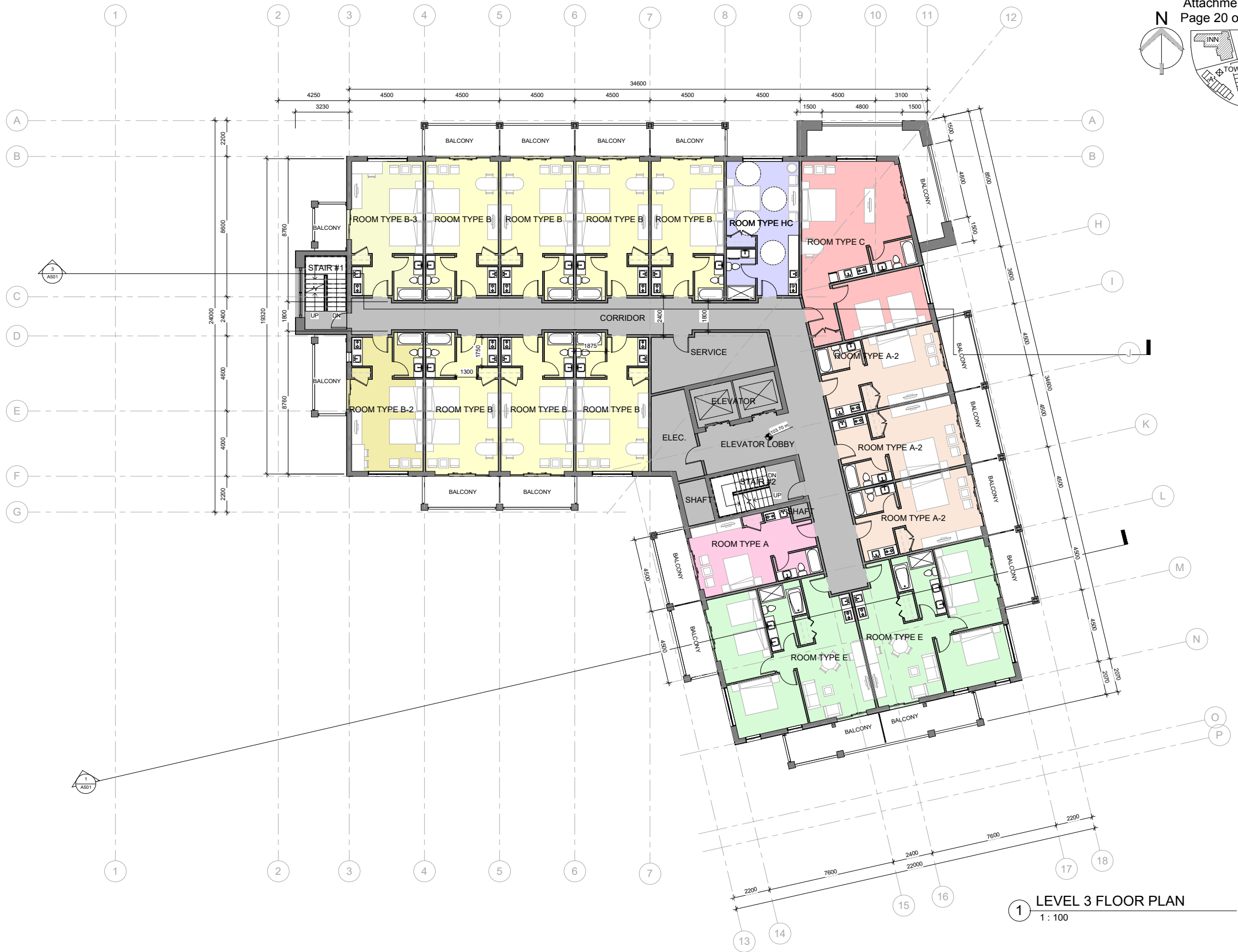
No.	Date/Rev	Description	Consultant
1	2018/11/23	ISSUED FOR CD	
2	2019/06/19	REVISED FOR CD SUBMISSION	

VILLAGE CENTRE INN  
GOSPEL ROCK VILLAGE  
1000 GOSPEL ROCK VILLAGE  
PROJECT NUMBER: 1003

LEVEL 2 FLOOR PLAN

A204

1 LEVEL 2 FLOOR PLAN  
1:100



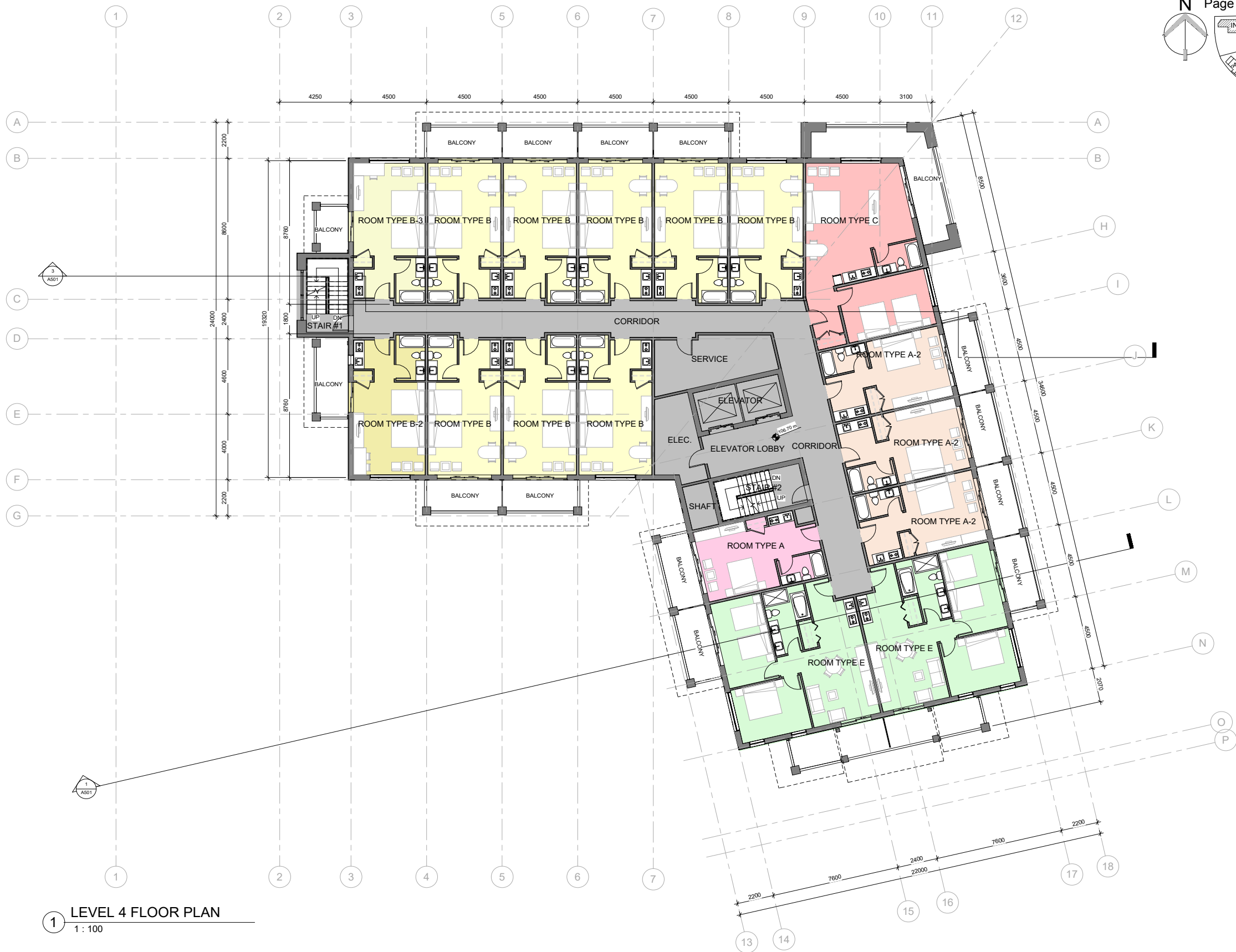
JYW ARCHITECTURE INC.  
211-211 Columbia Street, Vancouver, B.C. Canada, V6A 2R5  
Tel: 604 255-1188 Email: jyw@jyw.ca

No.	DATE/REV	Description	Consultant
1	2019/11/23	ISSUED FOR CD	
2	2019/06/19	REVISED FOR CD SUBMISSION	

VILLAGE CENTRE INN  
GOSPEL ROCK VILLAGE,  
1000 GOSPEL ROCK ROAD,  
VICTORIA, B.C. V8N 1G3  
PROJECT NUMBER: 1003

LEVEL 3 FLOOR PLAN

A205



1 LEVEL 4 FLOOR PLAN  
1 : 100

VILLAGE CENTRE INN

GOSPEL ROCK VILLAGE,  
1000 GOSPEL ROCK VILLAGE,  
PROJECT NUMBER: 1003

No. 1  
2

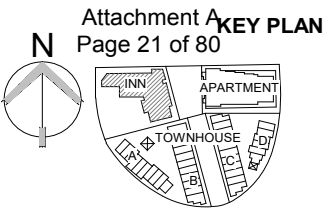
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ISSUED FOR CD  
REVISED FOR CD SUBMISSION

Consultant

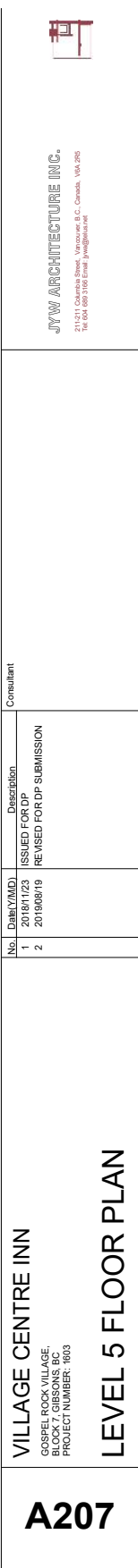
JYW ARCHITECTURE INC.  
211-011 Columbia Street, Vancouver, B.C. Canada, V6A 2R5  
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A206

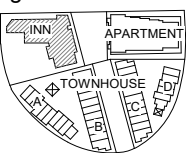
LEVEL 4 FLOOR PLAN



Attachment A  
Page 21 of 80







**JYW ARCHITECTURE INC.**  
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No.	Date(Y/M/D)	Description	Consultant
1	2018/11/23	ISSUED FOR DP	
2	2019/08/19	REVISED FOR DP SUBMISSION	

VILLAGE CENTRE INN  
GOSPEL ROCK VILLAGE,  
BLOCK 7, GIBSONS, BC  
PROJECT NUMBER: 1603

## ROOF PLAN

## A208





CERACLAD - ASH



CERACLAD - SLATE



ARRISCRAFT  
RENAISSANCE -  
CARBON



MUTUAL MATERIALS -  
EBONY



BENJAMIN MOORE -  
CC 420


No.	DATE/REVISION	Description	Consultant
1	2019/11/23	ISSUED FOR CD	
2	2019/06/19	REVISED FOR CD SUBMISSION	





EXTERIOR FINISHES	
NOTE	DESCRIPTION
1	METAL ROOFING: COLOR TO MATCH CASCADIA METALS- REGENT GREY OR SIMILAR
2	ASPHALT TORCHON ROOFING
3	FIBER CEMENT SIDING: CERACLAD- 8 REVEAL SLATE OR SIMILAR
4	FIBER CEMENT SIDING: CERACLAD- CONTEMPORARY SMOOTH ASH OR SIMILAR
5	BRICK VENEER: MUTUAL MATERIALS- EBONY OR SIMILAR
6	STONE VENEER: ARRICRAFT- RENAISSANCE CARBON OR SIMILAR
6A	STONE VENEER: NATURAL STONE
7	DECORATIVE WOOD BRACKET: COLOR TO MATCH OLYMPIC 716 CEDAR NATURAL TONE
8	DECORATIVE WOOD RAFTER: COLOR TO MATCH OLYMPIC 716 CEDAR NATURAL TONE
9	DECORATIVE WOOD TRUSS: COLOR TO MATCH OLYMPIC 716 CEDAR NATURAL TONE
12	SAFETY GLASS IN METAL POST GUARDRAIL
14	ALUMINUM FRAME WINDOW W/LOW "E" GLASS
19	WOOD / GLULAM POST: COLOR TO MATCH OLYMPIC 716 CEDAR NATURAL TONE
20	GLASS CANOPY IN STEEL FRAME
22	KITCHEN EXHAUST HOOD
23	SAFETY GLASS IN METAL POST GUARDRAIL BEHIND PARAPET

Page 25 of 80



**JYW ARCHITECTURE INC.**  
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Tel: 604 689 3166 Email: [jyw@jyw.net](mailto:jyw@jyw.net)

Consultant

No.	Date(Y/M/D)	Description
1	2018/11/23	ISSUED FOR DP
2	2019/08/19	REVISED FOR DP SUBMISSION

**VILLAGE CENTRE INN**  
GOSPEL ROCK VILLAGE,  
BLOCK 7, GIBSONS, BC  
PROJECT NUMBER: 1603

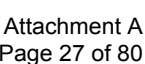
## ELEVATIONS

# A401









# A501

## SECTIONS

Description
ISSUED FOR DP REVISED FOR DP SUBMISSION

No.	Date(Y/M/D)
-----	-------------

**JYW ARCHITECTURE INC.**  
211-211 Columbia Street, Vancouver, B.C., Canada, V6A 2P5  
Tel: 604 680 3166 Email: [jyw@jyw.net](mailto:jyw@jyw.net)

VILLAGE CENTRE APARTMENT



PROJECT INFORMATION			
PROJECT NAME:	VILLAGE CENTRE APARTMENT		
PROJECT ADDRESS:	GIBSONS, BC, CANADA		
LEGAL DESCRIPTION:	LOT 12 BLOCK 7 DISTRICT LOT 842 GROUP 1 NEW WESTMINSTER DISTRICT PLAN EPP90953		
ZONING:	CDA-4, 1-C		
GROSS SITE AREA:	3017 m²		
SITE COVERAGE:	87.5%		
BUILDING HEIGHT:	4 STOREYS or 15.0 m (49 ft)      PROPOSED: 12.65 m		
SETBACKS:	NORTH SETBACK 3.0m	SOUTH SETBACK 3.0m	EAST SETBACK 3.0m
			WEST SETBACK N/A

BUILDING FLOOR AREA SUMMARY							
LEVEL	APARTMENT	LIVE / WORK	CIRCULATION	SERVICE	PARKING	BICYCLE PARKING	TOTAL AREA
LEVEL B2	335 m²	0 m²	37 m²	100 m²	1239 m²	27 m²	1738 m²
LEVEL B1	285 m²	0 m²	74 m²	27 m²	1222 m²	109 m²	1742 m²
LEVEL 1	826 m²	270 m²	181 m²	4 m²	0 m²	0 m²	1282 m²
LEVEL 2	877 m²	245 m²	102 m²	4 m²	0 m²	0 m²	1228 m²
LEVEL 3	990 m²	0 m²	102 m²	4 m²	0 m²	0 m²	1096 m²
LEVEL 4	894 m²	0 m²	92 m²	4 m²	0 m²	0 m²	990 m²
ROOF	0 m²	0 m²	33 m²	12 m²	0 m²	0 m²	45 m²
TOTAL	4207 m²	515 m²	621 m²	156 m²	2461 m²	136 m²	8121 m²

ROOM TYPE SUMMARY								
LEVEL	1 BEDROOM	1 BEDROOM +1 DEN	2 BEDROOM	2 BEDROOM +1 DEN	3 BEDROOM	LIVE / WORK	TOWNHOUSE	TOTAL UNITS PER FLOOR
LEVEL B2	0	0	0	0	0	0	5	5
LEVEL B1	0	0	0	0	0	0	0	0
LEVEL 1	0	3	1	3	2	4	0	13
LEVEL 2 (RENTAL TBD)	0	4	1	3	2	0	0	10
LEVEL 3	0	4	3	3	1	0	0	11
LEVEL 4	0	4	2	2	0	0	0	10
ROOF	0	0	0	0	0	0	0	0
TOTAL	0	15	7	11	7	4	5	49

PARKING STATISTICS		BICYCLE PARKING STATISTICS			
ZONING REQUIREMENT:	1.5 STALLS PER RESIDENTIAL UNIT	BYLAW		UNITS	REQUIRED BICYCLE PARKING SPACE
	1.5 STALLS PER LIVE / WORK UNIT	CLASS 1	1.25 spaces per dwelling unit	49	62
	TOTAL:	CLASS 2	0.2 spaces per dwelling unit		10
TOTAL PARKING PROVIDED	70	TOTAL:		72	
VISITOR PARKING RATIO		CLASS 1		62	
ZONING REQUIREMENT:	MIN: 15%	CLASS 2		10	
VISITOR PARKING:	12	TOTAL:		72	
SMALL CAR RATIO		LEVEL	COUNT	PARKING TYPE	
ZONING REQUIREMENT:	MAX: 30%	LEVEL B2	12	BICYCLE CLASS 1	
SMALL CAR	21	LEVEL B1	50	BICYCLE CLASS 1	
	30%	LEVEL 1	10	BICYCLE CLASS 2	
LEVEL	COUNT	TOTAL	72		
LEVEL B2	34				
LEVEL B1	36				
TOTAL	70				


CLASS 2 STALL ON LANDSCAPE DRAWINGS

DRAWING LIST	
A100	PROJECT INFORMATION
A101	SITE PLAN
A201	LEVEL B2 FLOOR PLAN
A202	LEVEL B1 FLOOR PLAN
A203	LEVEL 1 FLOOR PLAN
A204	LEVEL 2 FLOOR PLAN
A205	LEVEL 3 FLOOR PLAN
A206	LEVEL 4 FLOOR PLAN
A207	ROOF PLAN
A400	COLOUR & MATERIAL INSPIRATION
A401	ELEVATIONS
A402	ELEVATIONS
A501	SECTIONS

VILLAGE CENTRE APARTMENT  
GOSPEL ROCK VILLAGE,  
BLUESHIRE DRIVE, GIBSONS, BC  
PROJECT NUMBER: 1003

PROJECT INFORMATION

A100



JW ARCHITECTURE INC.  
211-211 Columbia Street, Vancouver, B.C., Canada, V6A 2P5  
Tel: 604 680 2168 Email: jw@jwarch.ca

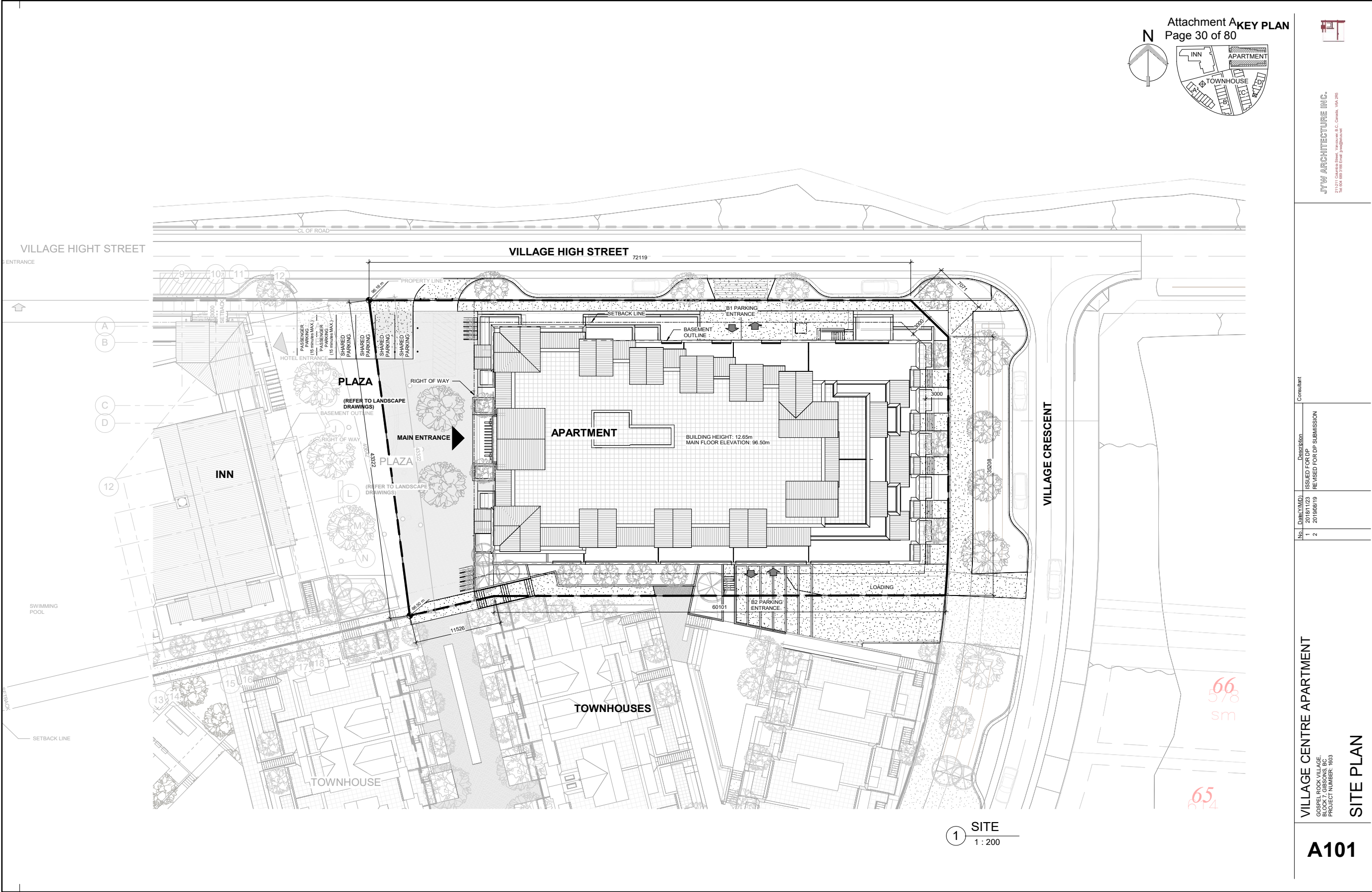
Consultant

Description:  
ISSUED FOR CDP  
REVISED FOR CDP SUBMISSION

DATE:  
2019/11/23  
2019/06/19

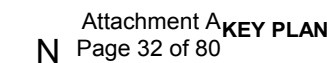
No.  
1  
2








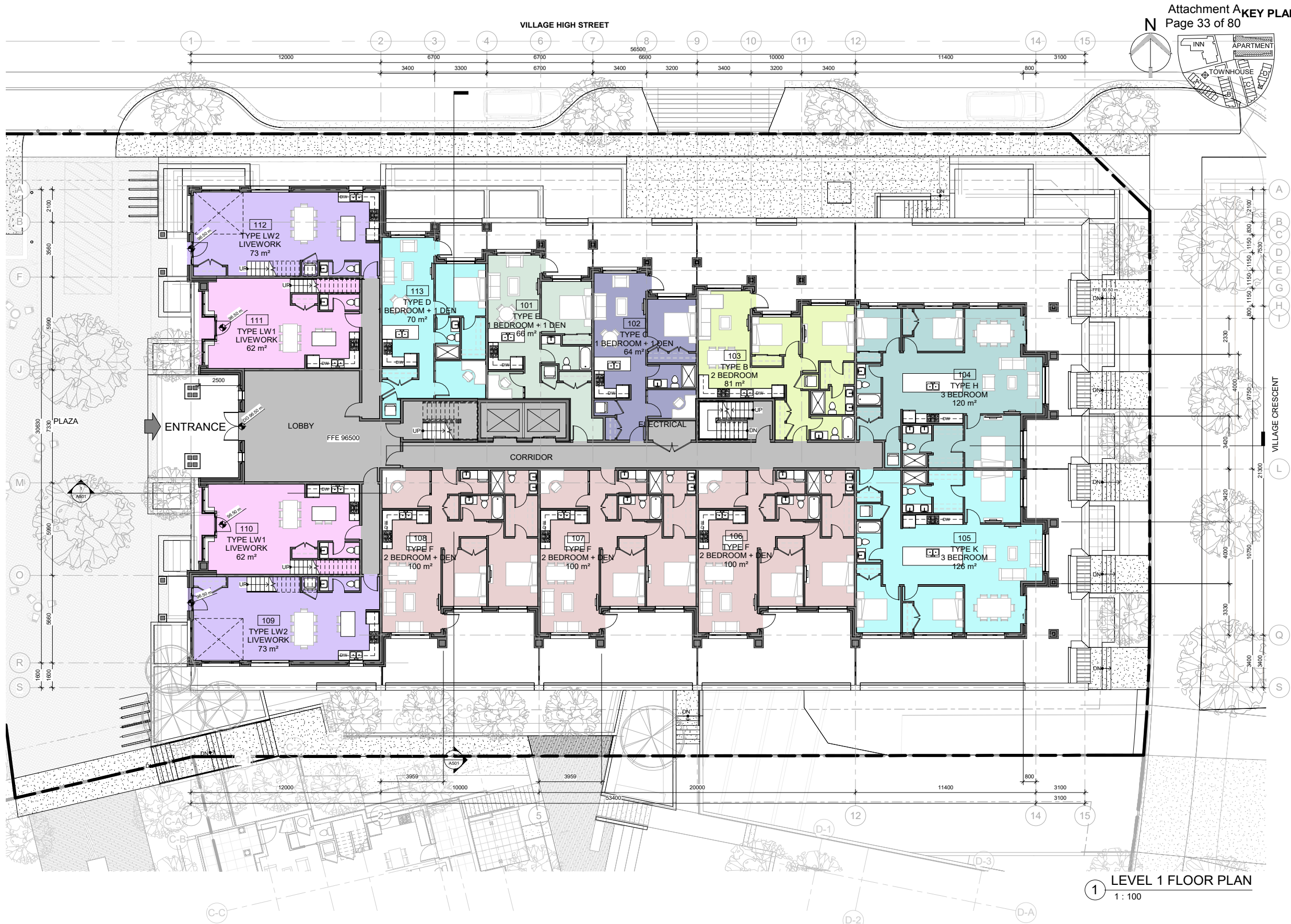






1 LOWER LEVEL B1 FLOOR PLAN  
1 : 100

<div><div>A202</div><div>VILLAGE CENTRE APARTMENT</div><div>GOSPEL ROCK VILLAGE, BLOCK 7, GIBBONS, BC</div><div>PROJECT NUMBER: 1603</div><div>LEVEL B1 FLOOR PLAN</div></div>	<table><tr><th>No.</th><th>Date (Y/M/D)</th><th>Description</th><th>Consultant</th></tr><tr><td>1</td><td>2018/11/23</td><td>ISSUED FOR DP</td><td rowspan="2"><div><div></div><div>JTW ARCHITECTURE INC.</div><div>311-311 Columbia Street, Vancouver, B.C. Canada, V6J 2G5</div><div>Tel: 604 699 3165 Email: jtw@jtwarch.net</div></div></td></tr><tr><td>2</td><td>2019/08/19</td><td>REVISED FOR DP SUBMISSION</td></tr></table>	No.	Date (Y/M/D)	Description	Consultant	1	2018/11/23	ISSUED FOR DP	<div><div></div><div>JTW ARCHITECTURE INC.</div><div>311-311 Columbia Street, Vancouver, B.C. Canada, V6J 2G5</div><div>Tel: 604 699 3165 Email: jtw@jtwarch.net</div></div>	2	2019/08/19	REVISED FOR DP SUBMISSION
		No.	Date (Y/M/D)	Description	Consultant							
		1	2018/11/23	ISSUED FOR DP	<div><div></div><div>JTW ARCHITECTURE INC.</div><div>311-311 Columbia Street, Vancouver, B.C. Canada, V6J 2G5</div><div>Tel: 604 699 3165 Email: jtw@jtwarch.net</div></div>							
2	2019/08/19	REVISED FOR DP SUBMISSION										



Attachment A  
Page 33 of 80

JYW ARCHITECTURE INC.  
211211 Columbia Street, Vancouver, B.C. Canada, V6A 2P6  
Tel: 604 255 2555 Email: jyw@jyw.ca

No. 1 Description: Consultant

1 2019/11/23 ISSUED FOR CD  
2 2019/06/19 REVISED FOR CD SUBMISSION

VILLAGE CENTRE APARTMENT  
GOSPEL ROCK VILLAGE  
12700 GOSPEL ROCK ROAD  
PROJECT NUMBER: 1003

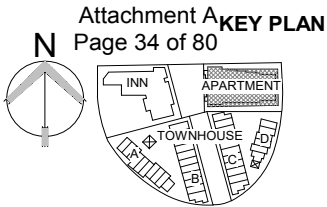
LEVEL 1 FLOOR PLAN

A203





1 LEVEL 2 FLOOR PLAN (MARKET RENTAL)  
1 : 100



JYW ARCHITECTURE INC.  
211011 Columbia Street, Vancouver, B.C. Canada, V6A 2R5  
Tel: 604 680 3160 Email: jyw@jyw.ca

No.	DATE/REV	Description	Consultant
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2	2019/06/19	REVISED FOR CD SUBMISSION	

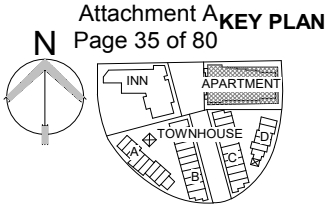
VILLAGE CENTRE APARTMENT  
GOSPEL ROCK VILLAGE  
1000 GOSPEL ROCK ROAD  
VICTORIA, B.C. V8N 1G3  
PROJECT NUMBER: 1003

LEVEL 2 FLOOR PLAN

A204



1 LEVEL 3 FLOOR PLAN  
1 : 100



JYW ARCHITECTURE INC.  
211-211 Columbia Street, Vancouver, B.C. Canada, V6A 2P6  
Tel: 604 680 3168 Email: jyw@jyw.ca

Consultant

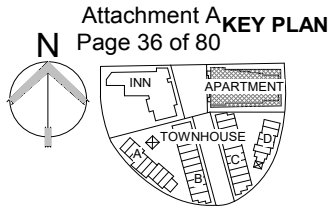
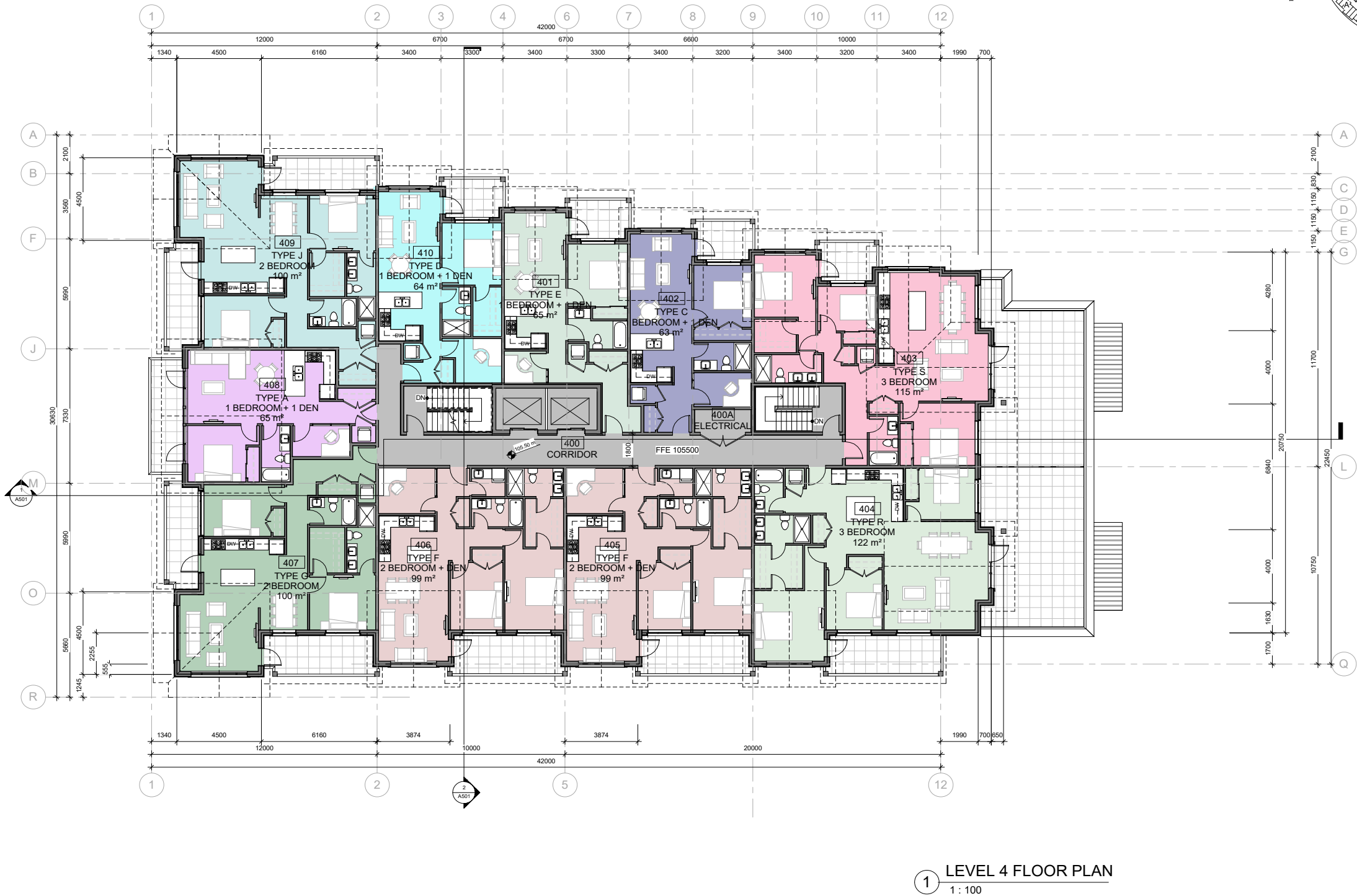
Description:  
ISSUED FOR CD  
REVISED FOR CD SUBMISSION

No. 1  
2

VILLAGE CENTRE APARTMENT  
GOSPEL ROCK VILLAGE  
REDACTED ADDRESS  
PROJECT NUMBER: 1003

LEVEL 3 FLOOR PLAN

A205



JYW ARCHITECTURE INC.  
211211 Columbia Street, Vancouver, B.C. Canada, V6A 2P5  
Tel: 604 278 2111 Email: jyw@jyw.ca

Consultant

Description:  
ISSUED FOR CD  
REVISED FOR CD SUBMISSION

No. 1  
2


VILLAGE CENTRE APARTMENT

GOSPEL ROCK VILLAGE  
BLOCK 1000, PHASE 2  
PROJECT NUMBER: 1003

LEVEL 4 FLOOR PLAN

A206





**JW ARCHITECTURE INC.**  
211-211 Columbia Street, Vancouver, B.C. Canada, V6A 2P6  
Tel: 604 689 3168 Email: jw@jwarch.ca

No.	DATE/NO.	Description	Consultant
1	2019/11/23	ISSUED FOR CD	
2	2019/09/19	REVISED FOR CD SUBMISSION	

**VILLAGE CENTRE APARTMENT**  
GOSPEL ROCK VILLAGE,  
1000 GOSPEL ROCK ROAD,  
VICTORIA, B.C. V8M 1A1  
PROJECT NUMBER: 1003

**ROOF PLAN**

**A207**





JAMES HARDIE -  
LIGHT MIST



JAMES HARDIE -  
TIMBER BARK



MUTUAL MATERIALS -  
REDONDO GRAY



MUTUAL MATERIALS -  
MAUNA LOA



BENJAMIN MOORE -  
CC 420



VILLAGE CENTRE APARTMENT  
GOSPEL ROCK VILLAGE  
1000 GOSPEL ROCK BLVD  
PROJECT NUMBER: 1003

COLOUR & MATERIAL  
INSPIRATION

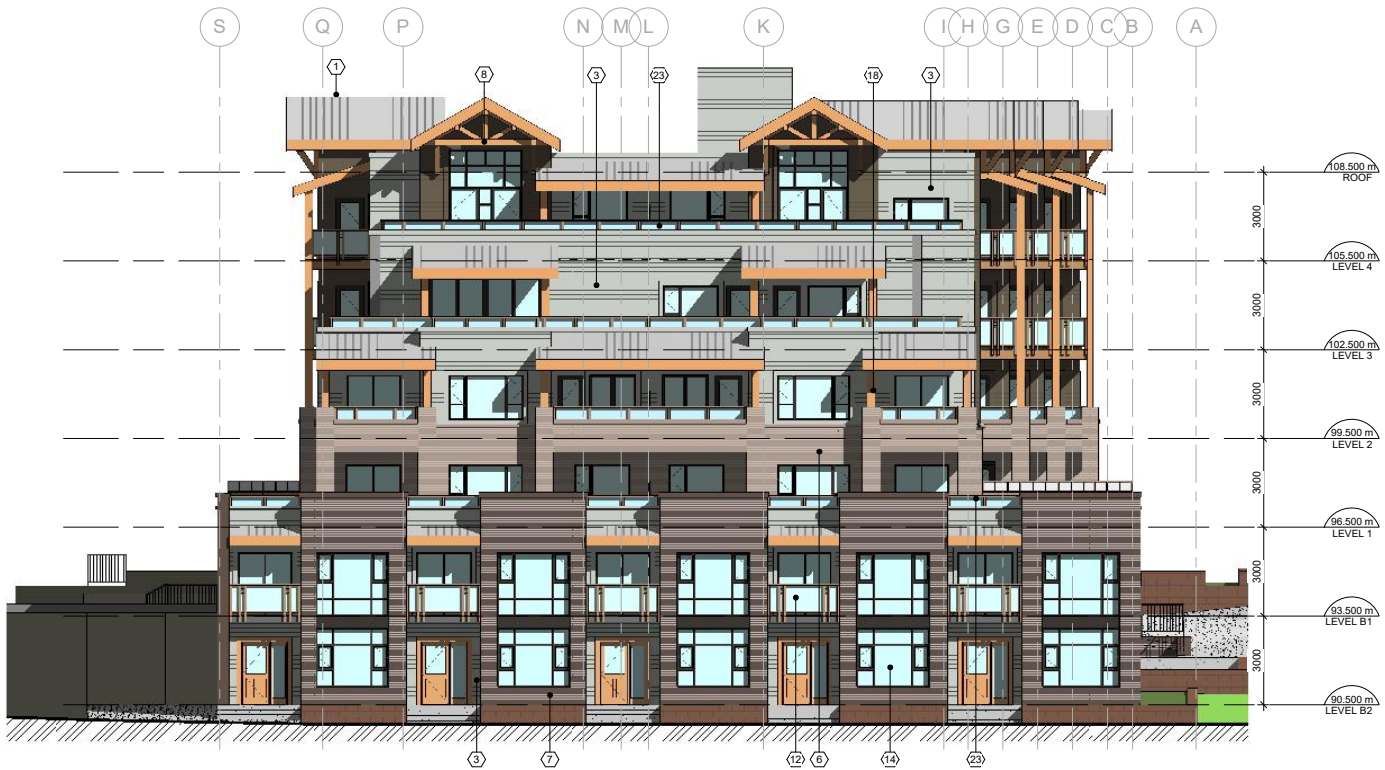
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No.	DATE/REVISION	Description	Consultant
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2	2019/06/19	REVISED FOR CD SUBMISSION	

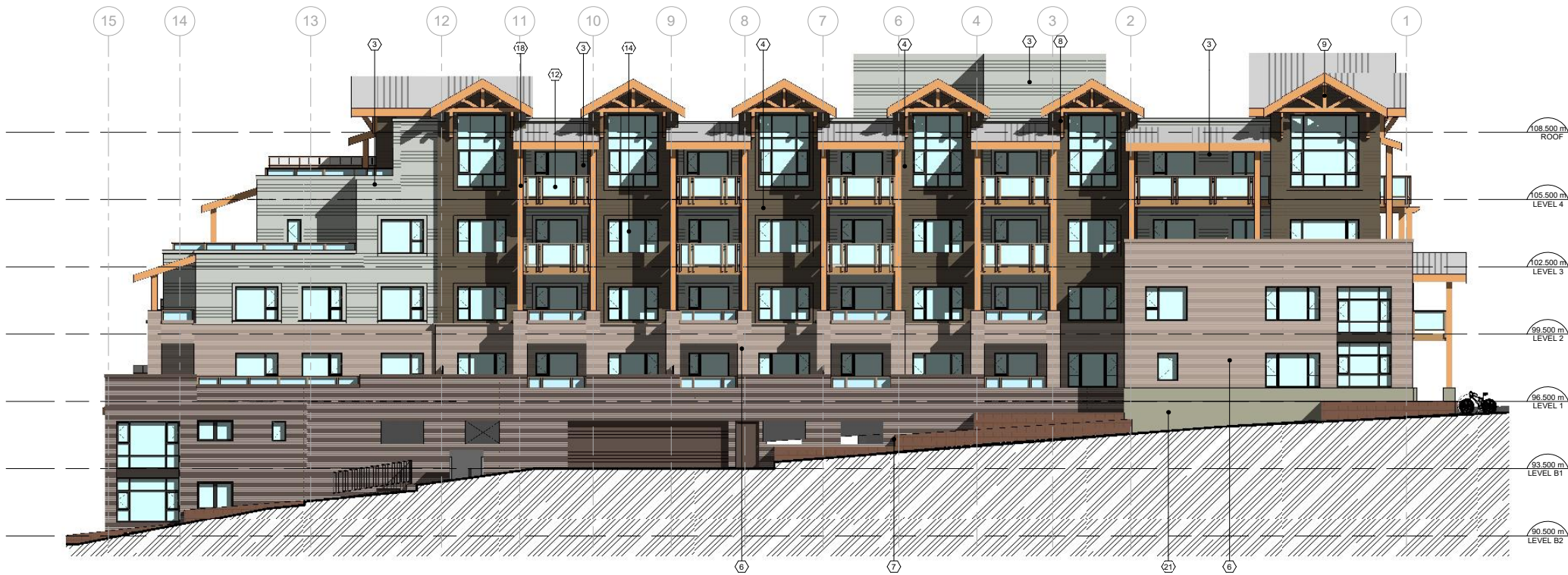


JTW ARCHITECTURE INC.  
211-011 Columbia Street, Vancouver, B.C. Canada, V6A 2P6  
Tel: 604 689 3768 Email: jtw@jtw.ca





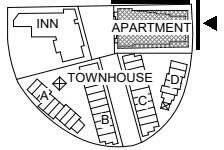
2 EAST ELEVATION  
1: 100



1 NORTH ELEVATION  
1: 100

EXTERIOR FINISHES	
NOTE	DESCRIPTION
1	METAL ROOFING: COLOR TO MATCH CASCADIA METALS- REGENT GREY OR SIMILAR
3	FIBER CEMENT SIDING HORIZONTAL- JAMES HARDI- LIGHT MIST OR SIMILAR
4	FIBER CEMENT SIDING HORIZONTAL- JAMES HARDI- TIMBER BARK OR SIMILAR
6	BRICK VENEER: MUTUAL MATERIALS- REDONDO GRAY OR SIMILAR
7	BRICK VENEER: MUTUAL MATERIALS- MALINA LOA OR SIMILAR
8	DECORATIVE WOOD BRACKET: COLOR TO MATCH OLYMPIC 716 CEDAR NATURALTONE
9	DECORATIVE WOOD TRUSS: COLOR TO MATCH OLYMPIC 716 CEDAR NATURALTONE
12	SAFETY GLASS IN METAL POST GUARDRAIL
14	ALUMINUM FRAME WINDOW W/LOW "E" GLASS
18	WOOD / GLULAM POST: COLOR TO MATCH OLYMPIC 716 CEDAR NATURALTONE
21	CONCRETE WITH ELASTOMERIC COATING FINISH
23	SAFETY GLASS IN METAL POST GUARDRAIL BEHIND PARAPET

Attachment A  
Page 39 of 80



JTW ARCHITECTURE INC.  
211-211 Columbia Street, Vancouver, B.C. Canada, V6A 2P6  
Tel: 604 255 2555 Email: jtw@jtw.ca

Consultant

Description:  
ISSUED FOR CD  
REVISED FOR CD SUBMISSION

No. 20191123  
20190619

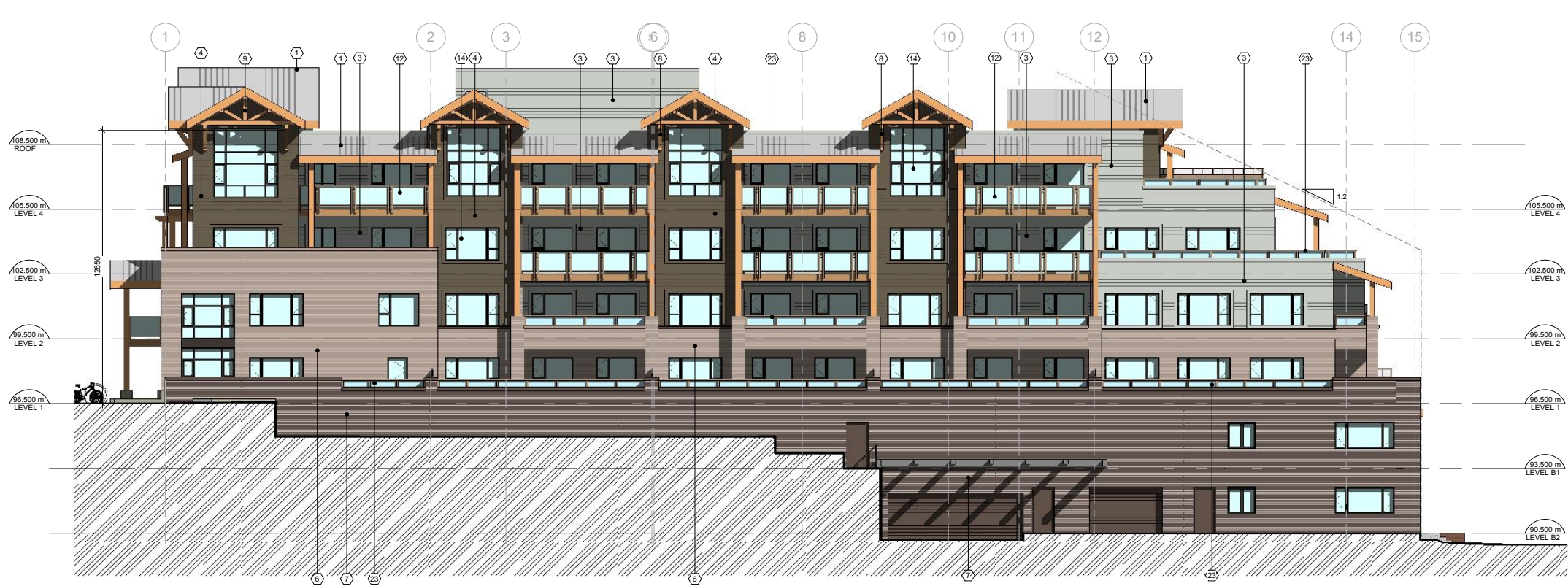
2

VILLAGE CENTRE APARTMENT  
GOSPEL ROCK VILLAGE  
1000 716 CEDAR STREET  
PROJECT NUMBER: 1003

ELEVATIONS

A401





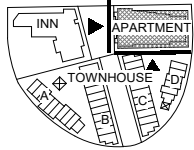
1 SOUTH ELEVATION  
1 : 100



2 WEST ELEVATION  
1 : 100

EXTERIOR FINISHES	
NOTE	DESCRIPTION
1	METAL ROOFING: COLOR TO MATCH CASCADIA METALS- REGENT GREY OR SIMILAR
3	FIBER CEMENT SIDING HORIZONTAL: JAMES HARDI- LIGHT MIST OR SIMILAR
4	FIBER CEMENT SIDING HORIZONTAL: JAMES HARDI- TIMBER BARK OR SIMILAR
6	BRICK VENEER: MUTUAL MATERIALS- REDONDO GRAY OR SIMILAR
7	BRICK VENEER: MUTUAL MATERIALS- MALINA LOA OR SIMILAR
8	DECORATIVE WOOD BRACKET: COLOR TO MATCH OLYMPIC 716 CEDAR NATURALTONE
9	DECORATIVE WOOD TRUSS: COLOR TO MATCH OLYMPIC 716 CEDAR NATURALTONE
12	SAFETY GLASS IN METAL POST GUARDRAIL
14	ALUMINUM FRAME WINDOW W/LOW "E" GLASS
18	WOOD / GLULAM POST: COLOR TO MATCH OLYMPIC 716 CEDAR NATURALTONE
19	DECORATIVE POST: COLOR TO MATCH OLYMPIC 716 CEDAR NATURALTONE
23	SAFETY GLASS IN METAL POST GUARDRAIL BEHIND PARAPET

Attachment A  
Page 40 of 80



JYW ARCHITECTURE INC.  
211211 Columbia Street, Vancouver, B.C. Canada, V6A 2P6  
Tel: 604 689 3168 Email: jyw@jyw.ca

Consultant

Description:  
ISSUED FOR CD  
REVISED FOR CD SUBMISSION

Drawn/MD:  
2019/11/23  
2019/06/19

No.:

1

2

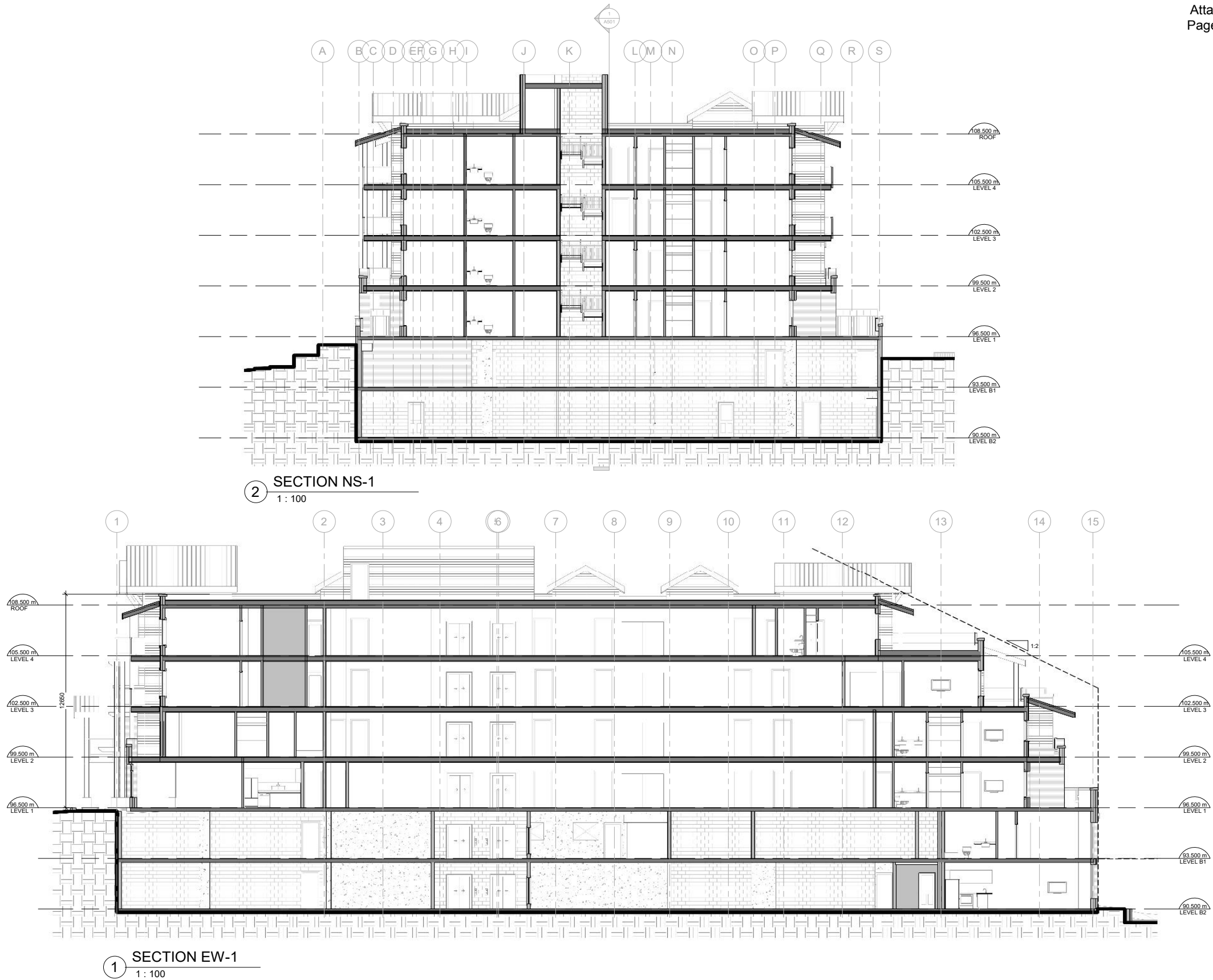
VILLAGE CENTRE APARTMENT


GOSPEL ROCK VILLAGE,  
BLISSVILLE, QUEBEC  
PROJECT NUMBER: 1003

ELEVATIONS

A402







**JYW ARCHITECTURE INC.**  
21121 Columbia Street, Vancouver, B.C. Canada, V6A 2P6  
Tel: 604 266 3168 Email: jyw@jywa.net

No.	DATE/NO.	Description	Consultant
1	20191123	ISSUED FOR CD	
2	20190619	REVISED FOR CD SUBMISSION	

**VILLAGE CENTRE APARTMENT**  
GOSPEL ROCK VILLAGE,  
1000 GOSPEL ROCK ROAD,  
VICTORIA, B.C. V8N 1G3  
PROJECT NUMBER: 1003

**SECTIONS**

**A501**

VILLAGE CENTRE TOWNHOUSE



AREA SUMMARY - BUILDING A				
Department	Level	TYPOLOGY	AREA	
A01	BUILDING A(1-3) LEVEL 01	TYPE 01	62 m²	
A01	BUILDING A(1-3) LEVEL 02	TYPE 01	62 m²	
A01	BUILDING A(1-3) LEVEL 03	TYPE 01	41 m²	
A01			165 m²	
A02	BUILDING A(1-3) LEVEL 01	TYPE 01	60 m²	
A02	BUILDING A(1-3) LEVEL 02	TYPE 01	60 m²	
A02	BUILDING A(1-3) LEVEL 03	TYPE 01	40 m²	
A02			160 m²	
A03	BUILDING A(1-3) LEVEL 01	TYPE 01	60 m²	
A03	BUILDING A(1-3) LEVEL 02	TYPE 01	60 m²	
A03	BUILDING A(1-3) LEVEL 03	TYPE 01	41 m²	
A03			161 m²	
A04	BUILDING A(1-3) LEVEL 01	TYPE 02	60 m²	
A04	BUILDING A(1-3) LEVEL 02	TYPE 02	60 m²	
A04	BUILDING A(1-3) LEVEL 03	TYPE 02	38 m²	
A04			159 m²	
A05	BUILDING A(1-3) LEVEL 01	TYPE 02	60 m²	
A05	BUILDING A(1-3) LEVEL 02	TYPE 02	60 m²	
A05	BUILDING A(1-3) LEVEL 03	TYPE 02	38 m²	
A05			158 m²	
A06	BUILDING A(1-3) LEVEL 01	TYPE 02	60 m²	
A06	BUILDING A(1-3) LEVEL 02	TYPE 02	60 m²	
A06	BUILDING A(1-3) LEVEL 03	TYPE 02	38 m²	
A06			159 m²	
A07	BUILDING A(1-3) LEVEL 01	TYPE 01	60 m²	
A07	BUILDING A(1-3) LEVEL 02	TYPE 01	60 m²	
A07	BUILDING A(1-3) LEVEL 03	TYPE 01	40 m²	
A07			161 m²	
A08	BUILDING A(1-3) LEVEL 01	TYPE 01	60 m²	
A08	BUILDING A(1-3) LEVEL 02	TYPE 01	60 m²	
A08	BUILDING A(1-3) LEVEL 03	TYPE 01	40 m²	
A08			160 m²	
A09	BUILDING A(1-3) LEVEL 01	TYPE 01	60 m²	
A09	BUILDING A(1-3) LEVEL 02	TYPE 01	60 m²	
A09	BUILDING A(1-3) LEVEL 03	TYPE 01	41 m²	
A09			162 m²	
A10	BUILDING A(1-3) LEVEL 01	TYPE 03	59 m²	
A10	BUILDING A(1-3) LEVEL 02	TYPE 03	62 m²	
A10			120 m²	
PARKING	BUILDING A(1-3) U/G PARKING			1,126 m²
PARKING				1,126 m²
TOTAL AREA:				2,691 m²

AREA SUMMARY - BUILDING B				
Department	Level	TYPOLOGY	AREA	
B01	LEVEL 01	TYPE 07	65 m²	
B01	LEVEL 02	TYPE 07	58 m²	
B01	LEVEL 03	TYPE 07	7 m²	
B01			130 m²	
B02	LEVEL 01	TYPE 04	60 m²	
B02	LEVEL 02	TYPE 04	60 m²	
B02	LEVEL 03	TYPE 04	41 m²	
B02			161 m²	
B03	LEVEL 01	TYPE 04	60 m²	
B03	LEVEL 02	TYPE 04	60 m²	
B03	LEVEL 03	TYPE 04	41 m²	
B03			161 m²	
B04	LEVEL 01	TYPE 05	67 m²	
B04	LEVEL 02	TYPE 05	70 m²	
B04	LEVEL 03	TYPE 05	14 m²	
B04			152 m²	
B05	LEVEL 01	TYPE 05	67 m²	
B05	LEVEL 02	TYPE 05	70 m²	
B05	LEVEL 03	TYPE 05	14 m²	
B05			152 m²	
B06	LEVEL 01	TYPE 04	60 m²	
B06	LEVEL 02	TYPE 04	60 m²	
B06	LEVEL 03	TYPE 04	41 m²	
B06			161 m²	
B07	LEVEL 01	TYPE 04	60 m²	
B07	LEVEL 02	TYPE 04	60 m²	
B07	LEVEL 03	TYPE 04	41 m²	
B07			161 m²	
B08	LEVEL 01	TYPE 05	67 m²	
B08	LEVEL 02	TYPE 05	70 m²	
B08	LEVEL 03	TYPE 05	14 m²	
B08			152 m²	
B09	LEVEL 01	TYPE 05	67 m²	
B09	LEVEL 02	TYPE 05	70 m²	
B09	LEVEL 03	TYPE 05	14 m²	
B09			152 m²	
B10	LEVEL 01	TYPE 04	60 m²	
B10	LEVEL 02	TYPE 04	60 m²	
B10	LEVEL 03	TYPE 04	41 m²	
B10			161 m²	
B11	LEVEL 01	TYPE 06	68 m²	
B11	LEVEL 02	TYPE 06	68 m²	
B11	LEVEL 03	TYPE 06	43 m²	
B11			179 m²	
B12	LEVEL B1	TYPE 11	105 m²	
B12			76 m²	
B13	LEVEL B1	TYPE 09	103 m²	
B13			103 m²	
B14	LEVEL B1	TYPE 09	103 m²	
B14			103 m²	
B15	LEVEL B1	TYPE 09	103 m²	
B15			103 m²	
B16	LEVEL B1	TYPE 10	104 m²	
B16			104 m²	
PARKING	LEVEL B2			1,799 m²
PARKING				39 m²
TOTAL AREA:				4,040 m²

AREA SUMMARY - BUILDING C				
Department	Level	TYPOLOGY	AREA	
C01	LEVEL 01	TYPE 08	50 m²	
C01	LEVEL 02	TYPE 08	50 m²	
C01	LEVEL 03	TYPE 08	11 m²	
C01			111 m²	
C02	LEVEL 01	TYPE 04	60 m²	
C02	LEVEL 02	TYPE 04	60 m²	
C02	LEVEL 03	TYPE 04	41 m²	
C02			161 m²	
C03	LEVEL 01	TYPE 04	60 m²	
C03	LEVEL 02	TYPE 04	60 m²	
C03	LEVEL 03	TYPE 04	41 m²	
C03			161 m²	
C04	LEVEL 01	TYPE 05	68 m²	
C04	LEVEL 02	TYPE 05	70 m²	
C04	LEVEL 03	TYPE 05	14 m²	
C04			152 m²	
C05	LEVEL 01	TYPE 05	67 m²	
C05	LEVEL 02	TYPE 05	70 m²	
C05	LEVEL 03	TYPE 05	14 m²	
C05			152 m²	
C06	LEVEL 01	TYPE 04	60 m²	
C06	LEVEL 02	TYPE 04	60 m²	
C06	LEVEL 03	TYPE 04	41 m²	
C06			161 m²	
C07	LEVEL 01	TYPE 04	60 m²	
C07	LEVEL 02	TYPE 04	60 m²	
C07	LEVEL 03	TYPE 04	41 m²	
C07			161 m²	
C08	LEVEL 01	TYPE 05	68 m²	
C08	LEVEL 02	TYPE 05	70 m²	
C08	LEVEL 03	TYPE 05	14 m²	
C08			152 m²	
C09	LEVEL 01	TYPE 05	67 m²	
C09	LEVEL 02	TYPE 05	70 m²	
C09	LEVEL 03	TYPE 05	14 m²	
C09			152 m²	
C10	LEVEL 01	TYPE 04	60 m²	
C10	LEVEL 02	TYPE 04	60 m²	
C10	LEVEL 03	TYPE 04	41 m²	
C10			161 m²	
C11	LEVEL 01	TYPE 06	68 m²	
C11	LEVEL 02	TYPE 06	68 m²	
C11	LEVEL 03	TYPE 06	43 m²	
C11			179 m²	
C12	LEVEL B1	TYPE 12	76 m²	
C12			76 m²	
C13	LEVEL B1	TYPE 09	103 m²	
C13			103 m²	
C14	LEVEL B1	TYPE 09	103 m²	
C14			103 m²	
C15	LEVEL B1	TYPE 09	103 m²	
C15			103 m²	
C16	LEVEL B1	TYPE 10	104 m²	
C16			104 m²	
PARKING	LEVEL B2			39 m²
PARKING				39 m²
TOTAL AREA:				2,232 m²

AREA SUMMARY - BUILDING D				
Department	Level	TYPOLOGY	AREA	
D01	BUILDING D 5-6 LEVEL 01	TYPE 13	68 m²	
D01	BUILDING D 5-6 LEVEL 02	TYPE 13	68 m²	
D01	BUILDING D 5-6 LEVEL 03	TYPE 13	7 m²	
D01			143 m²	
D02	BUILDING D 5-6 LEVEL 01	TYPE 14	66 m²	
D02	BUILDING D 5-6 LEVEL 02	TYPE 14	66 m²	
D02	BUILDING D 5-6 LEVEL 03	TYPE 14	7 m²	
D02			139 m²	
D03	BUILDING D 5-6 LEVEL 01	TYPE 13	66 m²	
D03	BUILDING D 5-6 LEVEL 02	TYPE 13	66 m²	
D03	BUILDING D 5-6 LEVEL 03	TYPE 13	7 m²	
D03			140 m²	
D04	BUILDING D 5-6 LEVEL 01	TYPE 14	66 m²	
D04	BUILDING D 5-6 LEVEL 02	TYPE 14	66 m²	
D04	BUILDING D 5-6 LEVEL 03	TYPE 14	7 m²	
D04			139 m²	
D05	BUILDING D 5-6 LEVEL 01	TYPE 15	70 m²	
D05	BUILDING D 5-6 LEVEL 02	TYPE 15	70 m²	
D05	BUILDING D 5-6 LEVEL 03	TYPE 15	7 m²	
D05			146 m²	
D06	BUILDING D 5-6 LEVEL 01	TYPE 16	74 m²	
D06	BUILDING D 5-6 LEVEL 02	TYPE 16	75 m²	
D06	BUILDING D 5-6 LEVEL 03	TYPE 16	7 m²	
D06			156 m²	
PARKING	BUILDING D 1-2 U/G PARKING			667 m²
PARKING				667 m²
TOTAL AREA:				1,530 m²

AREA SUMMARY - OVERALL		
BUILDING	Level	AREA
A	BUILDING A(1-3) LEVEL 01	602 m²
A	BUILDING A(1-3) LEVEL 02	605 m²
A	BUILDING A(1-3) LEVEL 03	359 m²
A		1,566 m²
A - BASEMENT	BUILDING A(1-3) U/G PARKING	1,126 m²
A - BASEMENT		1,126 m²
B	LEVEL 01	702 m²
B	LEVEL 02	706 m²
B	LEVEL 03	315 m²
B	LEVEL B1	517 m²
B		2,241 m²
B - BASEMENT	LEVEL B2	1,799 m²
B - BASEMENT		1,799 m²
C	LEVEL 01	687 m²
C	LEVEL 02	699 m²
C	LEVEL 03	319 m²
C	LEVEL B1	488 m²
C		2,194 m²
C - BASEMENT	LEVEL B2	39 m²
C - BASEMENT		39 m²
D	BUILDING D 5-6 LEVEL 01	410 m²
D	BUILDING D 5-6 LEVEL 02	411 m²
D	BUILDING D 5-6 LEVEL 03	41 m²
D		863 m²
D - BASEMENT	BUILDING D 1-2 U/G PARKING	667 m²
D - BASEMENT		667 m²
PAVILION E	BUILDING A(7-9) LEVEL 01	31 m²
PAVILION E		31 m²
PAVILION F	BUILDING D 5-6 LEVEL 01	29 m²
PAVILION F		29 m²
TOTAL AREA:		10,554 m²


PROJECT INFORMATION	
PROJECT NAME:	VILLAGE CENTRE TOWNHOUSE
PROJECT ADDRESS:	BLOCK 7, GIBSONS, B.C. CANADA
LEGAL DESCRIPTION:	LOT 13 BLOCK 7 DISTRICT LOT 842 GROUP 1 NEW WESTMINSTER DISTRICT PLAN EPF90953
ZONING:	CD4-4, SUBAREA 2
GROSS SITE AREA:	80,843 SF
SITE COVERAGE:	78%
BUILDING HEIGHT:	MAX. 11.5m
SETBACK:	
NORTH SETBACK	3.0m
SOUTH SETBACK	3.0m
EAST SETBACK	3.0m
WEST SETBACK	3.0m

ROOM TYPE SUMMARY			
BUILDING	ROOM TYPE	SUMMARY	UNIT
A	3 BEDROOM	TOWNHOUSE	1
A	3 BEDROOM + 1 DEN	TOWNHOUSE	9
A			10
B	2 BEDROOM	GARDEN SUITE	5
B	2 BEDROOM W/ROOF DECK	TOWNHOUSE	1
B	3 BEDROOM + 1 DEN	TOWNHOUSE	5
B	3 BEDROOM W/ROOF DECK	TOWNHOUSE	4
B	4 BEDROOM	TOWNHOUSE	1
B			16
C	1 BEDROOM	GARDEN SUITE	1
C	2 BEDROOM	GARDEN SUITE	4
C	2 BEDROOM W/ROOF DECK	TOWNHOUSE	1
C	3 BEDROOM + 1 DEN	TOWNHOUSE	6
C	3 BEDROOM W/ROOF DECK	TOWNHOUSE	4
C			16
D	3 BEDROOM	TOWNHOUSE	6
D			6
TOTAL UNITS:			48

PARKING STATISTICS	
ZONING REQUIREMENT:	2 STALLS PER RESIDENTIAL UNIT
	TOTAL: 96
TOTAL PARKING PROVIDED	96
VISITOR PARKING RATIO	
ZONING REQUIREMENT:	MIN. 15%
VISITOR PARKING	15
	16%
SMALL CAR RATIO	
ZONING REQUIREMENT:	MAX. 30%
SMALL CAR	20
	21%
LEVEL COUNT	
BUILDING A(1-3) U/G PARKING	3
BUILDING A(4-6) U/G PARKING	11
BUILDING A(7-9) U/G PARKING	15
BUILDING D 1-2 U/G PARKING	8
LEVEL B2	59
TOTAL	96

BICYCLE PARKING STATISTICS		
ZONING REQUIREMENT:	CLASS 1 1.25 SPACES PER DWELLING UNIT	60
	CLASS 2 0.2 SPACES PER DWELLING UNIT	10
TOTAL PARKING PROVIDED	CLASS 1	65
	CLASS 2	10
* CLASS 2 BICYCLE PARKING SHOWN ON LANDSCAPE DRAWING		
TYPE COUNT		
CLASS 1		56
CLASS 1 (V)		9
CLASS 2		10
TOTAL		75

DRAWING LIST	
A100	PROJECT INFORMATION
A101	SITE PLAN
A102	LEVEL B2 FLOOR PLAN
A221	TOWNHOUSE BUILDING A FLOOR PLANS
A222	TOWNHOUSE BUILDING A FLOOR PLANS
A241	TOWNHOUSE BUILDING B FLOOR PLANS
A242	TOWNHOUSE BUILDING B FLOOR PLANS
A243	TOWNHOUSE BUILDING B FLOOR PLANS
A261	TOWNHOUSE BUILDING C FLOOR PLANS
A262	TOWNHOUSE BUILDING C FLOOR PLANS
A263	TOWNHOUSE BUILDING C FLOOR PLANS
A281	TOWNHOUSE BUILDING D FLOOR PLANS
A282	TOWNHOUSE BUILDING D FLOOR PLANS
A300	COLOUR & MATERIAL INSPIRATION
A321	TOWNHOUSE BUILDING A AND PAVILION E ELEVATIONS
A322	TOWNHOUSE BUILDING A ELEVATIONS
A341	TOWNHOUSE BUILDING B ELEVATIONS
A342	TOWNHOUSE BUILDING B ELEVATIONS
A361	TOWNHOUSE BUILDING C ELEVATIONS
A362	TOWNHOUSE BUILDING C ELEVATIONS
A381	TOWNHOUSE BUILDING D ELEVATIONS
A421	TOWNHOUSE BUILDING A SECTIONS
A441	TOWNHOUSE BUILDING B SECTIONS
A442	TOWNHOUSE BUILDING B SECTIONS
A461	TOWNHOUSE BUILDING C SECTIONS
A462	TOWNHOUSE BUILDING C SECTIONS
A481	TOWNHOUSE BUILDING D SECTIONS



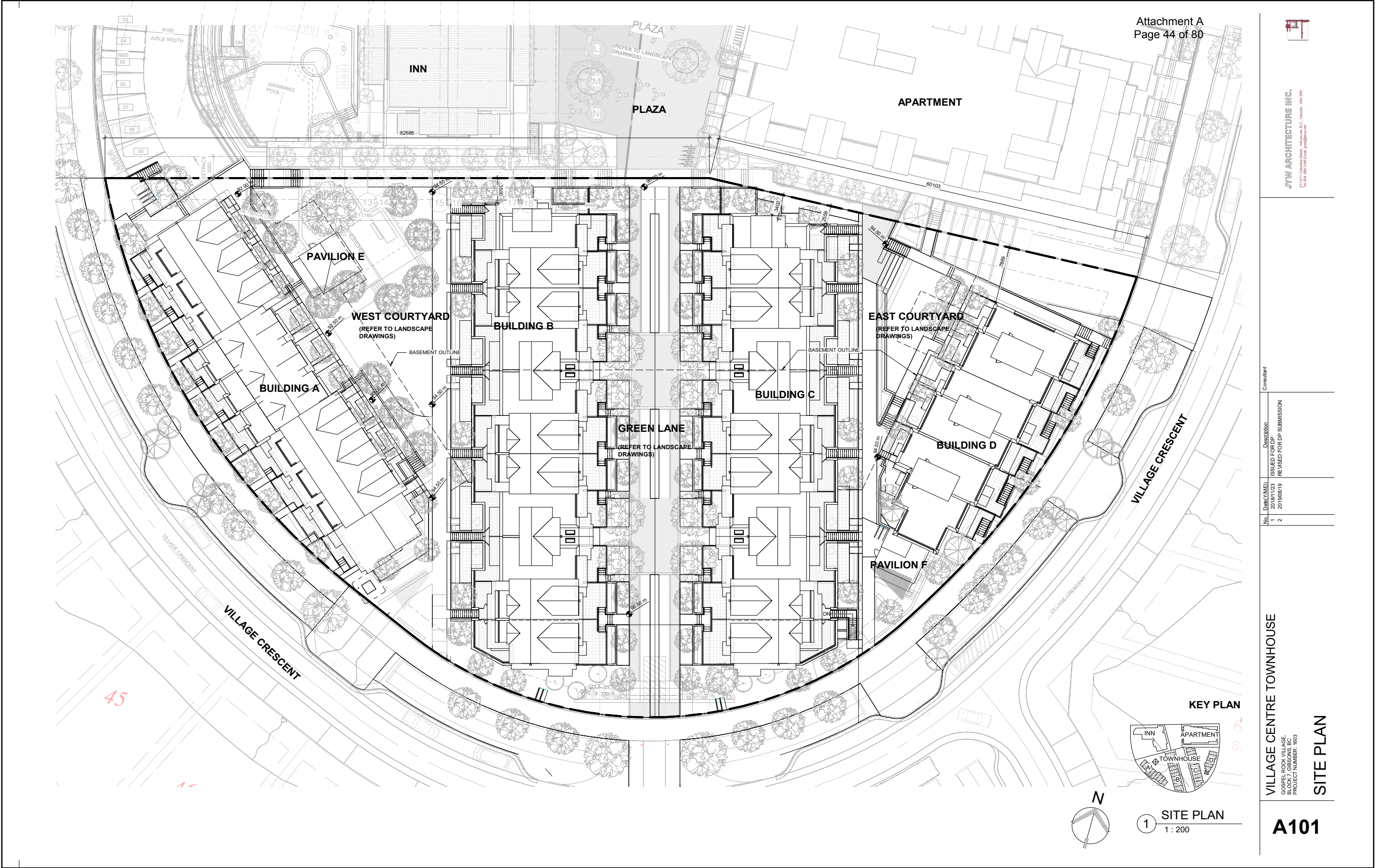
JYW ARCHITECTURE INC.  
211211 Columbia Street, Vancouver, B.C. Canada, V6A 2R5  
Tel: 604 680 2588 Email: jyw@jyw.ca

VILLAGE CENTRE TOWNHOUSE  
GOSPEL ROCK VILLAGE,  
BLOCK 7, GIBSONS, B.C.  
PROJECT NUMBER: 1003

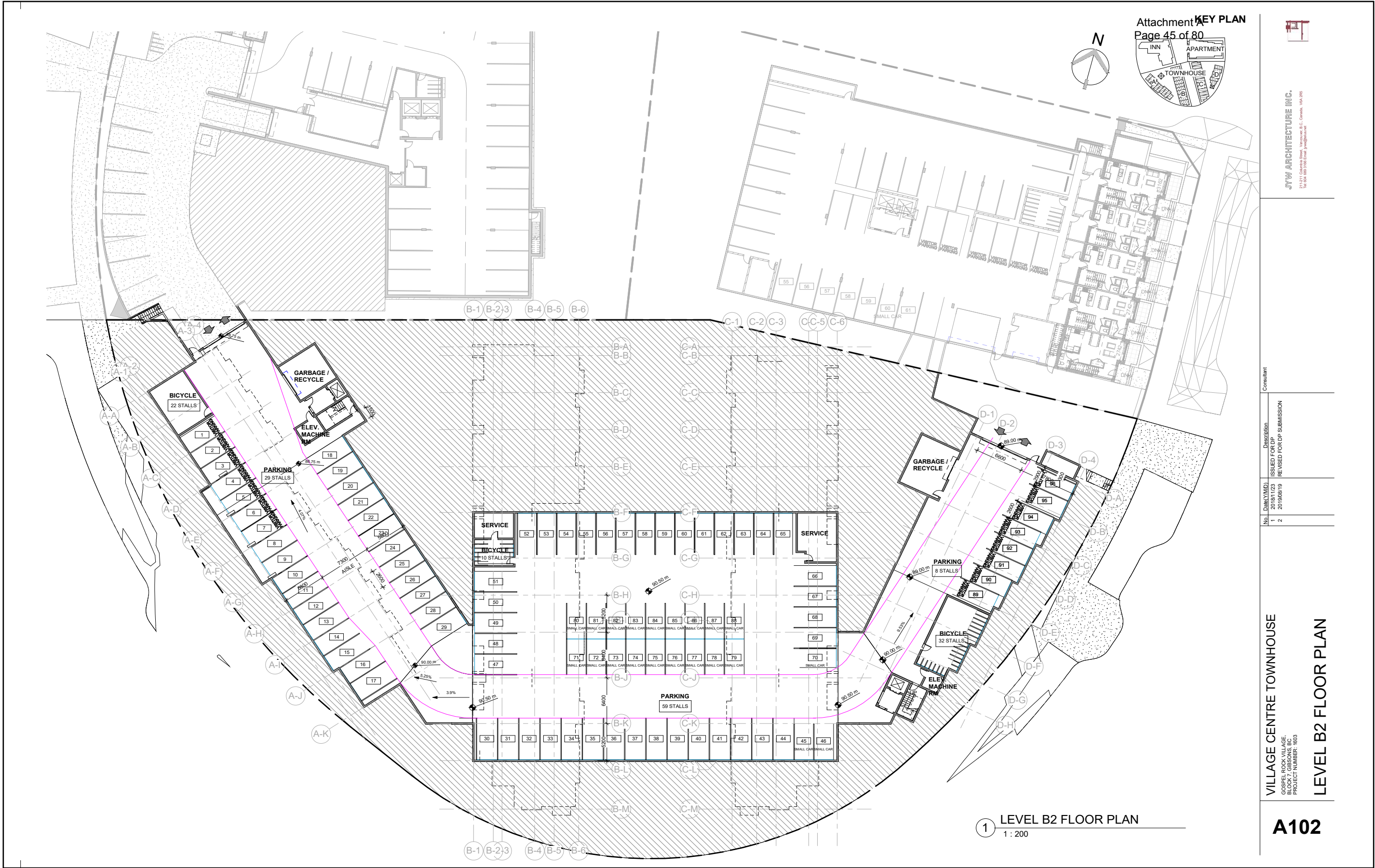
PROJECT INFORMATION

A100

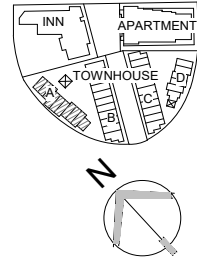
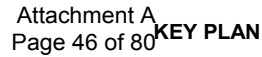








1 LEVEL B2 FLOOR PLAN  
1 : 200

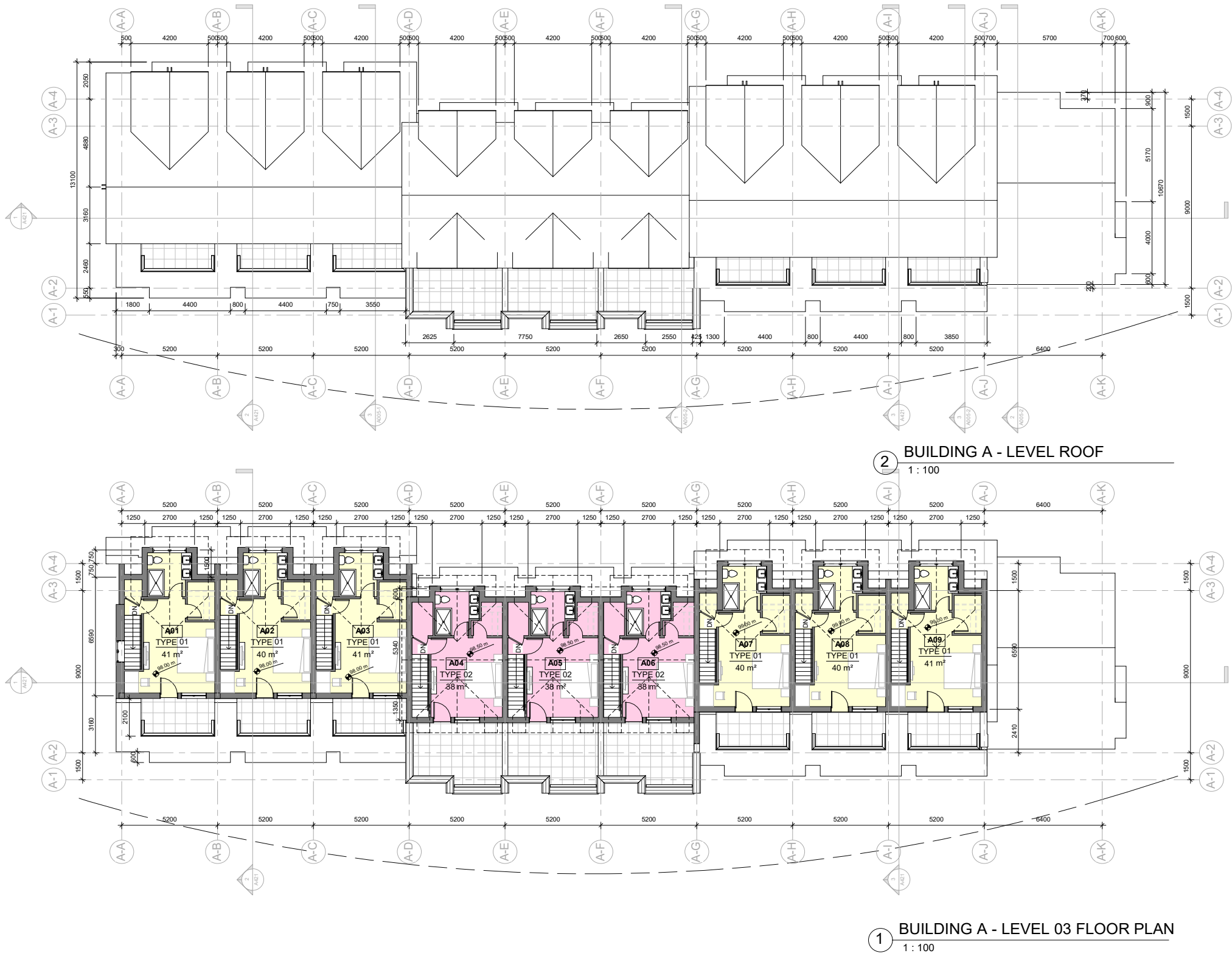


**JYW ARCHITECTURE INC.**  
211-211 Columbia Street, Vancouver, B.C., Canada, V6A 2R5  
Tel: 604 689 3166 Email: [jyw@telus.net](mailto:jyw@telus.net)

No.	Date(Y/M/D)	Description	Consultant
1	2018/11/23	ISSUED FOR DP	
2	2019/06/19	REVISED FOR DP SUBMISSION	

**VILLAGE CENTRE TOWNHOUSE**  
GOSPEL ROCK VILLAGE,  
1600 GOSPEL ROCK BLVD.,  
PROJECT NUMBER: 1603

**A221**

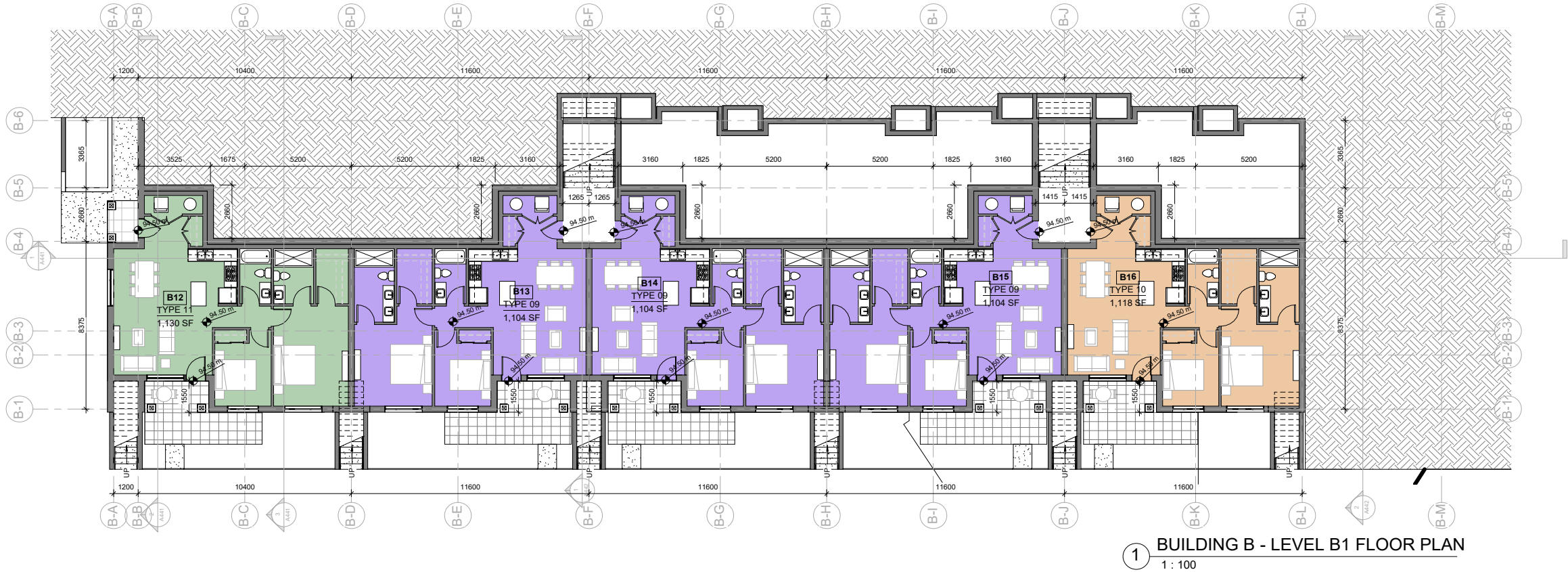


Attachment A  
Page 47 of 80

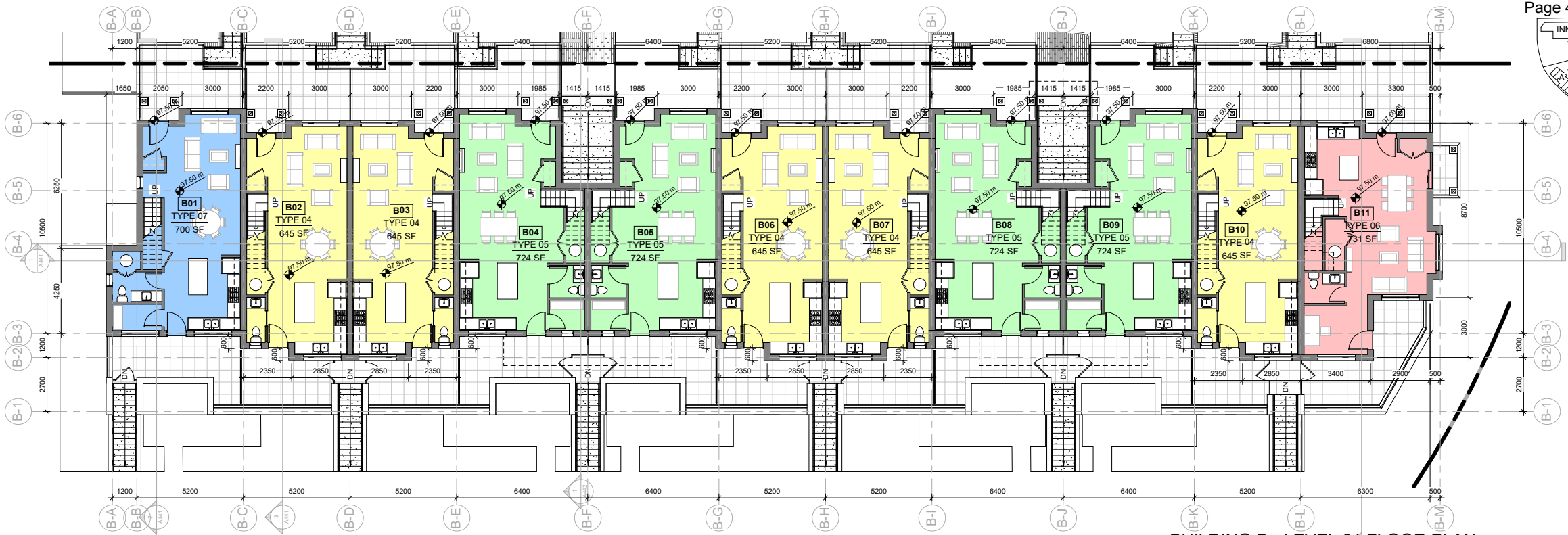


**JYW ARCHITECTURE INC.**  
271-211 Columbia Street, Vancouver, B.C. Canada, V6A 2R5  
Tel: 604 588 5168 Email: jyw@jyw.ca



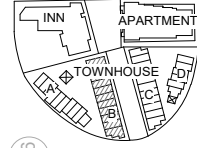


1 BUILDING B - LEVEL B1 FLOOR PLAN  
1 : 100



2 BUILDING B - LEVEL 01 FLOOR PLAN  
1 : 100

Attachment A KEY PLAN  
Page 48 of 80



VILLAGE CENTRE TOWNHOUSE  
GOSPEL ROCK VILLAGE,  
BROCKTON, MASSACHUSETTS  
PROJECT NUMBER: 1003

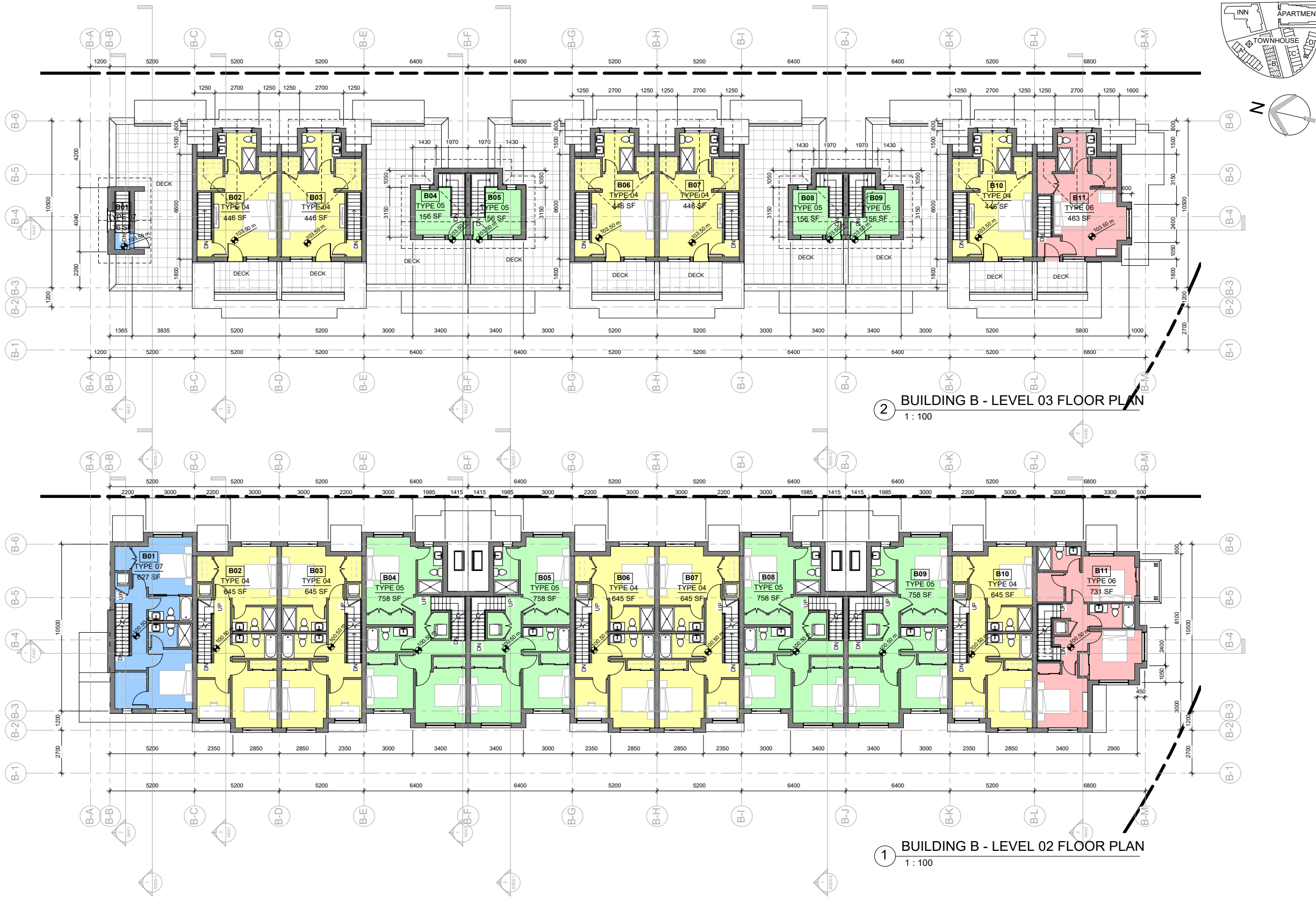
TOWNHOUSE BUILDING B  
FLOOR PLANS

A241

No.	Date/Rev	Description	Consultant
1	2019/11/23	ISSUED FOR CD	
2	2019/06/19	REVISED FOR CD SUBMISSION	

JTW ARCHITECTURE INC.  
211211 Columbia Street, Vancouver, B.C. Canada, V6A 2R5  
Tel: 604 680 3368 Email: jtw@jtwarch.ca





Attachment A  
Page 49 of 80



JW ARCHITECTURE INC.  
211211 Columbia Street, Vancouver, B.C. Canada, V6A 2B5  
Tel: 604 683 5165 Email: jw@jwarch.ca

Consultant

Description:  
ISSUED FOR CIP  
REVISED FOR CIP SUBMISSION

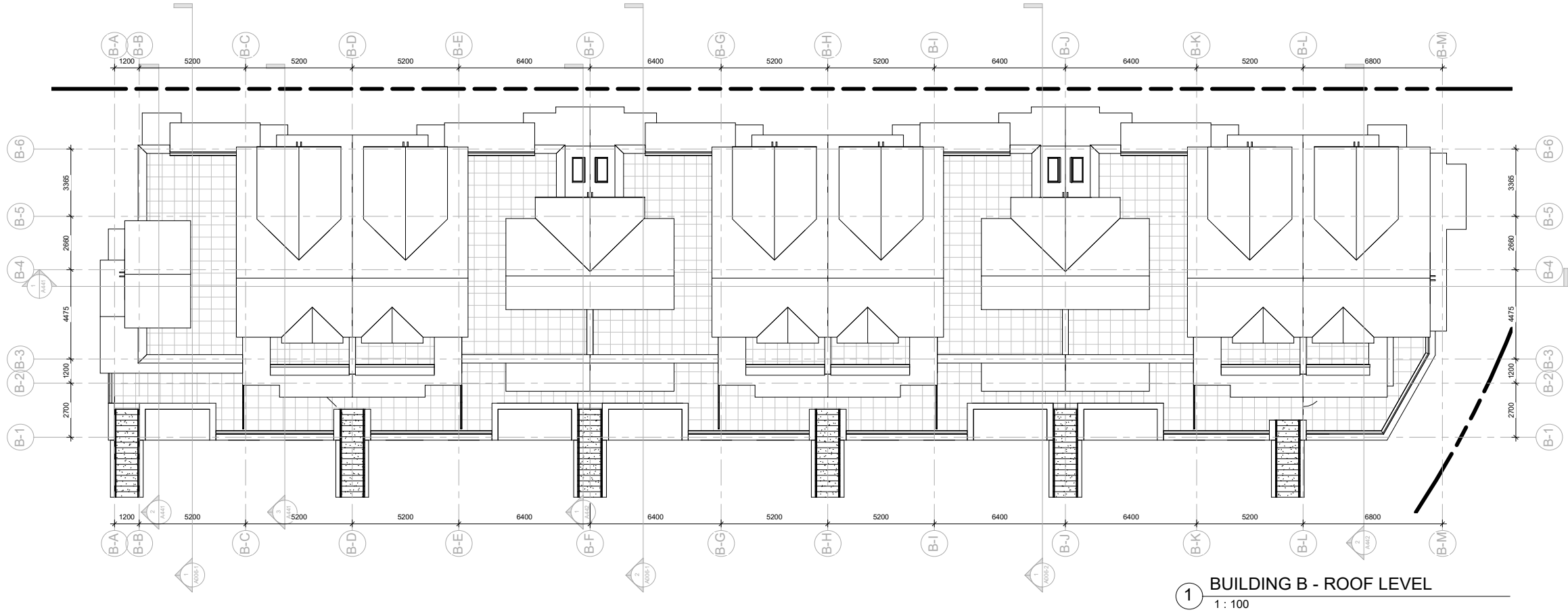
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VILLAGE CENTRE TOWNHOUSE

GOSPEL ROCK VILLAGE  
BLOCK 10, PHASE 1  
PROJECT NUMBER: 1003

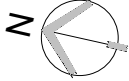
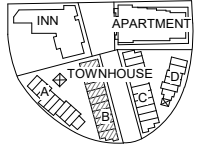
TOWNHOUSE BUILDING B  
FLOOR PLANS

A242



1 BUILDING B - ROOF LEVEL  
1 : 100

Attachment A  
Page 50 of 80



**JYW ARCHITECTURE INC.**  
211211 Columbia Street, Vancouver, B.C. Canada, V6A 2P5  
Tel: 604 680 3168 Email: jyw@jywanet.net

Consultant

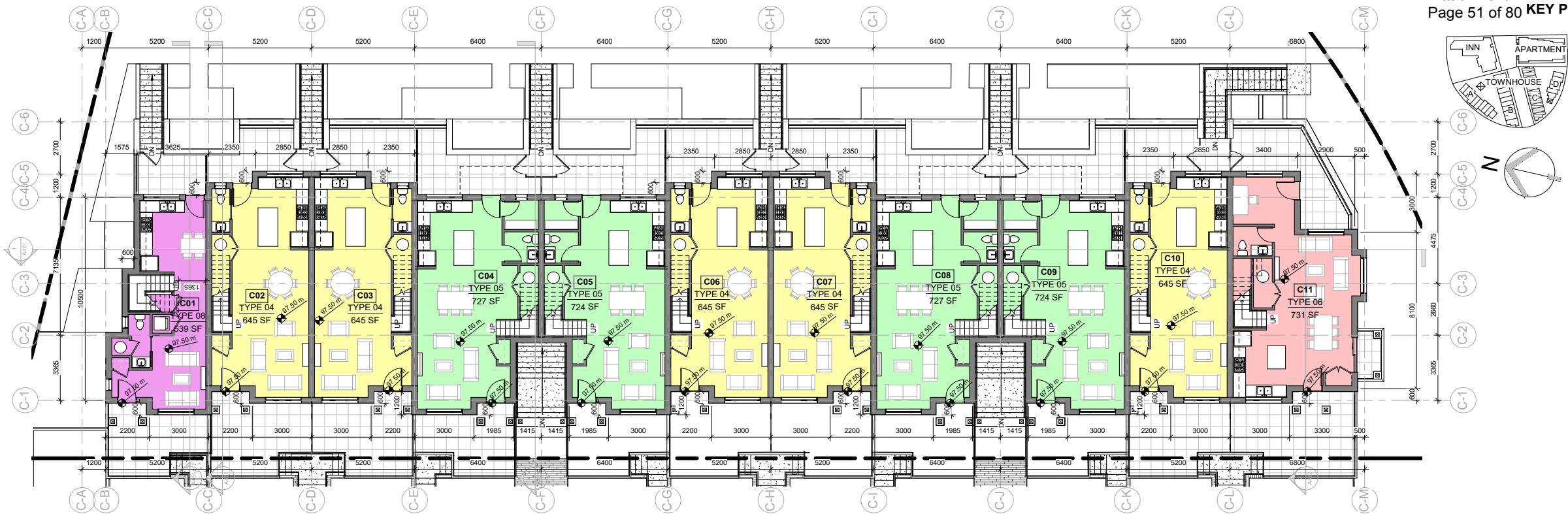
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20190619 REVISED FOR CP SUBMISSION

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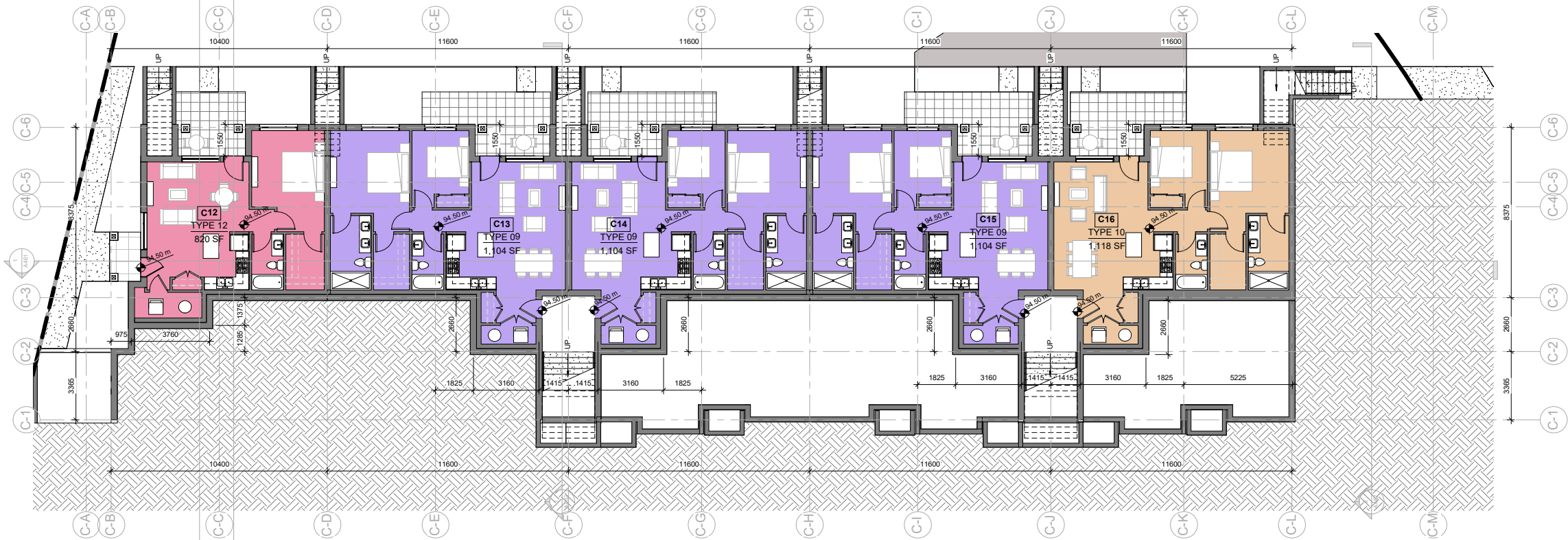
GOSPEL ROCK VILLAGE,  
1000 GOSPEL ROCK ROAD,  
VICTORIA, BC V8N 1G3  
PROJECT NUMBER: 1003

TOWNHOUSE BUILDING B  
FLOOR PLANS

**A243**

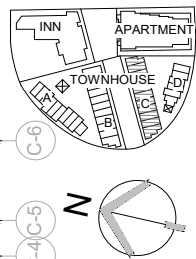


2 BUILDING C - LEVEL 01 FLOOR PLAN  
1 : 100



1 BUILDING C - LEVEL B1 FLOOR PLAN  
1 : 100

Attachment A  
Page 51 of 80 KEY PLAN



JVW ARCHITECTURE INC.  
211-211 Columbia Street, Vancouver, B.C. Canada, V6A 2P6  
Tel: 604 255-1188 Email: jvw@jvw.ca

Consultant

Description

No. 1

2

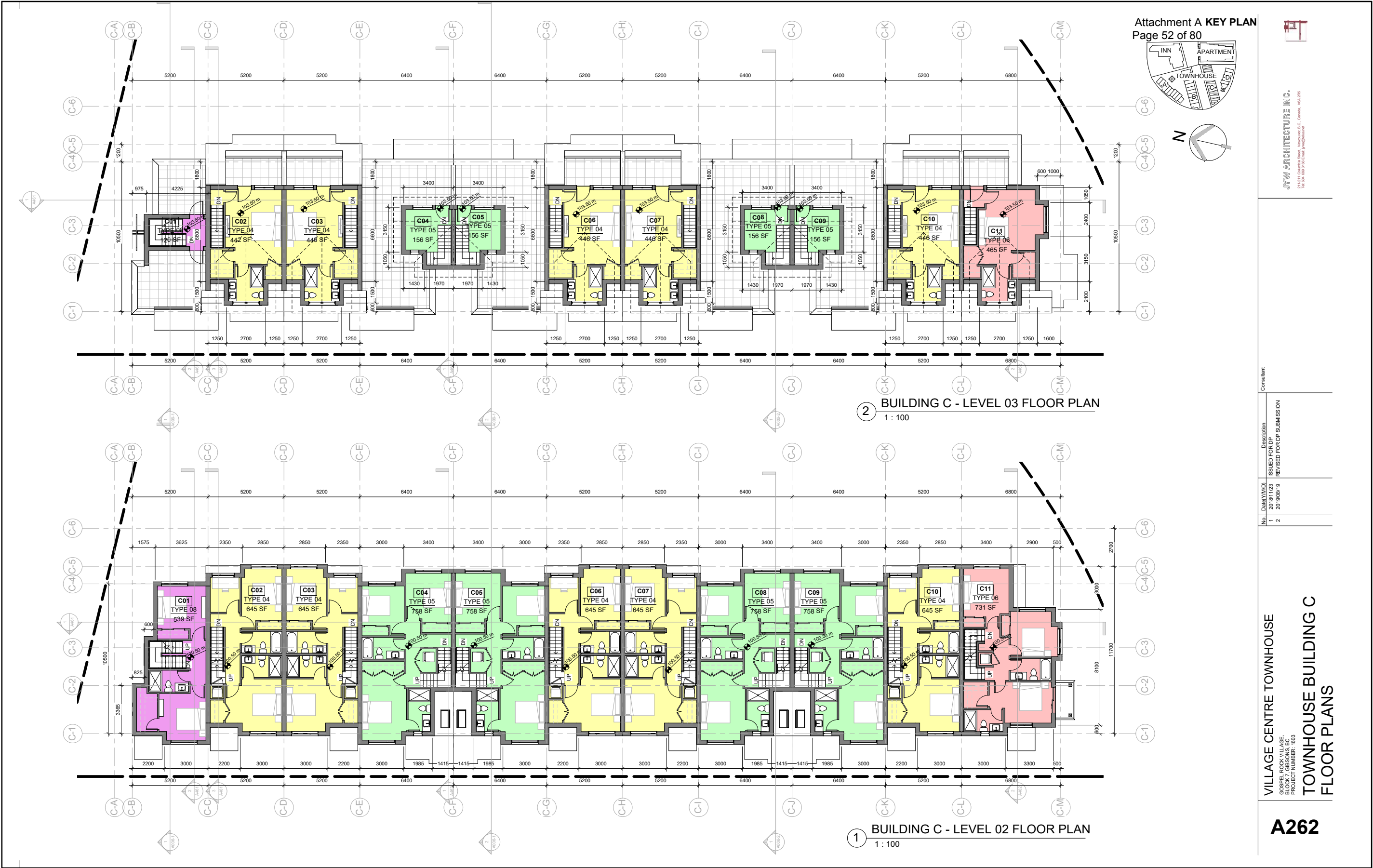
ISSUED FOR CD  
REVISED FOR CD SUBMISSION

VILLAGE CENTRE TOWNHOUSE

GOSPEL ROCK VILLAGE  
BLOCK 10, PHASE 2  
PROJECT NUMBER: 1003

TOWNHOUSE BUILDING C  
FLOOR PLANS

A261



VILLAGE CENTRE TOWNHOUSE  
GOSPEL ROCK VILLAGE  
BLOCK 1000  
PROJECT NUMBER: 1003

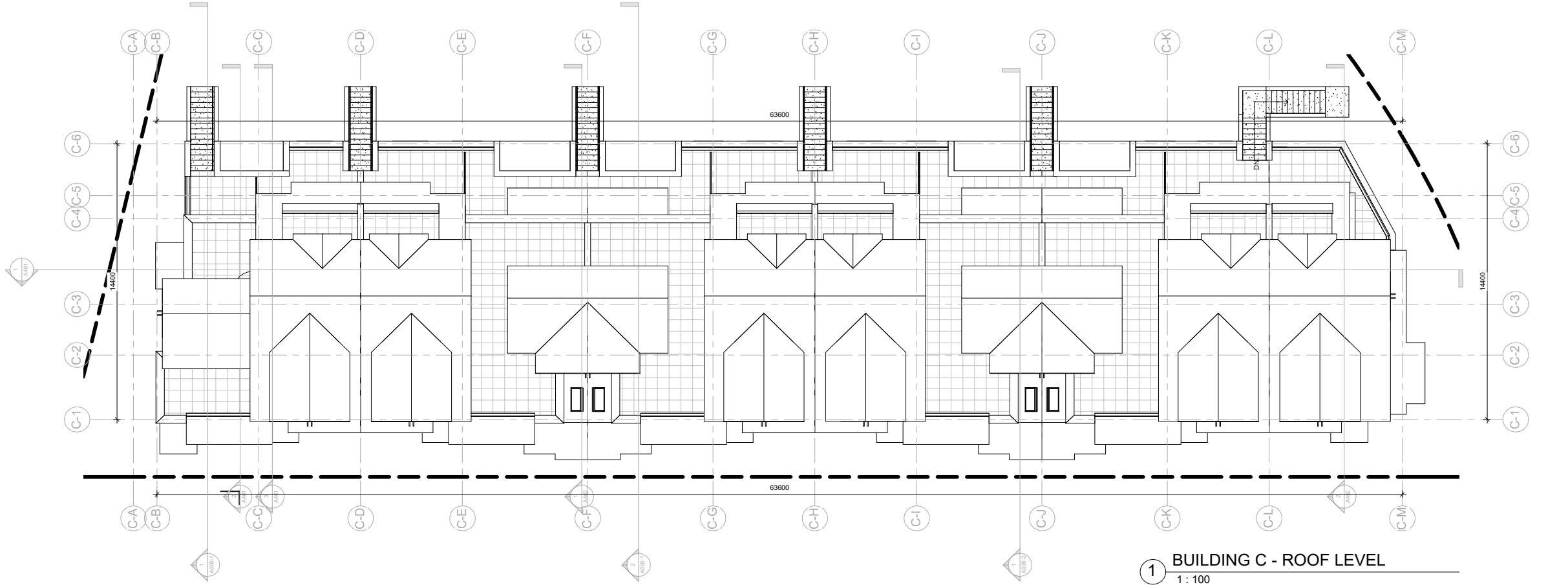
TOWNHOUSE BUILDING C  
FLOOR PLANS

A262

No.	Date/Rev	Description	Consultant
1	20/01/13	ISSUED FOR CIP	
2	20/06/19	REVISED FOR CIP SUBMISSION	

JYW ARCHITECTURE INC.  
21121 Columbia Street, Vancouver, B.C. Canada, V6A 2P5  
Tel: 604 276 0100 Email: jyw@jyw.ca

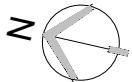
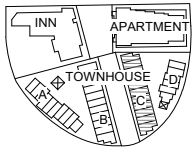




1 BUILDING C - ROOF LEVEL  
1 : 100

Attachment A  
Page 53 of 80

KEY PLAN



JYW ARCHITECTURE INC.  
211211 Columbia Street, Vancouver, B.C. Canada, V6A 2P5  
Tel: 604 281 2111 Email: jyw@jyw.ca

Consultant

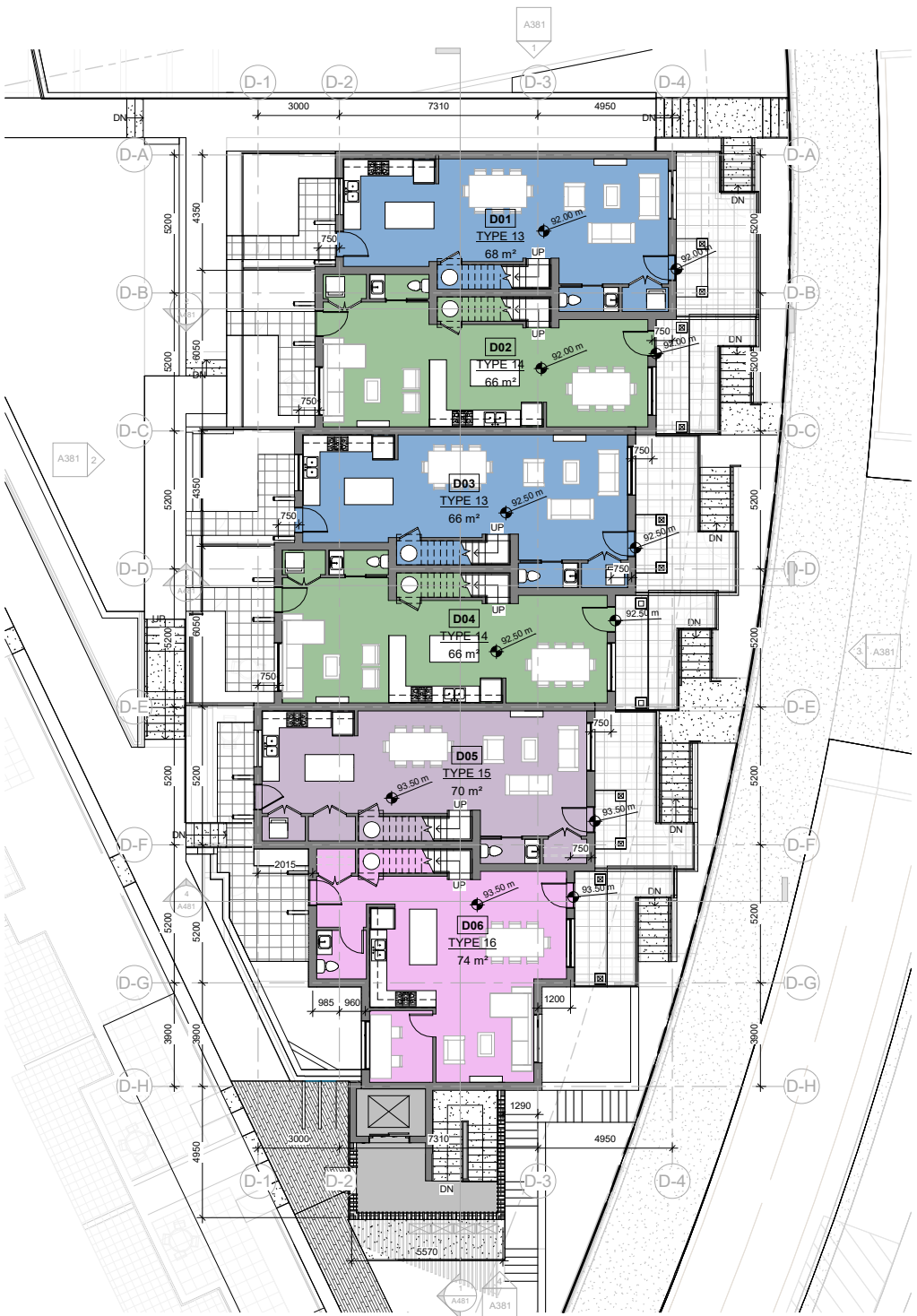
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Description:  
ISSUED FOR CD  
REVISED FOR CD SUBMISSION

VILLAGE CENTRE TOWNHOUSE  
GOSPEL ROCK VILLAGE  
PROJECT NUMBER: 1003

TOWNHOUSE BUILDING C  
FLOOR PLANS

A263

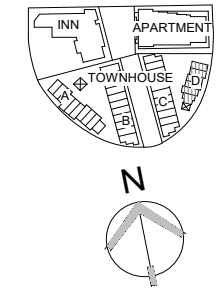


1 BUILDING D - LEVEL 01  
1 : 100



2 BUILDING D - LEVEL 02  
1 : 100

Attachment A  
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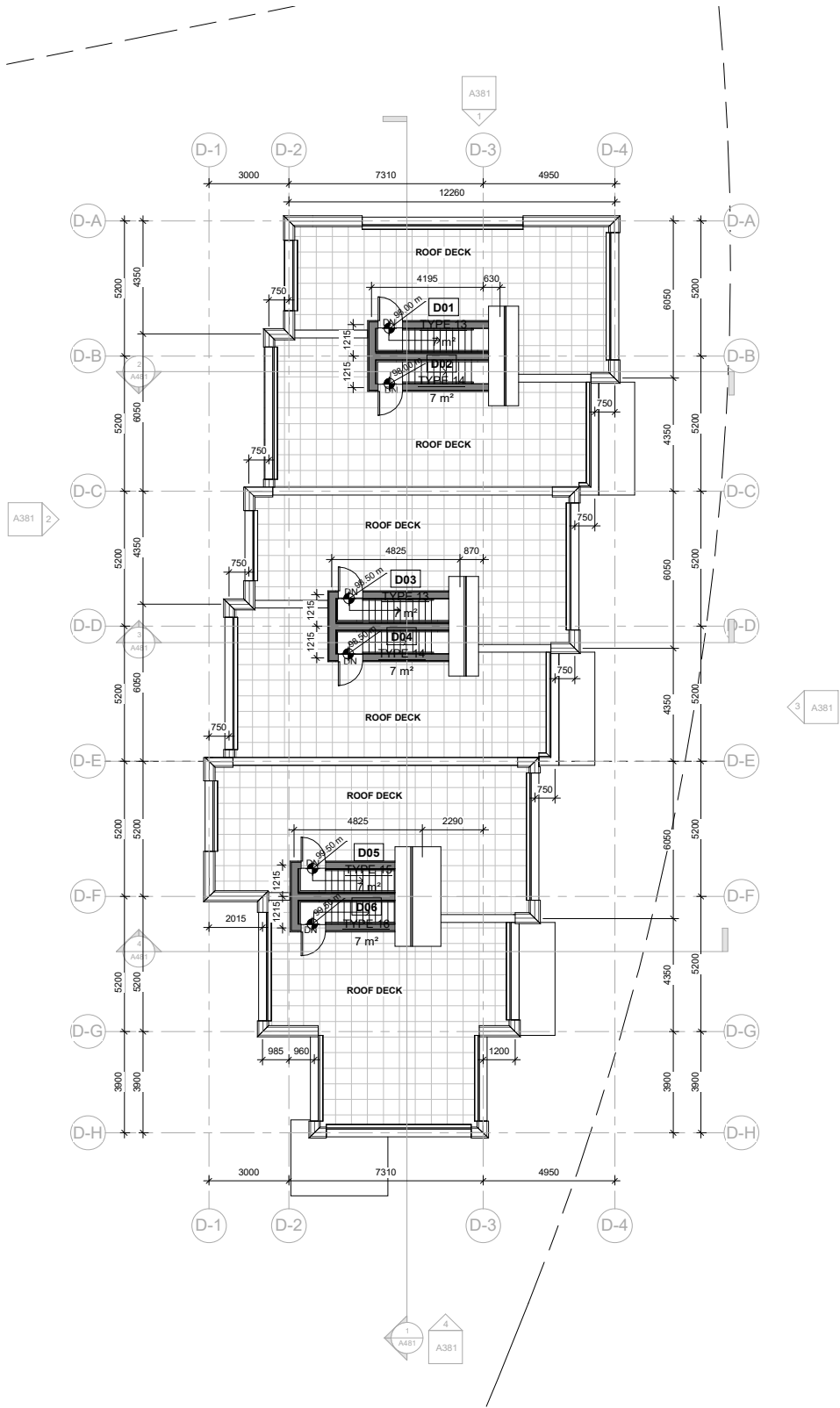


JTW ARCHITECTURE INC.  
211-211 Columbia Street, Vancouver, B.C. Canada, V6A 2R5  
Tel: 604 681 5161 Email: jtw@jtw.ca

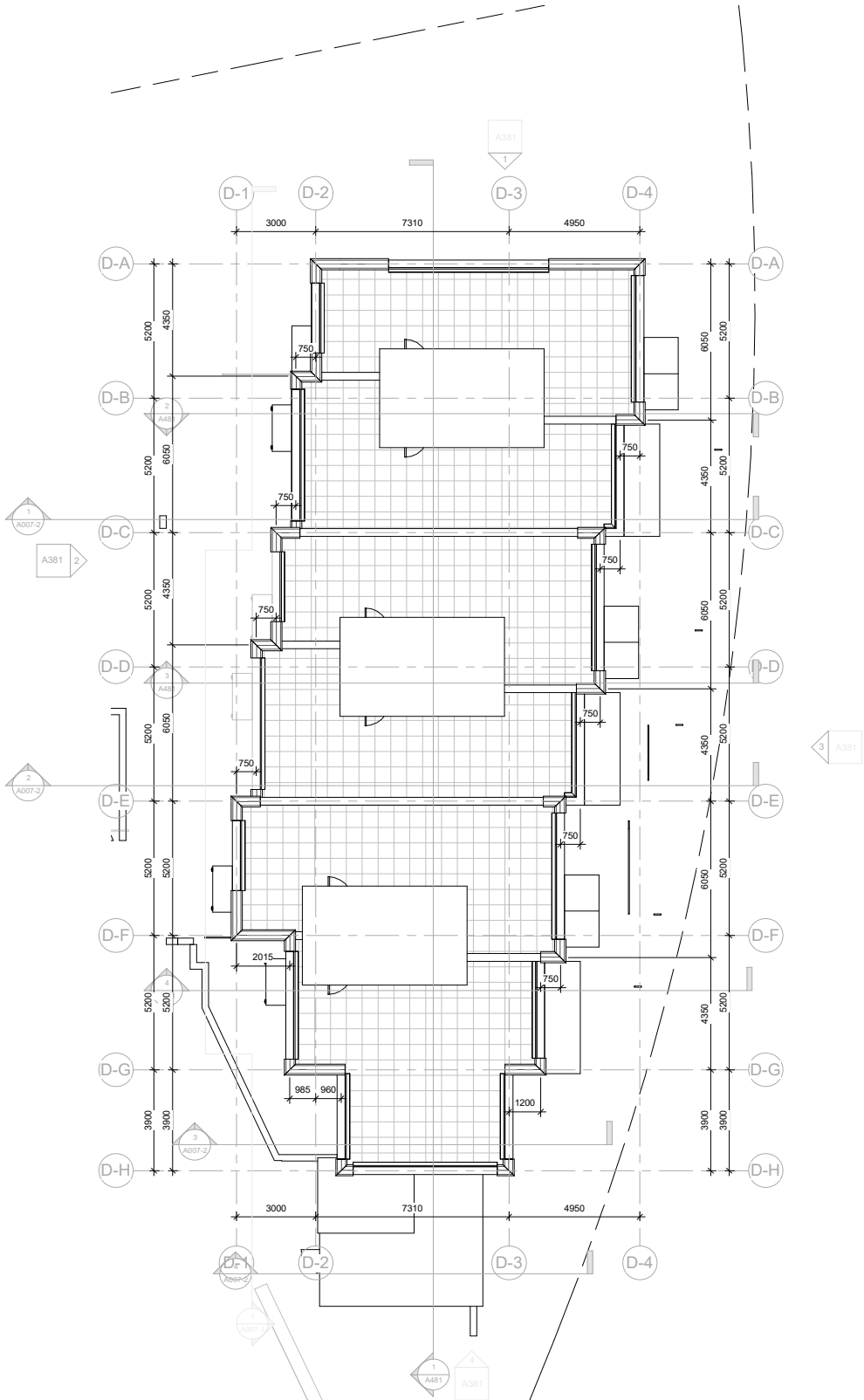
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1	2018/11/23	ISSUED FOR CP	
2	2019/06/19	REVISED FOR CP SUBMISSION	

VILLAGE CENTRE TOWNHOUSE  
GOSPEL ROCK VILLAGE  
BUILDING D  
PROJECT NUMBER: 1003  
TOWNHOUSE BUILDING D  
FLOOR PLANS

A281

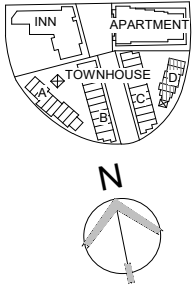


1 BUILDING D - LEVEL 03  
1 : 100



2 BUILDING D - ROOF PLAN  
1 : 100

Attachment A  
Page 55 of 88



JYW ARCHITECTURE INC.  
211-211 Columbia Street, Vancouver, B.C. Canada, V6A 2P6  
Tel: 604 255 5555 Email: jyw@jyw.ca

Consultant

No. 1  
2

Date/Rev

Description

ISSUED FOR CP  
REVISED FOR CP SUBMISSION

VILLAGE CENTRE TOWNHOUSE

GOSPEL ROCK VILLAGE  
BLOCK 10, PHASE 1  
PROJECT NUMBER: 1003

TOWNHOUSE BUILDING D  
FLOOR PLANS

A282





JAMES HARDIE -  
LIGHT MIST



JAMES HARDIE -  
TIMBER BARK



MUTUAL MATERIALS -  
REDONDO GRAY



BENJAMIN MOORE -  
CC 420

VILLAGE CENTRE TOWNHOUSE  
GOSPEL ROCK VILLAGE  
1000 GOSPEL ROCK BLVD  
PROJECT NUMBER: 1003

COLOUR & MATERIAL  
INSPIRATION

A300

No.	Date/Time	Description	Consultant
1	2019/11/23	ISSUED FOR CD	
2	2019/06/19	REVISED FOR CD SUBMISSION	



JTW ARCHITECTURE INC.  
211-211 Columbia Street, Vancouver, B.C. Canada, V6A 2P6  
Tel: 604 683 3768 Email: jtw@jtw.ca





**JYW ARCHITECTURE INC.**  
21121 Columbia Street, Vancouver, B.C. Canada, V6A 2R5  
Tel: 604 276 3163 Email: jyw@jywan.ca

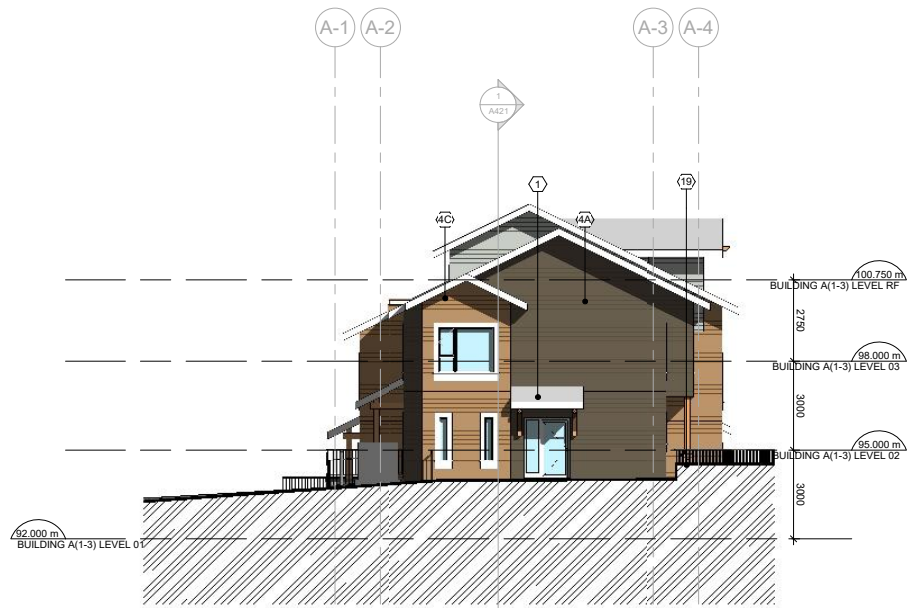
**VILLAGE CENTRE TOWNHOUSE**  
GOSPEL ROCK VILLAGE,  
BLUFFVIEW, VANCOUVER, BC  
PROJECT NUMBER: 1003

**TOWNHOUSE BUILDING A AND  
PAVILION E ELEVATIONS**

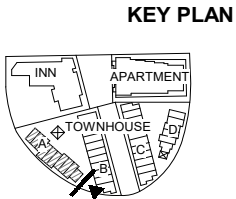
**A321**

No.	DATE/YMD	Description
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2	20190619	REVISED FOR CD SUBMISSION

Consultant



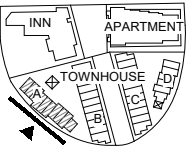
1 BUILDING A - SOUTHEAST ELEVATION  
1 : 100



EXTERIOR FINISHES	
NOTE	DESCRIPTION
1	ASPHALT SHINGLES ROOFING: CERTANTEED- LANDMARK SILVER BIRCH OR SIMILAR
4	FIBER CEMENT SIDING
4A	FIBER CEMENT SIDING: JAMES HARDI- TIMBER BARK OR SIMILAR
4B	FIBER CEMENT SIDING: JAMES HARDI- LIGHT MIST OR SIMILAR
4C	FIBER CEMENT SIDING: WOODTONE- COLOR TO MATCH BENJAMIN MOORE CC-420
8	DECORATIVE ROOF BRACKET: COLOR OLYMPIC 716 CEDAR NATURALTONE OR SIMILAR
13	SAFETY GLASS IN METAL POST GUARDRAIL BEHIND WALL, COLOR TO MATCH BENJAMIN MOORE CC-420
16	POLYMER-MODIFIED CEMENT MouldING AND FASCIA TRIM BOARD: COLOR TBC
19	WOOD POST: COLOR OLYMPIC 716 CEDAR NATURALTONE OR SIMILAR



2 BUILDING A - SOUTHWEST ELEVATION  
1 : 100



JYW ARCHITECTURE INC.  
211-211 Columbia Street, Vancouver, B.C., Canada, V6A 2R5  
Tel: 604 689 3168 Email: jyw@jyw.ca

Consultant

Description:  
ISSUED FOR CIP  
REVISED FOR CIP SUBMISSION

Drawn/MD:  
2019/11/23  
2019/06/19

No.  
1  
2

VILLAGE CENTRE TOWNHOUSE  
GOSPEL ROCK VILLAGE  
REDACTED ADDRESS  
PROJECT NUMBER: 1003  
TOWNHOUSE BUILDING A  
ELEVATIONS

A322





JYW ARCHITECTURE INC.  
211-211 Columbia Street, Vancouver, B.C. Canada, V6A 2R5  
Tel: 604 255-1181 Email: jyw@jyw.ca

Consultant

Description:  
ISSUED FOR CIP  
REVISED FOR CIP SUBMISSION

No. 2

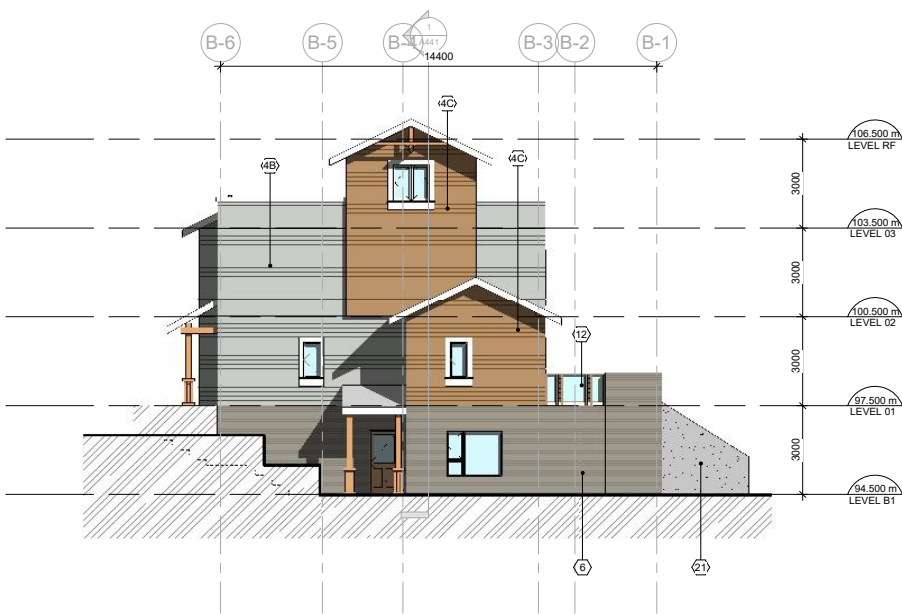
VILLAGE CENTRE TOWNHOUSE  
GOSPEL ROCK VILLAGE  
PROJECT NUMBER: 1003

TOWNHOUSE BUILDING B  
ELEVATIONS

A341

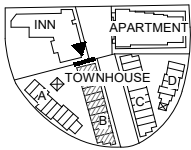


1 BUILDING B - EAST ELEVATION  
1 : 100



2 BUILDING B - NORTH ELEVATION  
1 : 100

KEY PLAN



EXTERIOR FINISHES	
NOTE	DESCRIPTION
1	ASPHALT SHINGLES ROOFING: CERTAINTED- LANDMARK SILVER BIRCH OR SIMILAR
4A	FIBER CEMENT SIDING: JAMES HARDI- TIMBER BARK OR SIMILAR
4B	FIBER CEMENT SIDING: JAMES HARDI- LIGHT MIST OR SIMILAR
4C	FIBER CEMENT SIDING: WOODTONE- COLOR TO MATCH BENJAMIN MOORE CC-420
6	BRICK VENEER: MUTUAL MATERIALS- REDONDO GRAY OR SIMILAR
9	DECORATIVE ROOF BRACKET: COLOR OLYMPIC 716 CEDAR NATURALTONE OR SIMILAR
12	SAFETY GLASS IN METAL POST GUARDRAIL, COLOR TO MATCH BENJAMIN MOORE CC-420
14	152mm WINDOW TRIM, WHITE
19	WOOD POST: COLOR OLYMPIC 716 CEDAR NATURALTONE OR SIMILAR
21	CONCRETE WITH ELASTOMERIC COATING FINISH





JTW ARCHITECTURE INC.  
211-211 Columbia Street, Vancouver, B.C. Canada, V6A 2R5  
Tel: 604 255 3165 Email: jtw@jtw.ca

Consultant

Description:  
ISSUED FOR CD  
REVISED FOR CD SUBMISSION

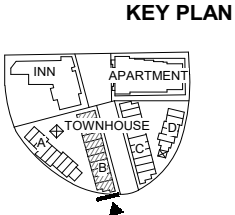
No. 1  
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VILLAGE CENTRE TOWNHOUSE  
GOSPEL ROCK VILLAGE,  
1000 SHERBOURNE STREET  
PROJECT NUMBER: 1003

A342



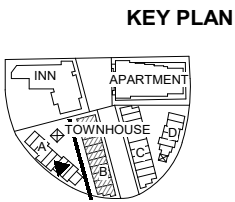
1 BUILDING B - SOUTH ELEVATION  
1 : 100

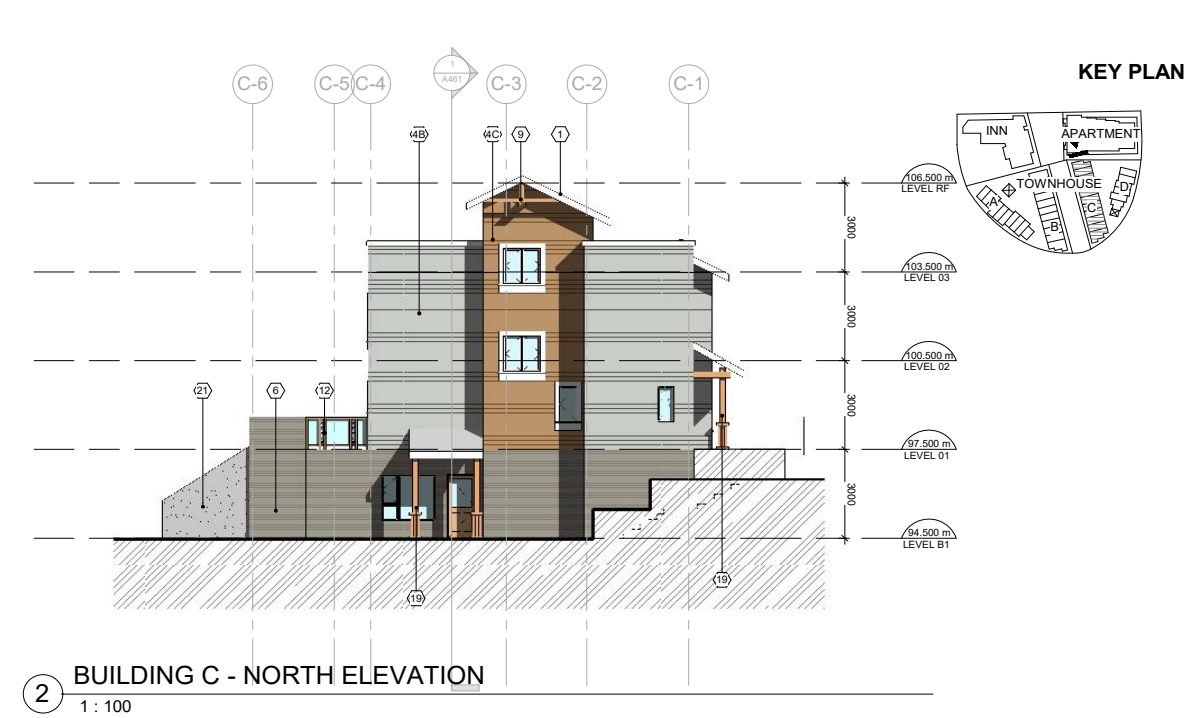


EXTERIOR FINISHES	
NOTE	DESCRIPTION
1	ASPHALT SHINGLES ROOFING: CERTAINTED- LANDMARK SILVER BIRCH OR SIMILAR
4A	FIBER CEMENT SIDING: JAMES HARDI- TIMBER BARK OR SIMILAR
4B	FIBER CEMENT SIDING: JAMES HARDI- LIGHT MIST OR SIMILAR
4C	FIBER CEMENT SIDING: WOODTONE- COLOR TO MATCH BENJAMIN MOORE CC-420
6	BRICK VENEER: MUTUAL MATERIALS: REDONDO GRAY OR SIMILAR
12	SAFETY GLASS IN METAL POST GUARDRAIL, COLOR TO MATCH BENJAMIN MOORE CC-420
13	SAFETY GLASS IN METAL POST GUARDRAIL BEHIND WALL, COLOR TO MATCH BENJAMIN MOORE CC-420
19	WOOD POST: COLOR OLYMPIC 716 CEDAR NATURALTONE OR SIMILAR
21	CONCRETE WITH ELASTOMERIC COATING FINISH



2 BUILDING B - WEST ELEVATION  
1 : 100





EXTERIOR FINISHES	
NOTE	DESCRIPTION
1	ASPHALT SHINGLES ROOFING: CERTAINTED® LANDMARK SILVER BIRCH OR SIMILAR
4A	FIBER CEMENT SIDING: JAMES HARDI- TIMBER BARK OR SIMILAR
4B	FIBER CEMENT SIDING: JAMES HARDI- LIGHT MIST OR SIMILAR
4C	FIBER CEMENT SIDING: WOODTONE- COLOR TO MATCH BENJAMIN MOORE CC-420
6	BRICK VENEER: MUTUAL MATERIALS- REDONDO GRAY OR SIMILAR
9	DECORATIVE ROOF BRACKET: COLOR OLYMPIC 716 CEDAR NATURALTONE OR SIMILAR
12	SAFETY GLASS IN METAL POST GUARDRAIL. COLOR TO MATCH BENJAMIN MOORE CC-420
13	SAFETY GLASS IN METAL POST GUARDRAIL BEHIND WALL. COLOR TO MATCH BENJAMIN MOORE CC-420
19	WOOD POST: COLOR OLYMPIC 716 CEDAR NATURALTONE OR SIMILAR
21	CONCRETE WITH ELASTOMERIC COATING FINISH

VILLAGE CENTRE TOWNHOUSE  
GOSPEL ROCK VILLAGE,  
1000 GOSPEL ROCK ROAD,  
PORT CHARLOTTE, FL 34684  
PROJECT NUMBER: 1003

TOWNHOUSE BUILDING C  
ELEVATIONS

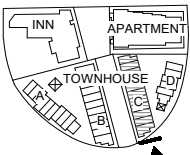
A361





1 BUILDING C - SOUTH ELEVATION  
1 : 100

KEY PLAN

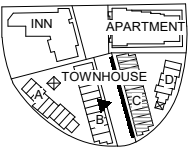


EXTERIOR FINISHES	
NOTE	DESCRIPTION
1	ASPHALT SHINGLES ROOFING: CERTAINTED- LANDMARK SILVER BIRCH OR SIMILAR
4A	FIBER CEMENT SIDING: JAMES HARDI- TIMBER BARK OR SIMILAR
4B	FIBER CEMENT SIDING: JAMES HARDI- LIGHT MIST OR SIMILAR
4C	FIBER CEMENT SIDING: WOODTONE- COLOR TO MATCH BENJAMIN MOORE CC-420
6	BRICK VENEER: MUTUAL MATERIALS- REDONDO GRAY OR SIMILAR
9	DECORATIVE ROOF BRACKET: COLOR OLYMPIC 716 CEDAR NATURALTONE OR SIMILAR
12	SAFETY GLASS IN METAL POST GUARDRAIL, COLOR TO MATCH BENJAMIN MOORE CC-420
13	SAFETY GLASS IN METAL POST GUARDRAIL BEHIND WALL, COLOR TO MATCH BENJAMIN MOORE CC-420
19	WOOD POST: COLOR OLYMPIC 716 CEDAR NATURALTONE OR SIMILAR



2 BUILDING C - WEST ELEVATION  
1 : 100

KEY PLAN



JYW ARCHITECTURE INC.  
211-011 Columbia Street, Vancouver, B.C. Canada, V6A 2P6  
Tel: 604 288 2168 Email: jyw@jyw.ca

Consultant

Description:  
ISSUED FOR CD  
REVISED FOR CD SUBMISSION

No. 1  
2

VILLAGE CENTRE TOWNHOUSE

GOSPEL ROCK VILLAGE,  
BLUESKY QUARTERS  
PROJECT NUMBER: 1003

TOWNHOUSE BUILDING C  
ELEVATIONS

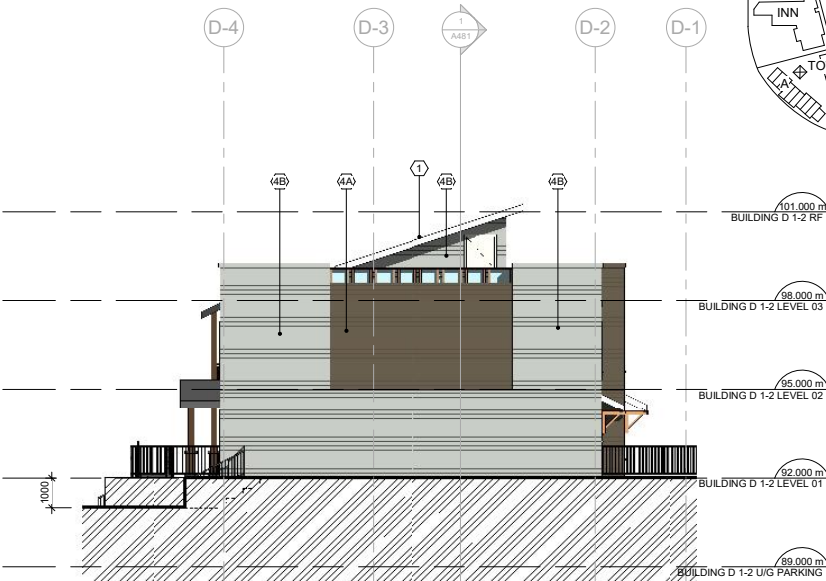
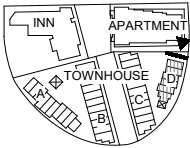
A362





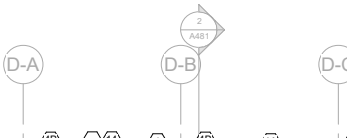
JYW ARCHITECTURE INC.  
211211 Columbia Street, Vancouver, B.C. Canada, V6A 2R5  
Tel: 604 255 5155 Email: jyw@jyw.ca

KEY PLAN



1 BUILDING D - NORTH ELEVATION  
1 : 100

KEY PLAN

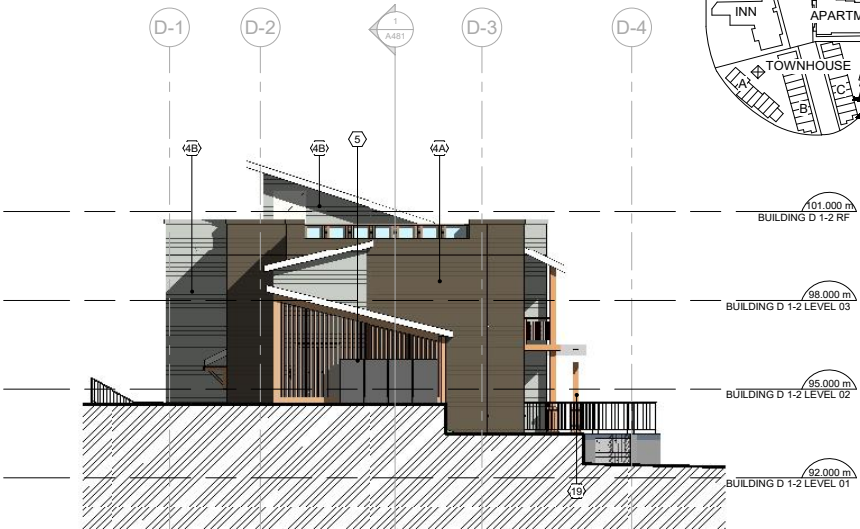
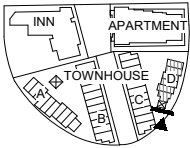


2 BUILDING D - WEST ELEVATION  
1 : 100

EXTERIOR FINISHES	
Key Value	Keynote Text
1	ASPHALT SHINGLES ROOFING: CERTAINTED- LANDMARK SILVER BIRCH OR SIMILAR
4A	FIBER CEMENT SIDING: JAMES HARDI- TIMBER BARK OR SIMILAR
4B	FIBER CEMENT SIDING: JAMES HARDI- LIGHT MIST OR SIMILAR
5	PREFINISH METAL SCREENING: COLOR TO MATCH BENJAMIN MOORE CC-420
13	SAFETY GLASS IN METAL POST GUARDRAIL BEHIND WALL, COLOR TO MATCH BENJAMIN MOORE CC-420
14	152mm WINDOW TRIM, WHITE
19	WOOD POST: COLOR OLYMPIC 716 CEDAR NATURALTONE OR SIMILAR

KEY PLAN

KEY PLAN



4 BUILDING D - SOUTH ELEVATION  
1 : 100

KEY PLAN



3 BUILDING D - EAST ELEVATION  
1 : 100

Consultant

Description:

ISSUED FOR CD

REUSED FOR CD SUBMISSION

No. 1

2

VILLAGE CENTRE TOWNHOUSE

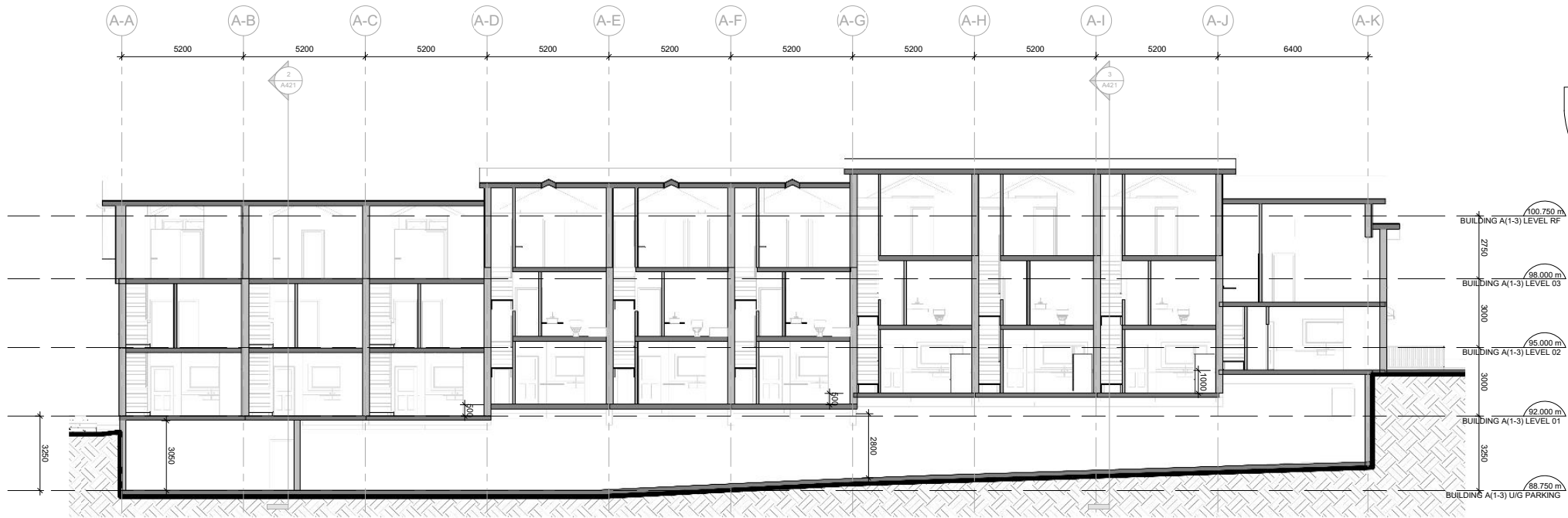
GOSPEL ROCK VILLAGE

PROJECT NUMBER: 1003

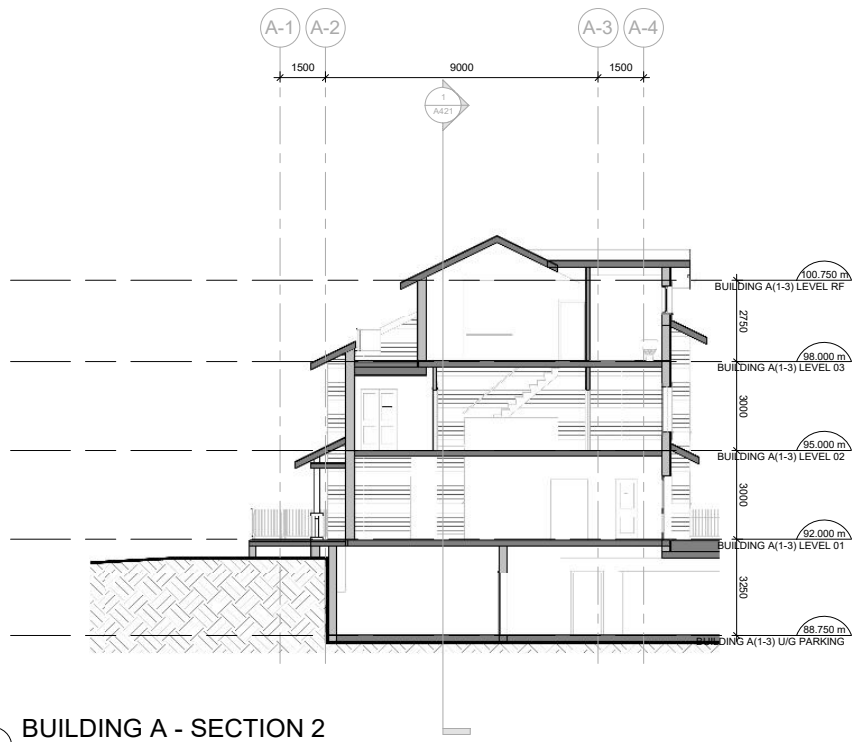
TOWNHOUSE BUILDING D

ELEVATIONS

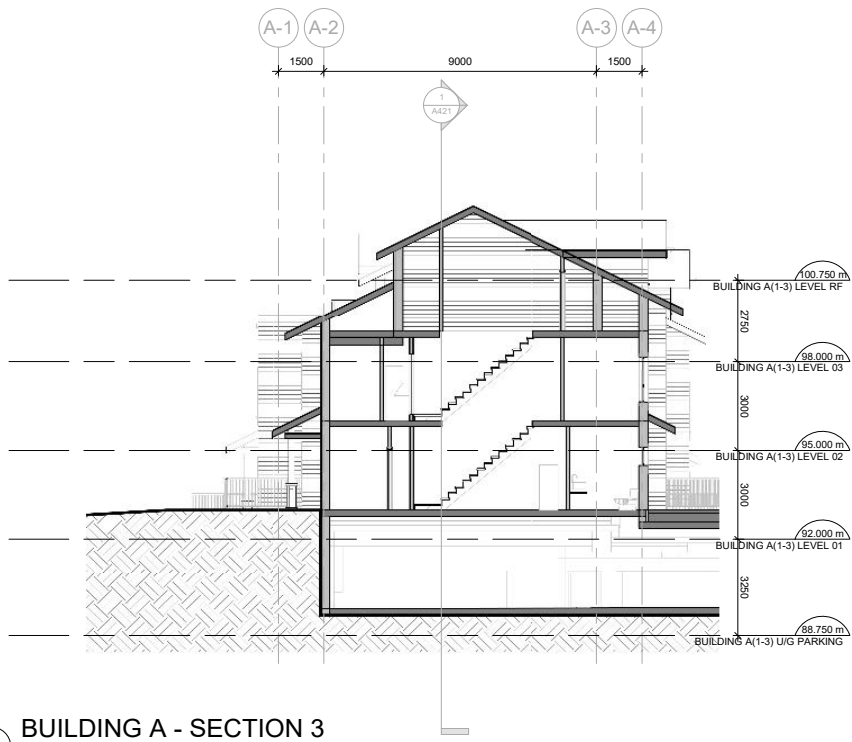
A381



1 BUILDING A - SECTION 1  
1 : 100

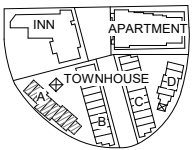



2 BUILDING A - SECTION 2  
1 : 100



3 BUILDING A - SECTION 3  
1 : 100

KEY PLAN





JYW ARCHITECTURE INC.  
211-211 Columbia Street, Vancouver, B.C. Canada, V6A 2P6  
Tel: 604 680 3168 Email: jyw@jywanal.ca

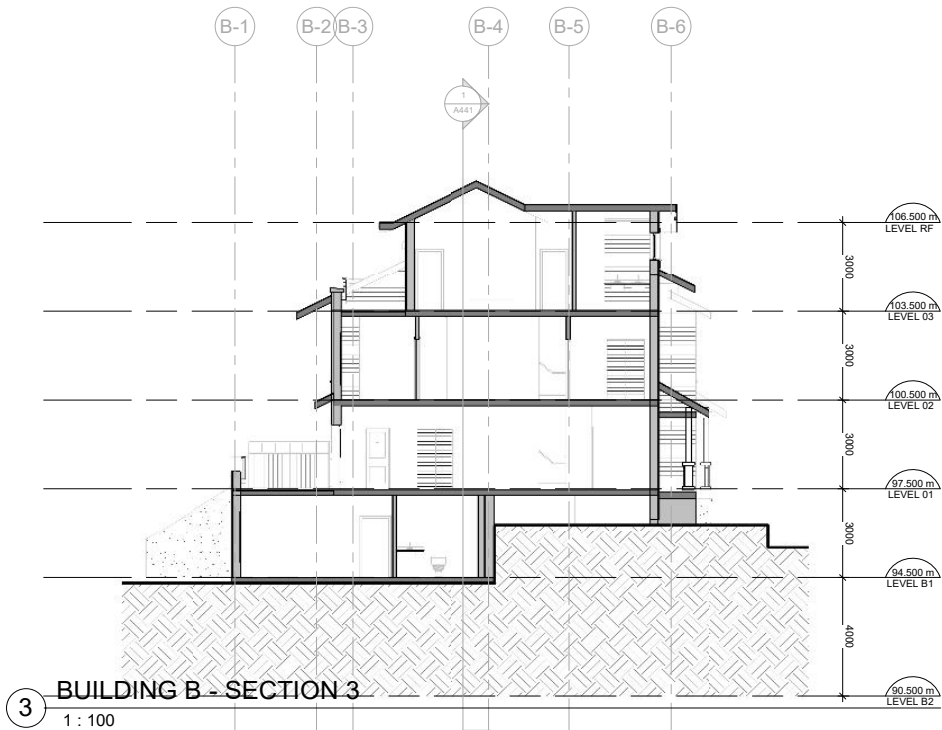
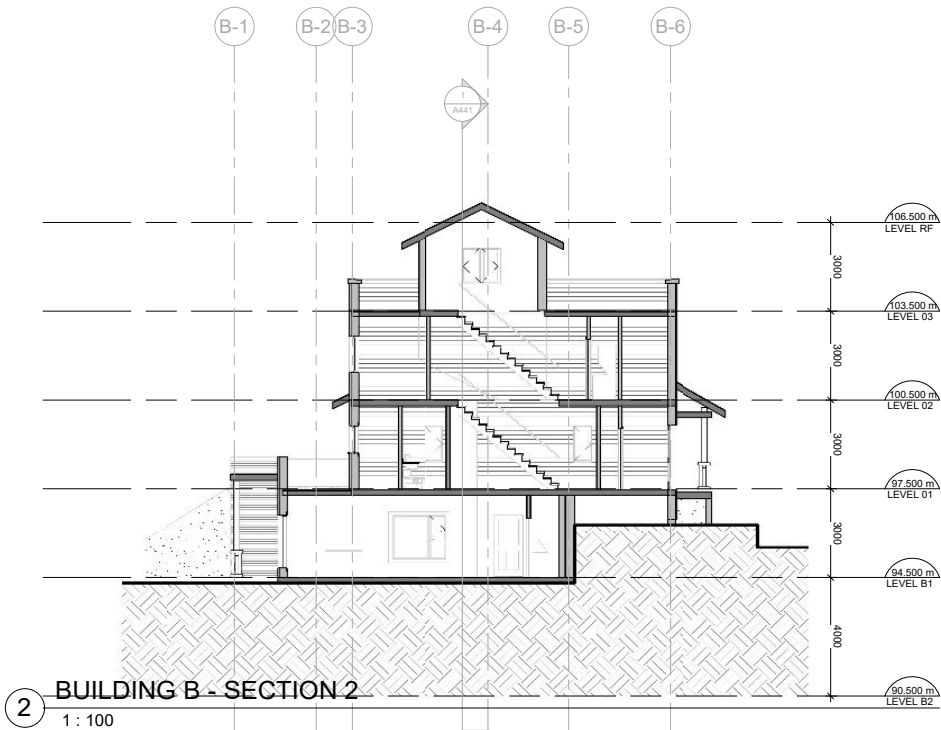
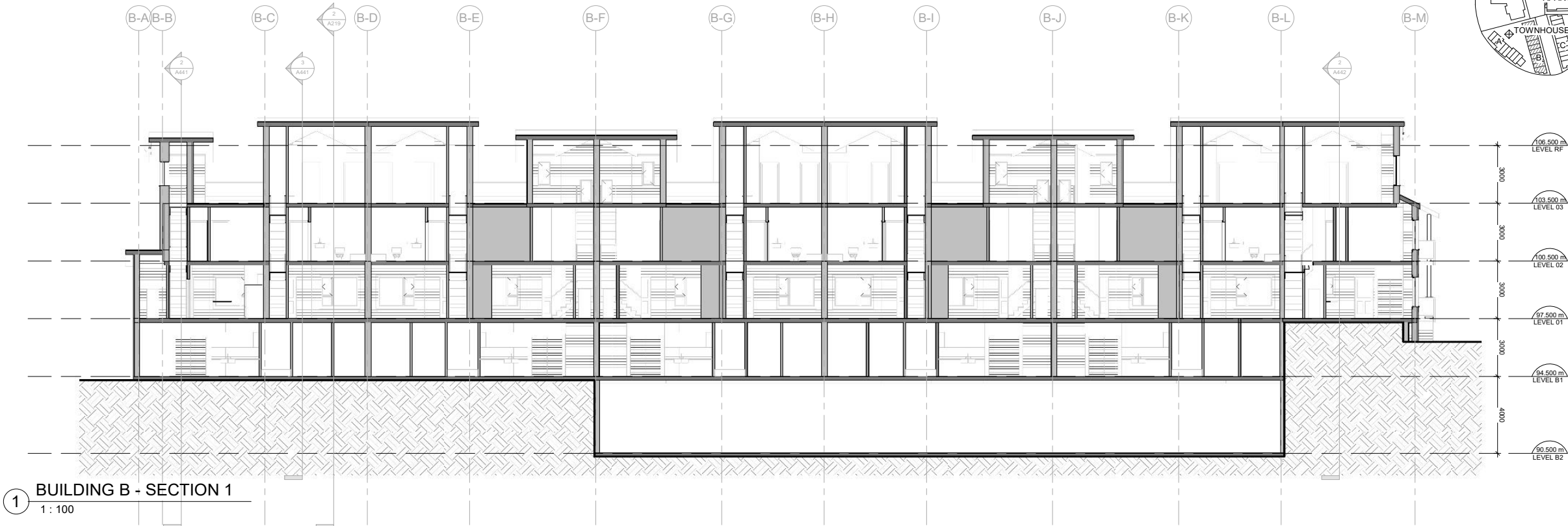
No.	DATE/REV	DESCRIPTION	CONSULTANT
1	20191123	ISSUED FOR CD	
2	20190619	REVISED FOR CD SUBMISSION	

VILLAGE CENTRE TOWNHOUSE  
GOSPEL ROCK VILLAGE,  
1000 GOSPEL ROCK ROAD,  
VANCOUVER, BC  
PROJECT NUMBER: 1003

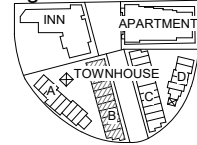
TOWNHOUSE BUILDING A  
SECTIONS

A421





Attachment A KEY PLAN  
Page 65 of 80



JYW ARCHITECTURE INC.  
211-211 Columbia Street, Vancouver, B.C. Canada, V6A 2R5  
Tel: 604 680 3160 Email: jyw@jyw.ca

Consultant

Description:

No. 1

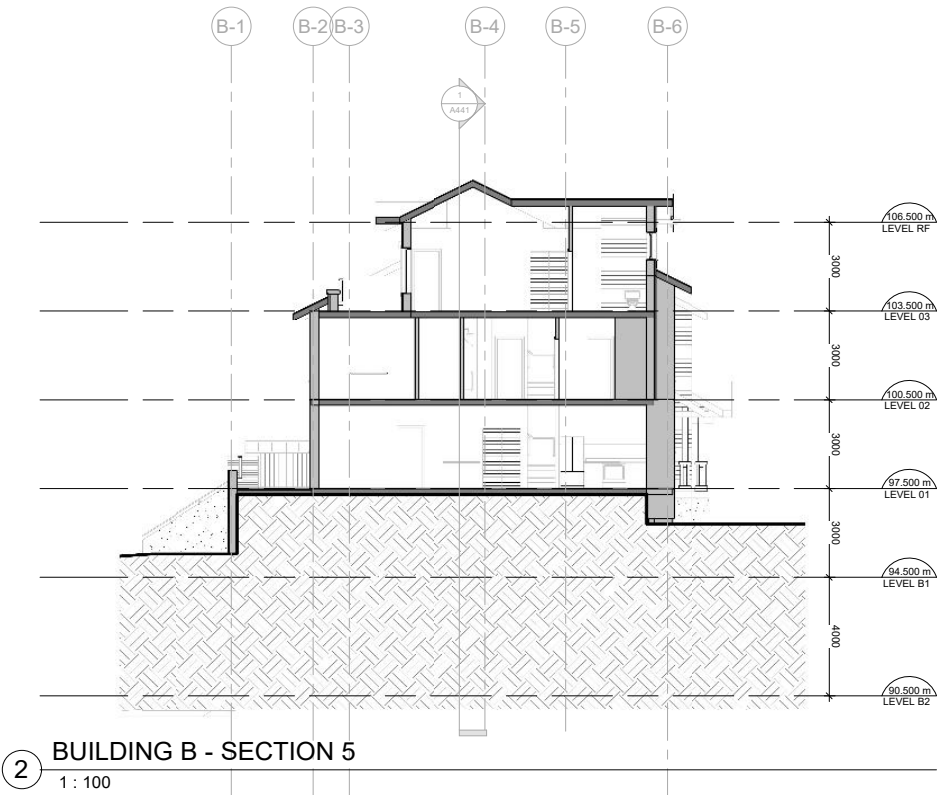
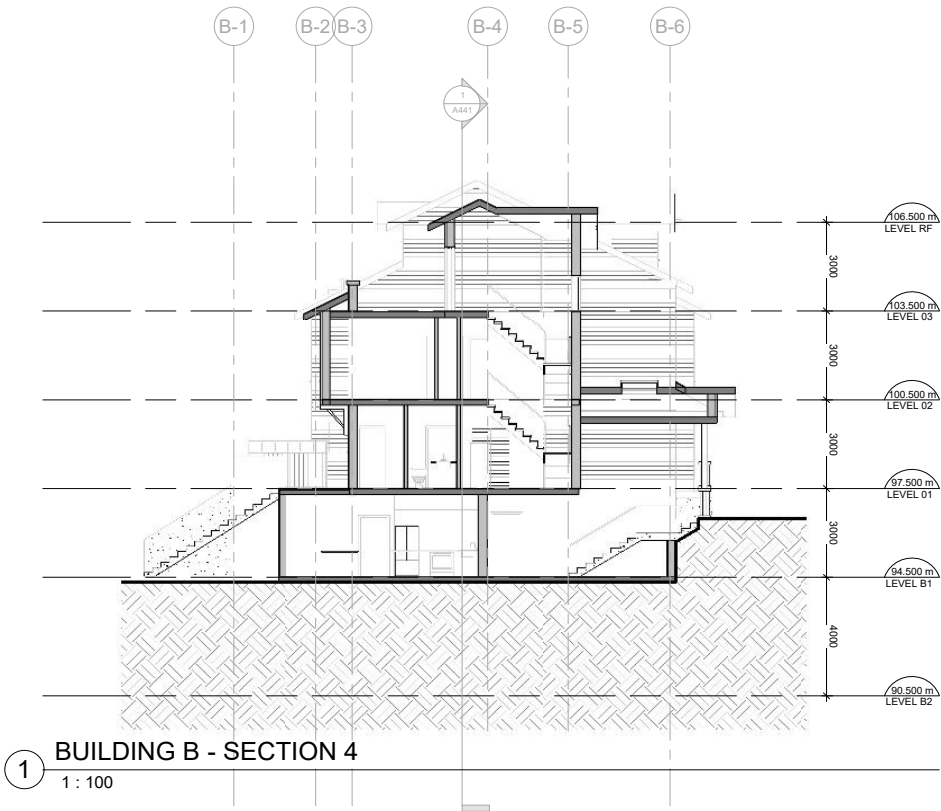
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VILLAGE CENTRE TOWNHOUSE  
GOSPEL ROCK VILLAGE  
BLOCK 1000  
PROJECT NUMBER: 1003

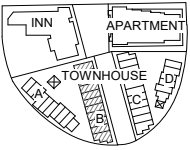
A441

TOWNHOUSE BUILDING B  
SECTIONS





Attachment A  
Page 66 of 86



JYW ARCHITECTURE INC.  
211-211 Columbia Street, Vancouver, B.C. Canada, V6A 2P6  
Tel: 604 689 3168 Email: jyw@jywanet.net

VILLAGE CENTRE TOWNHOUSE  
GOSPEL ROCK VILLAGE,  
1000 SHERBOURNE ST. W.  
PROJECT NUMBER: 1003

TOWNHOUSE BUILDING B  
SECTIONS

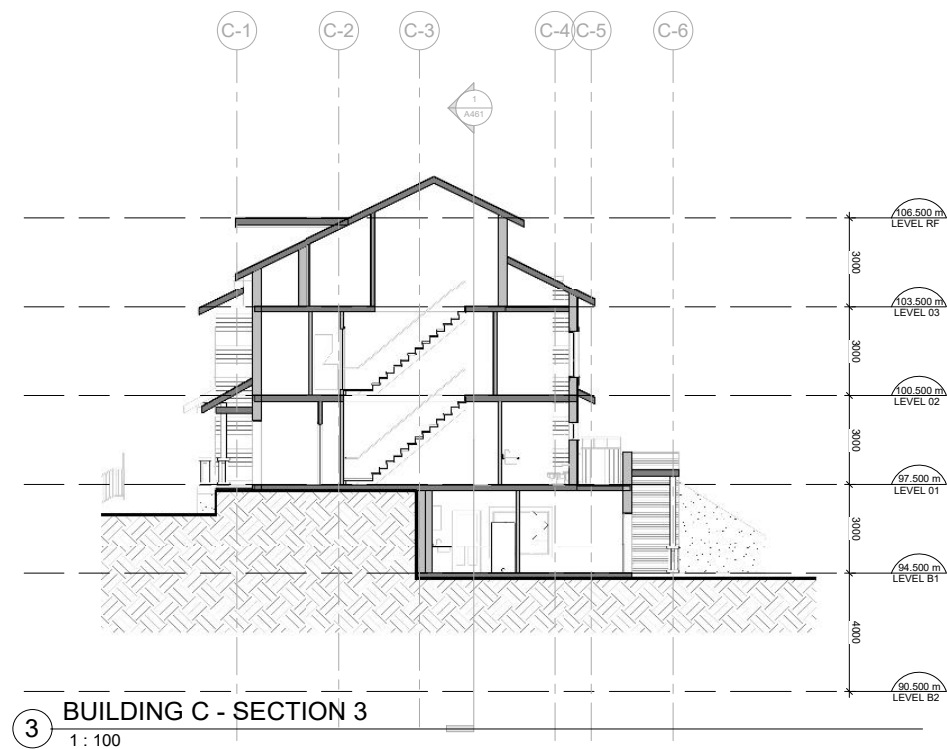
A442

Consultant

Description:

1 ISSUED FOR CD  
2 REVISED FOR CD SUBMISSION

No. Date/Time

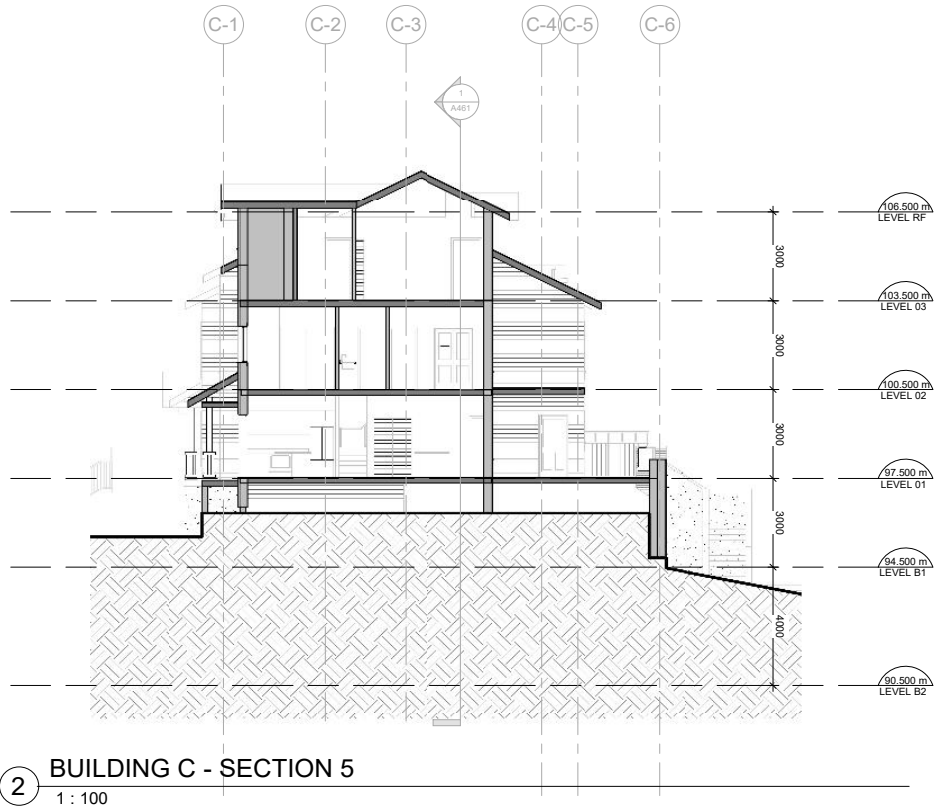
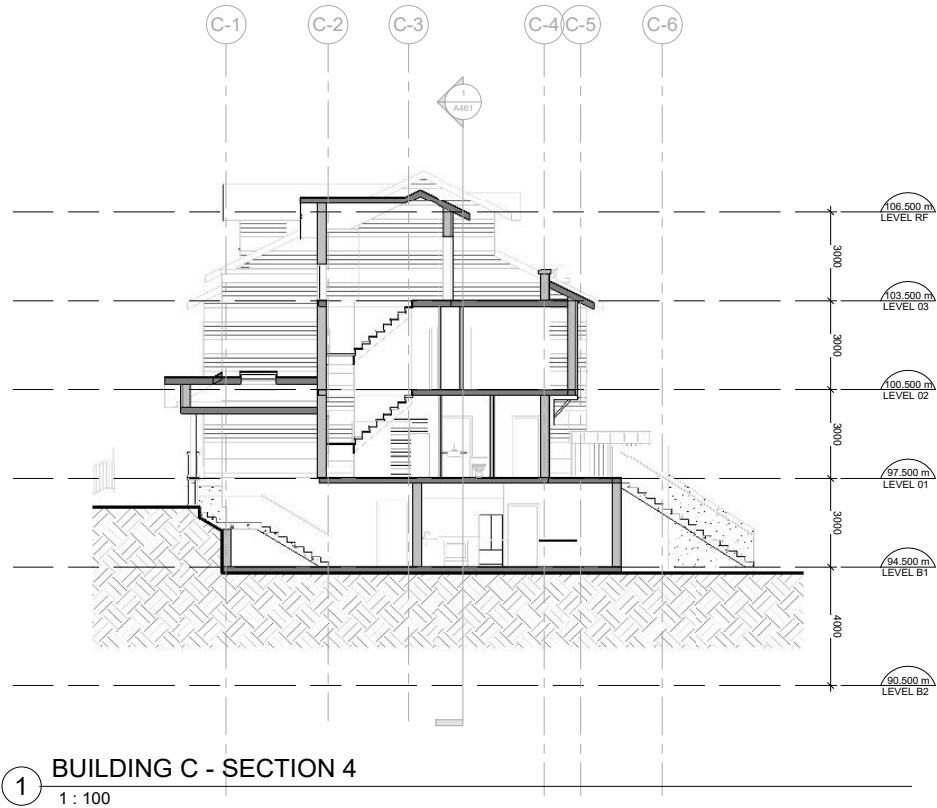


**JYW ARCHITECTURE INC.**  
211-211 Columbia Street, Vancouver, B.C., Canada, V6A 2R5

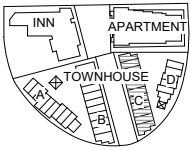
No.	Date (Y/M/D)	Description
1	2018/11/23	ISSUED FOR DP
2	2019/08/19	REVISED FOR DP SUBMISSION

VILLAGE CENTRE TOWNHOUSE  
GOSPEL ROCK VILLAGE,  
BLOCK 7, GIBSONS BC  
PROJECT NUMBER: 1603

## A461



Attachment A  
Page 68 of 80



JYW ARCHITECTURE INC.  
271211 Columbia Street, Vancouver, B.C. Canada, V6A 2P6  
Tel: 604 680 3168 Email: jyw@jywanet.net

Consultant

Description

ISSUED FOR CD  
REVISED FOR CD SUBMISSION

No. 1  
2

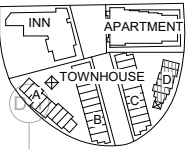
VILLAGE CENTRE TOWNHOUSE

GOSPEL ROCK VILLAGE,  
2018/1723  
PROJECT NUMBER: 1003

TOWNHOUSE BUILDING C  
SECTIONS

A462





JYW ARCHITECTURE INC.  
21121 Columbia Street, Vancouver, B.C. Canada, V6A 2R5  
Tel: 604 278 2181 Email: jyw@jyw.ca

Consultant

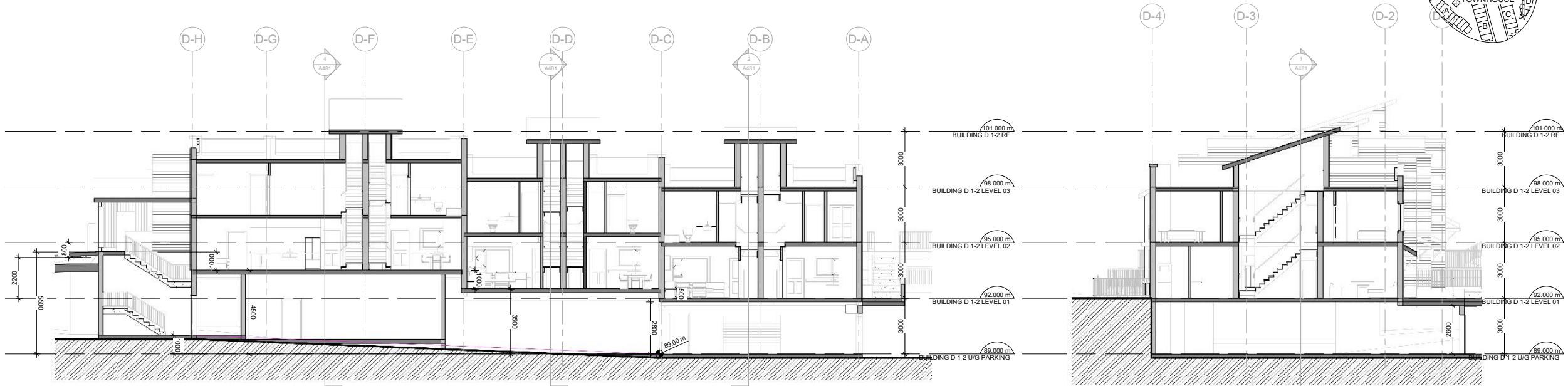
Description:  
ISSUED FOR CD  
REVISED FOR CD SUBMISSION

No. 1  
2

VILLAGE CENTRE TOWNHOUSE  
GOSPEL ROCK VILLAGE  
BUILDING D TOWNHOUSE  
PROJECT NUMBER: 1003

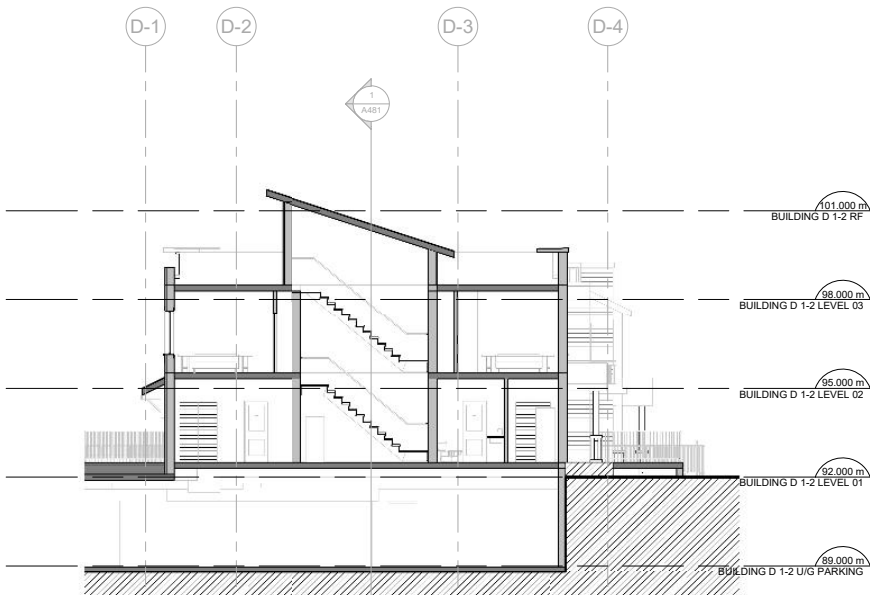
TOWNHOUSE BUILDING D  
SECTIONS

A481

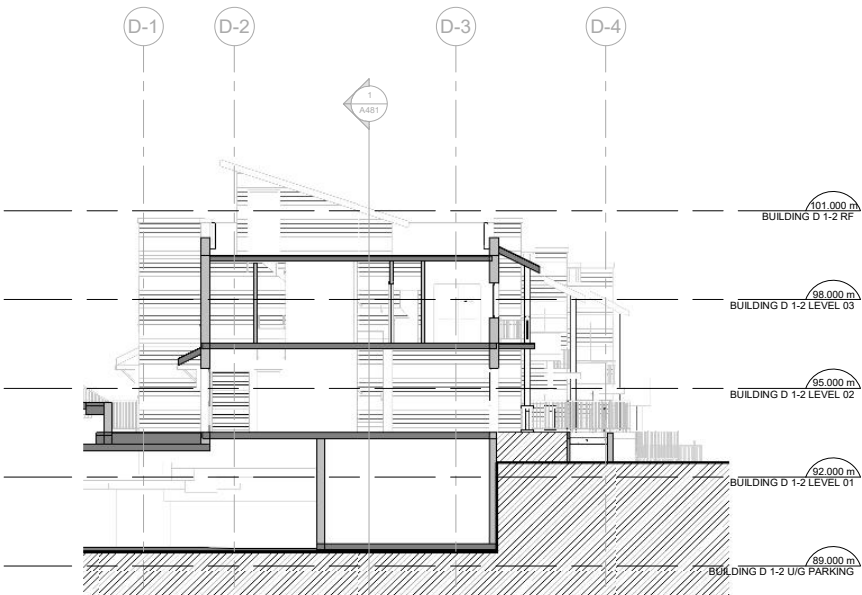


1 BUILDING D - SECTION 1  
1 : 100

2 BUILDING D - SECTION 2  
1 : 100



3 BUILDING D - SECTION 3  
1 : 100



4 BUILDING D - SECTION 4  
1 : 100

VILLAGE CENTRE LANDSCAPE DESIGN



3	Re-issued for DP	2019/08/19
2	Issued for DP	2018/11/23
1	Issued for Review	2018/11/02
Revision No.		Date

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Client:

**Greenlane  
Homes Ltd.**

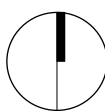
Project Title:

**Gospel Rock Village**

Drawing Title:

**Context Plan**

Project North:



Drawn By:

TT

Checked By:

JM

Scale:

1:750

Job No.:

18-044

Sheet No.:

**L0.0**



**DRAWING LEGEND**

- ① HOTEL
- ② APARTMENT
- ③ TOWNHOUSE
- ④ VILLAGE PLAZA
- ⑤ VILLAGE GREENWAY
- ⑥ SINGLE FAMILY LOTS
- ⑦ PARK GREENWAY
- ⑧ FUTURE DEVELOPMENT POTENTIAL PEDESTRIAN
- ⑨ POTENTIAL PEDESTRIAN CONNECTION



3	Re-issued for DP	2019/08/19
2	Issued for DP	2018/11/23
1	Issued for Review	2018/11/02
Revision No.		Date

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Client:

**Greenlane  
Homes Ltd.**

Project Title:

**Gospel Rock Village**

Drawing Title:

**Existing Vegetation Plan**

Project North:



Drawn By:

TT

Checked By:

JM

Scale:

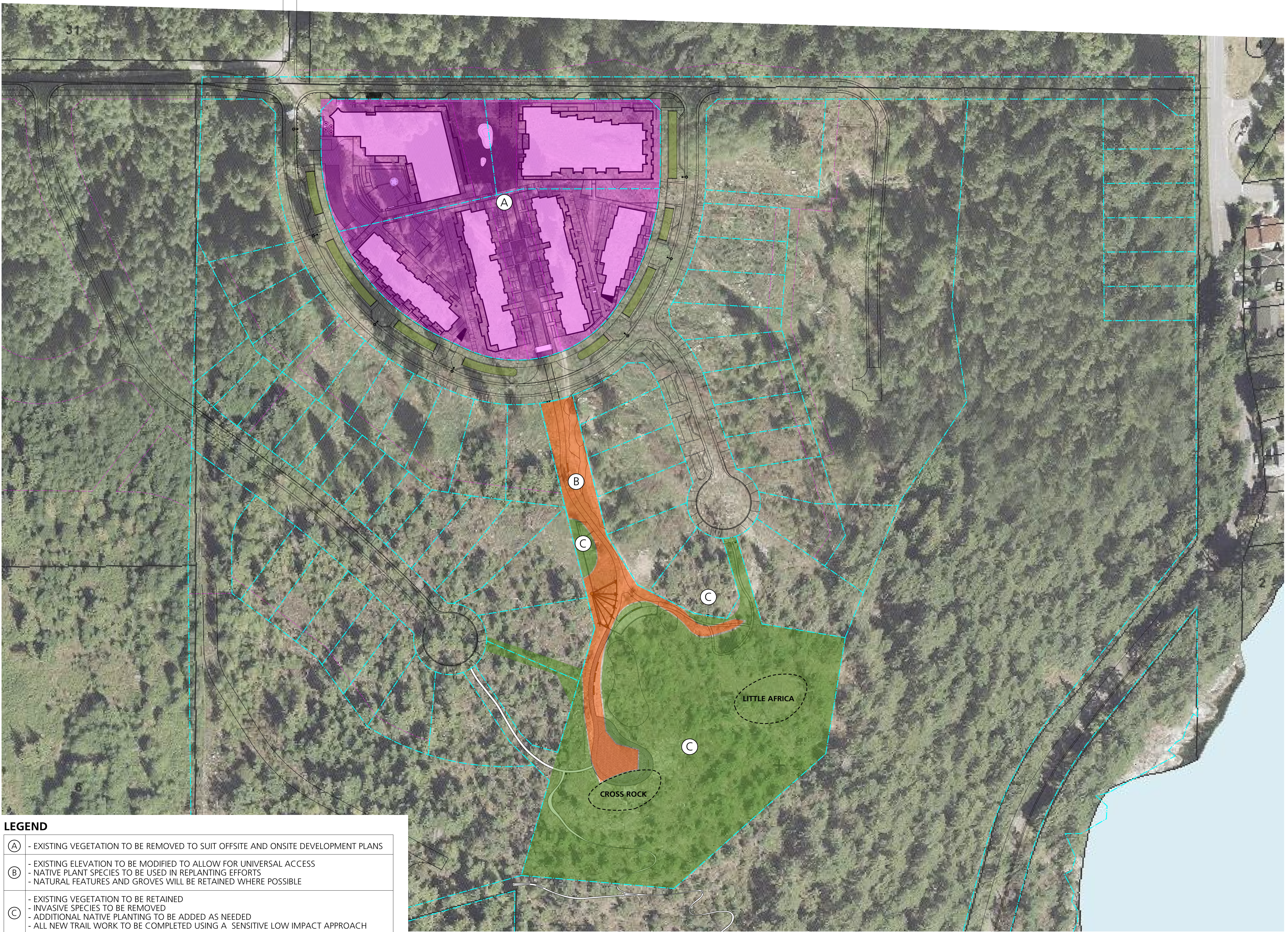
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Job No.:

18-044

Sheet No.:

**L0.1**



**LEGEND**

(A)	- EXISTING VEGETATION TO BE REMOVED TO SUIT OFFSITE AND ONSITE DEVELOPMENT PLANS
(B)	- EXISTING ELEVATION TO BE MODIFIED TO ALLOW FOR UNIVERSAL ACCESS - NATIVE PLANT SPECIES TO BE USED IN REPLANTING EFFORTS - NATURAL FEATURES AND GROVES WILL BE RETAINED WHERE POSSIBLE
(C)	- EXISTING VEGETATION TO BE RETAINED - INVASIVE SPECIES TO BE REMOVED - ADDITIONAL NATIVE PLANTING TO BE ADDED AS NEEDED - ALL NEW TRAIL WORK TO BE COMPLETED USING A SENSITIVE LOW IMPACT APPROACH



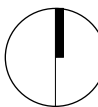
3	Re-issued for DP	2019/08/19
2	Issued for DP	2018/11/23
1	Issued for Review	2018/11/02
Revision No.		Date

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Client: **Greenlane Homes Ltd.**

Project Title: **Gospel Rock Village**

Drawing Title: **Landscape Key Plan**

Project North:	Drawn By:	TT
	Checked By:	JM
Scale:	Job No.:	18-044
Sheet No.:		

L1.0



- |                                 |                             |                               |
|---------------------------------|-----------------------------|-------------------------------|
| <b>PLAZA</b>                    | <b>GREENWAY + SITE</b>      | <b>OFFSITE</b>                |
| 01 Mutual Material Plank Pavers | 19 Light bollard            | 45 Raised pedestrian crossing |
| 02 Feature "remnant touchstone" | 20 Cast in Place Concrete   | 46 Planted swales             |
| 03 Large plaza trees            | 21 Hydrapressed Slabs       | 47 Multiuse path              |
| 04 Benches                      | 22 Concrete stairs          | 48 Street trees               |
| 05 Moveable furnishing          | 23 TBD                      | 49 Feature offsite planting   |
| 06 Restaurant patio             | 24 TBD                      | 50 Street lights              |
| 07 Plaza lighting               | 25 Raingarden               | 51 Removable Bollard          |
| <b>POOL DECK AREA</b>           | 26 Entry seat wall          | * Potential Art Location      |
| 08 Infinity edge pool           | 27 Bridge element           |                               |
| 09 Hot tub                      | 28 Custom bench             |                               |
| 10 Waterfall                    | 29 Feature stone walls      |                               |
| 11 Pool deck furnishing         | 30 Mailbox mode             |                               |
| 12 Fire pits                    | 31 Pedestrian gateway arbor |                               |
| 13 Water edge planters          | 32 Trellis                  |                               |
| 14 Ramp                         | 33 Bollards                 |                               |
| <b>GREEN LANE + SITE</b>        | 34 Removable bollards       |                               |
| 15 Unit paving                  | 35 Lawn                     |                               |
| 16 Planter bench seating        | 36 Fence + gate             |                               |
| 17 Terraced planting            | 37 Landscape planting       |                               |
| 18 Planted rainwater trench     | 38 Feature step wall        |                               |
|                                 | 39 Concrete retaining wall  |                               |
|                                 | 40 Bike racks               |                               |
|                                 | 41 Trash cans TBD           |                               |
|                                 | 42 Tree Grates TBD          |                               |
|                                 | 43 Feature planting         |                               |
|                                 | 44 TBD                      |                               |







3	Re-issued for DP	2019/08/19
2	Issued for DP	2018/11/23
1	Issued for Review	2018/11/02
Revision No.		Date

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Client:

**Greenlane  
Homes Ltd.**

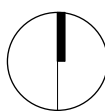
Project Title:

**Gospel Rock Village**

Drawing Title:

**Landscape  
Grading Plan**

Project North:



Drawn By:

TT

Checked By:

JM

Scale:

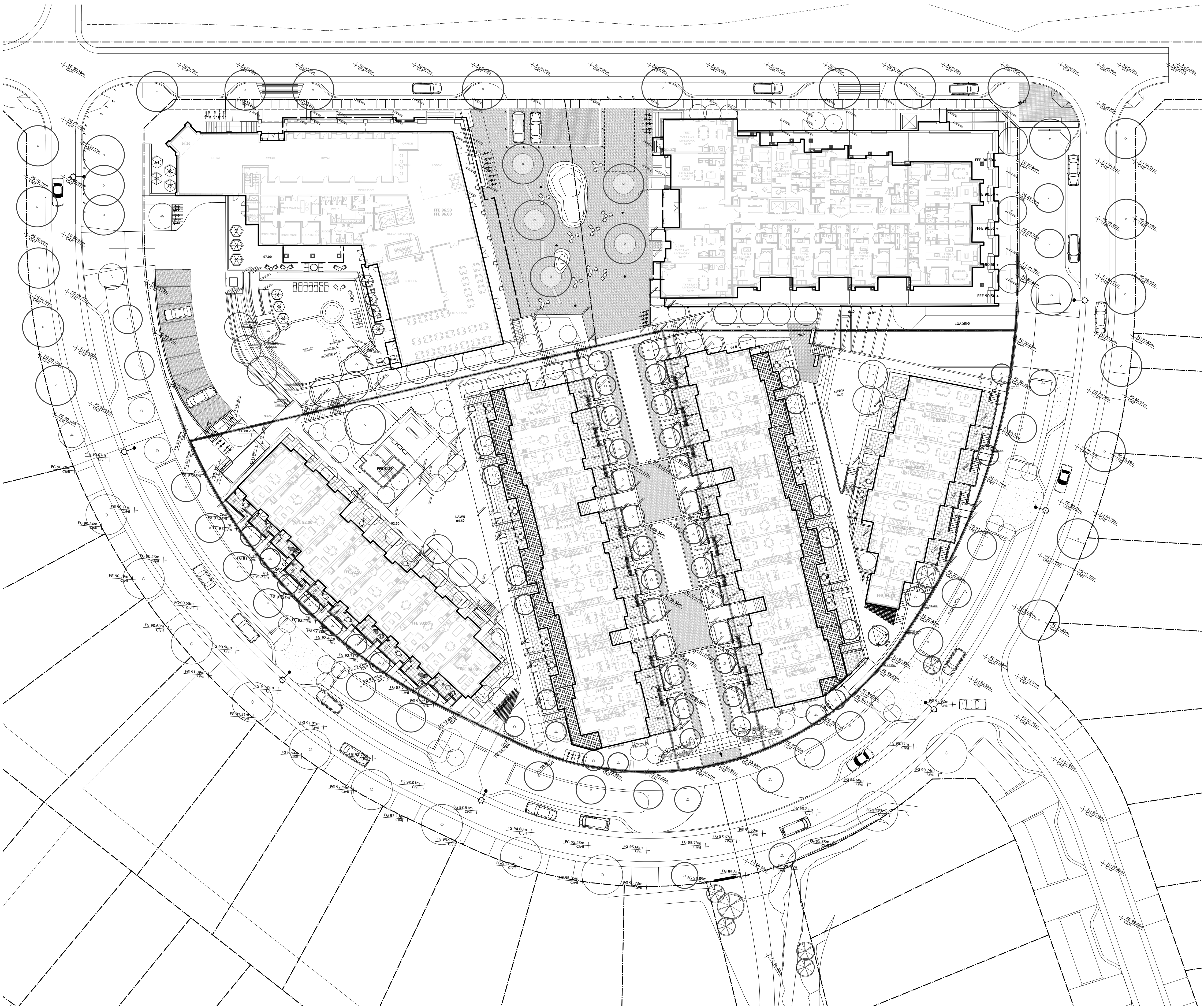
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Job No.:

18-044

Sheet No.:

**L2.0**





3 Re-issued for DP 2019/08/19  
2 Issued for DP 2018/11/23  
1 Issued for Review 2018/11/02  
Revision No. Date

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Client:

**Greenlane  
Homes Ltd.**

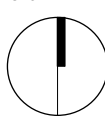
Project Title:

**Gospel Rock Village**

Drawing Title:

**Landscape  
Planting Plan**

Project North:



Drawn By:

TT

Checked By:

JM

Scale:

1:300

Job No.:

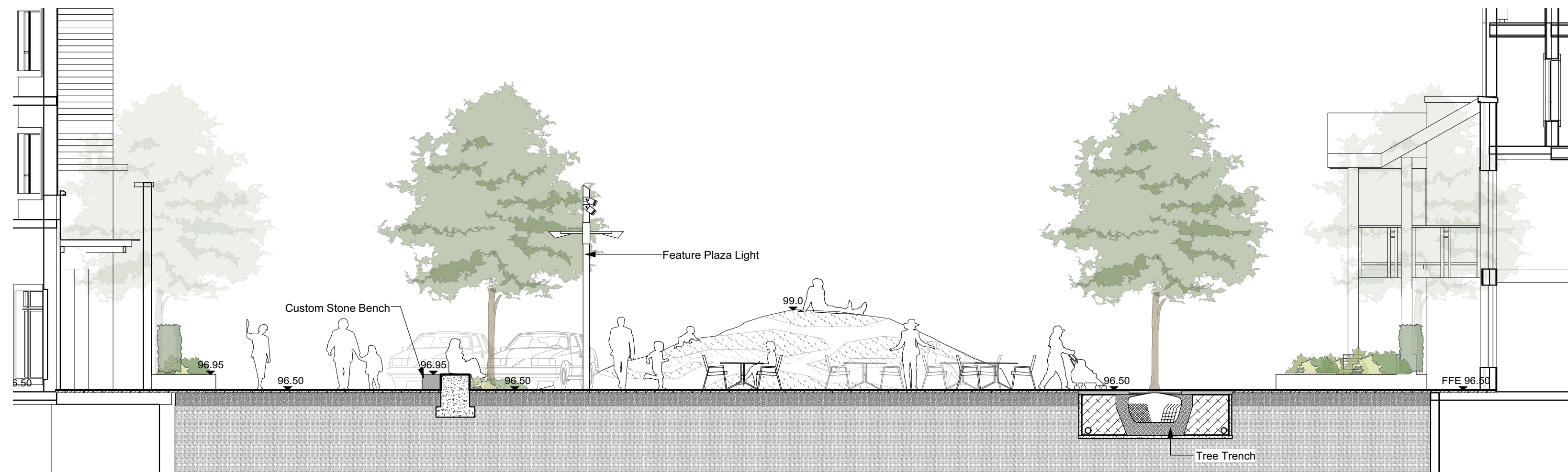
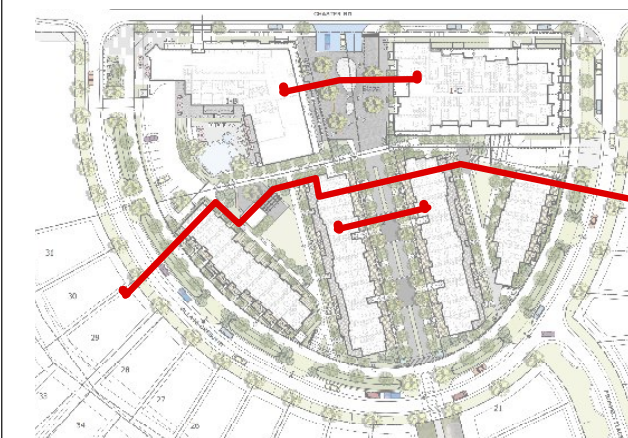
18-044

Sheet No.:

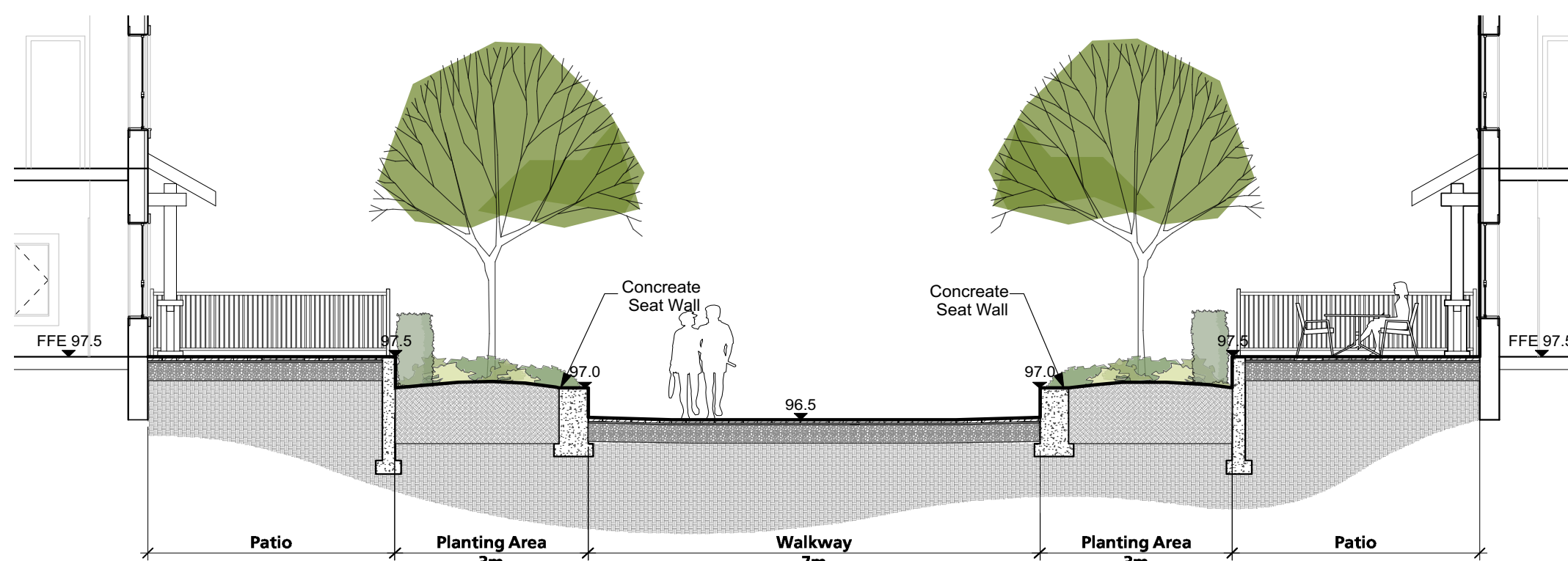
**L3.0**

Plant List	Qty.	Botanical Name	Common Name	Scheduled Size
Trees				
Auc	3	Arbutus unedo 'Compacta'	Compact Strawberry Bush	#2 pot
Az	2	Azalea japonica 'Gumpo White'	Gumpo White Azalea	#2 pot
Az	4	Azalea japonica 'Hino Crimson'	Hino Crimson Azalea	#2 pot
B	1	Berberis thunbergii 'Hana Goro'	Hana Goro Japanese Barberry	#2 pot
BmW	2	Buxus microphylla 'Winter Gem'	Winter Gem Boxwood	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
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C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
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C	2	Camellia sasanqua 'Vivian'	Vivian Camellia	#2 pot
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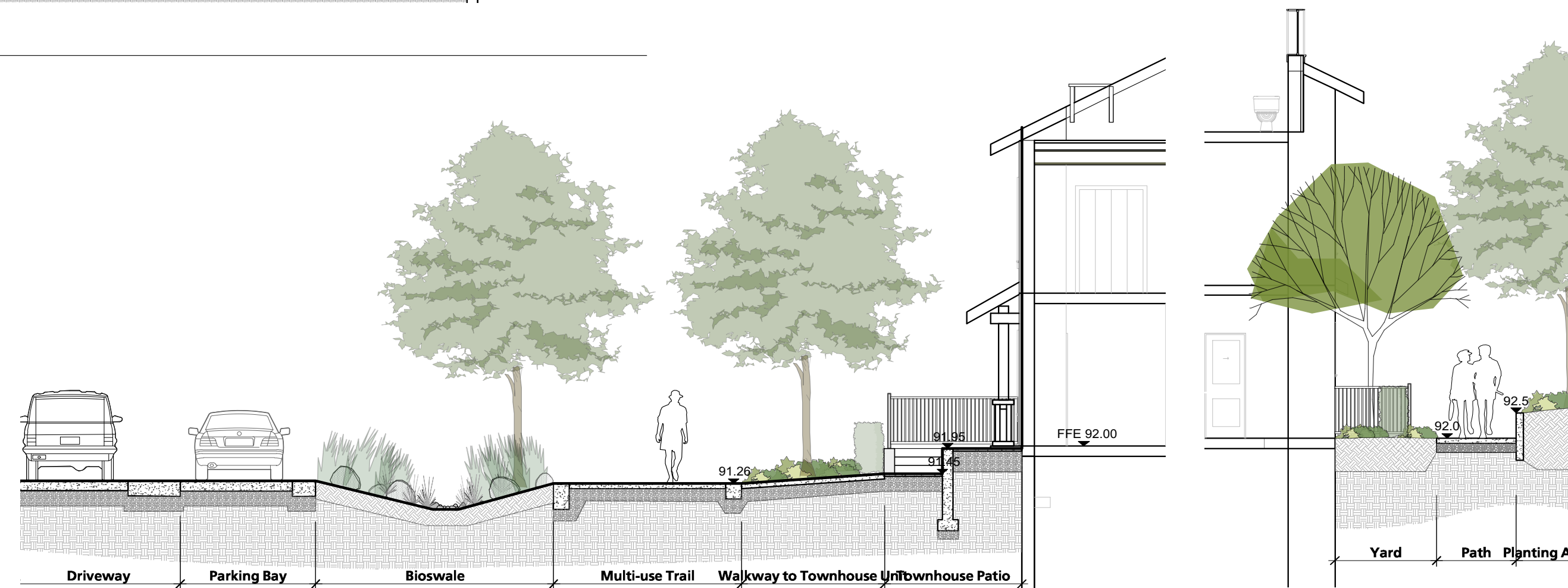




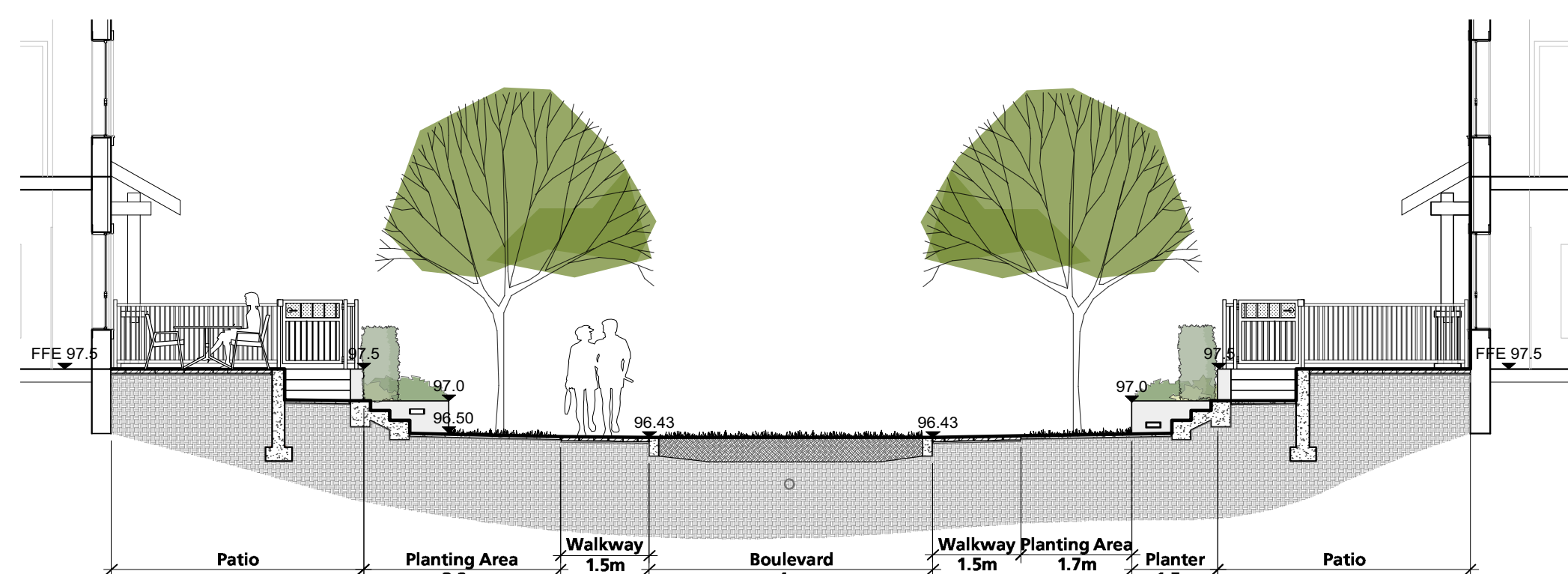
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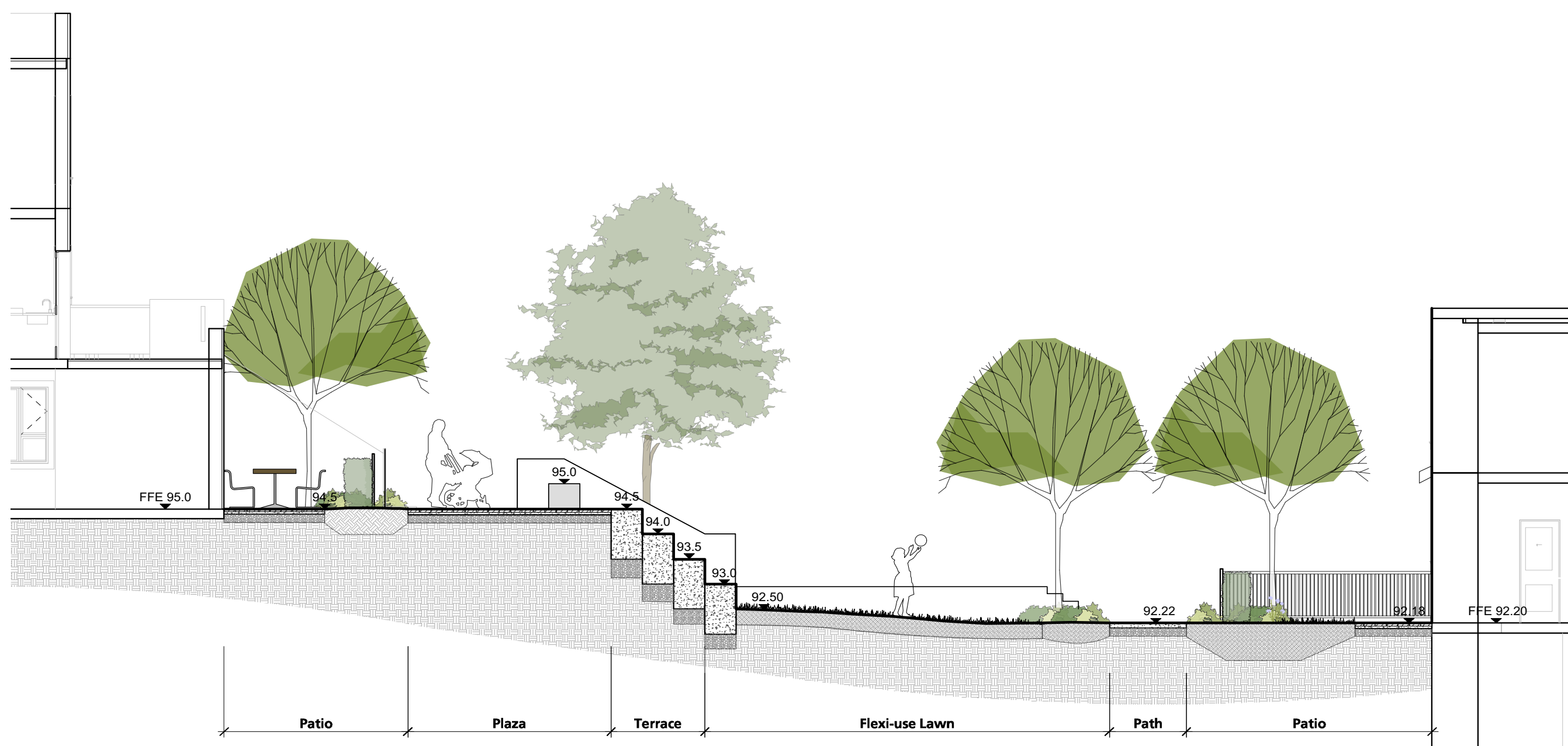
2 SECTION THROUGH TOWNHOME B&C AND CENTRAL BOULEVARD  
1:75



3 SECTION THROUGH TOWNHOME A AND BIOSWALE  
1:75



4 SECTION THROUGH TOWNHOME B&C AND CENTRAL BOULEVARD  
1:75



5 SECTION THROUGH TOWNHOME C&D AND AMENITY SPACE  
1:75

3	Re-issued for DP	2019/08/19
2	Issued for DP	2018/11/23
1	Issued for Review	2018/11/02
	Revision No.	Date

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Client:

**Greenlane Homes Ltd.**

Project Title:

**Gospel Rock Village**

Drawing Title:

**Sections**

Project North:



Drawn By:

TT

Checked By:

JM

Scale:

SEE BY DRAWING

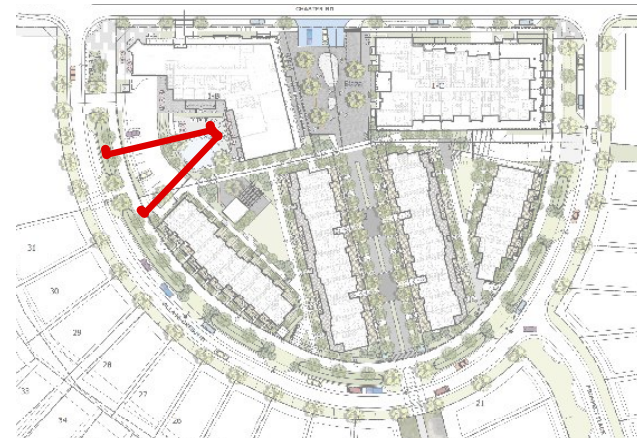
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18-044

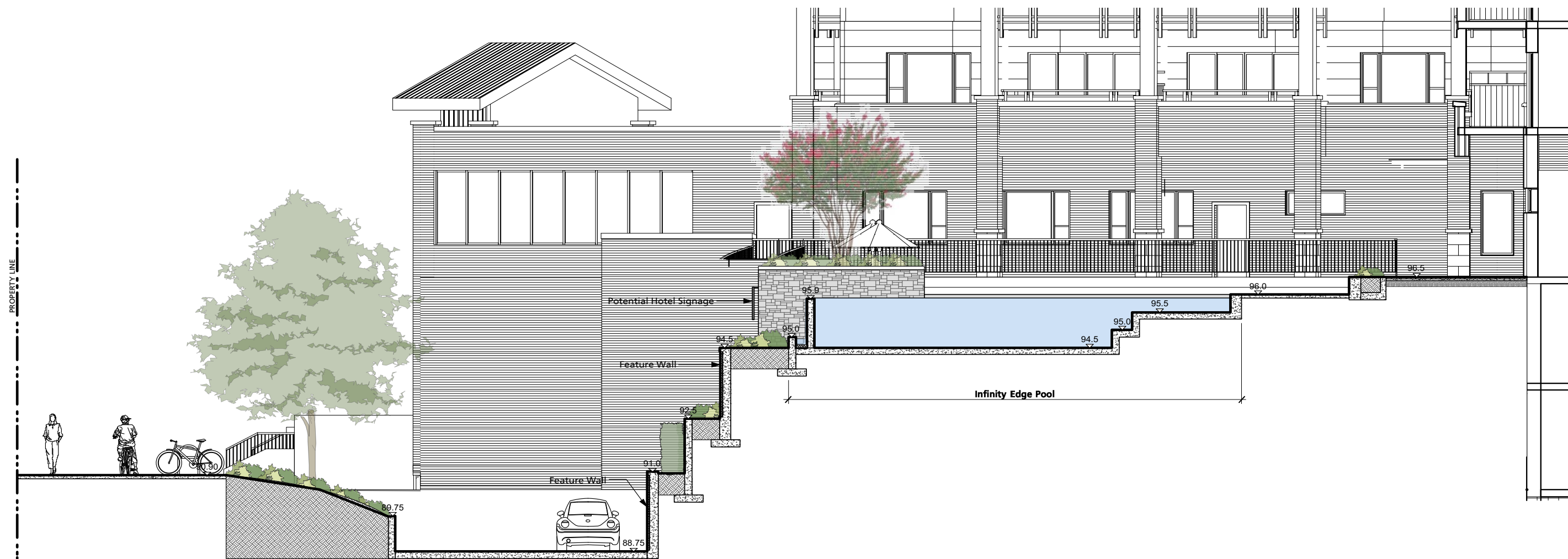
Sheet No.:

**L4.0**





6 SECTION A THROUGH RETAINING WALL AND INFINITY EDGE POOL  
1:100



7 SECTION B THROUGH RETAINING WALL AND INFINITY EDGE POOL  
1:100

3 Re-issued for DP	2019/08/19
2 Issued for DP	2018/11/23
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Client:

**Greenlane Homes Ltd.**

Project Title:

**Gospel Rock Village**

Drawing Title:

**SECTIONS**

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	Checked By:	JM
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SEE BY DRAWING	18-044	
Sheet No.:		

**L4.1**



# GOSPEL ROCK PLAZA



Feature Remnant ' Stone'



Feature Remnant ' Stone'



Plaza Paving with Trees



Round Treegrates



Unit Paving at Parking Stalls



Custom Local Stone Benches



Movable Furniture



Bike Racks



Pedestrian Scale Lighting



Plaza Uplighting

# POOL DECK AREA



Infinity Edge Pool with Feature Stone Wall



Waterfall Element



Japanese Maple



Moveable Deck Chair



Privacy Screen / Glazing



Furniture Layout to Enhance Views



Fire Pit



Lounge Chairs with Planting Behind

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Client:  
**Greenlane Homes Ltd.**

Project Title:  
**Gospel Rock Village**

Drawing Title:  
**Landscape Precedent Images**

Project North:	Drawn By:	TT
	Checked By:	JM
Scale:	Job No.:	18-044
Sheet No.:		

**L5.0**



# SITE AND GREENWAY MATERIALS AND PRECEDENTS



Terraced Planting



Rain Channel



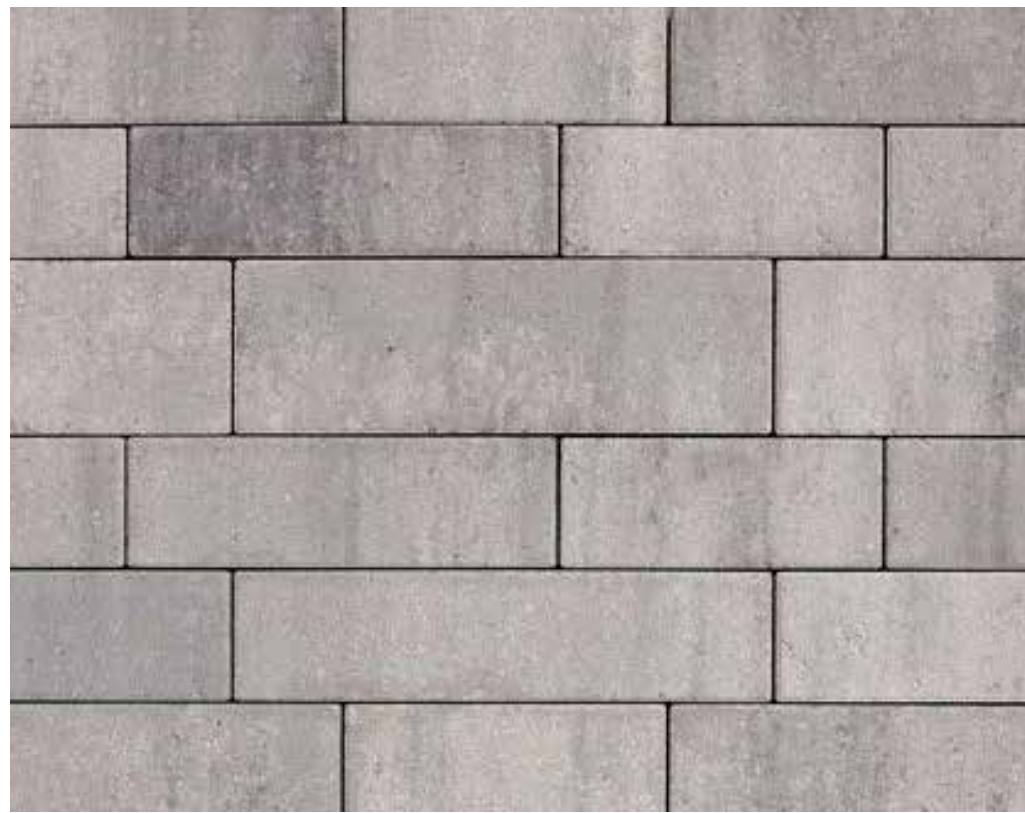
Seating Nodes with Planting



Terraced Seating



Trellis Materials



Unit Paving Pattern



Trellis Patterning



Privacy Screen Options

## PLANTING



Columnar Trees



Evergreen Planting



Evergreen Hedging



Feature Trees



Seasonal Interest



Feature Planting



Environmental / Rain Garden Planting

## LIGHTING



Feature Wall Lighting



Uplighted Trees



Paving Lights

3 Re-issued for DP	2019/08/19
2 Issued for DP	2018/11/23
1 Issued for Review	2018/11/02
Revision No.	Date

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Client:

**Greenlane  
Homes Ltd.**

Project Title:

**Gospel Rock  
Village**

Drawing Title:

**Landscape  
Precedent Images**

Project North:

Drawn By:

TT

Checked By:

JM

Scale:

Job No.:

NTS 18-044

Sheet No.:

**L5.1**





## MEMO

**TO:** Town of Gibsons  
**FROM:** Souzan Saadat, P.Eng., PTOE, Mark Merlo, P.Eng., PTOE  
**SUBJECT:** Gospel Rock Village Shared-use Parking Strategy  
**DATE:** July 17, 2019

---

## INTRODUCTION

Greenlane Homes Ltd. is proposing to develop a subdivision (Block 7) in the Town of Gibsons within the Gospel Rock Neighbourhood Plan Area. The proposed development includes a 48-unit townhouse, a 49-unit apartment, and a 50-room inn. As part of the Development Permit application, Greenlane Homes Ltd. retained WSP Canada Group Limited (WSP) to prepare a Parking Study for the proposed development. The developer is proposing to provide 238 on-site parking spots plus 30 on-street public parking spots on Village Crescent and Village High Street resulting in total 268 parking spots as shown in Attachment A. The on-street parking supply adds to the development's street life, reduces travel speeds, and improves safety outcomes.

This technical memorandum presents the parking ratios recommended for the residential and hotel land uses in this development. The recommendations are based on the results of an investigation into opportunities to further reduce the need for car ownership, and therefore parking ratios, by providing initiatives such as car sharing as well as the concept of shared parking based on variations in the accumulation of vehicles by hour of the individual land uses, and relationships among the land uses that result in visiting multiple land uses on the same trip.

## OBJECTIVES OF PARKING STANDARDS

Parking decisions affect land use and development patterns, as well as travel behaviour. Therefore, parking is a complex policy issue involving many interests and viewpoints. Historically, parking standards have been used by cities to specify the amount of parking that must be provided with new development to ensure that ample off-street spaces are provided to meet the development's own parking needs. These standards have often been developed under the approach that more parking is better. However, with a growing desire to build higher-density, compact, and pedestrian-friendly development, support urban redevelopment, and encourage non-auto modes of transportation, it is recognized that responsible parking standards can balance a variety of transportation and development objectives. In this study, parking standards are viewed as tools to help achieve the following objectives:

- Encourage transportation alternatives to the automobile; and,
- Ensure sufficient off-street parking to minimize impact to the neighbourhood.

## APPROACH TO DEVELOPING PARKING RATIOS

Parking standards are most commonly developed by either reviewing standards from other jurisdictions or from published sources determine actual parking requirements for various uses. The parking ratios presented in this study were developed from published sources, namely:

- ✓ *Town of Gibsons Zoning Bylaw No. 1065, 2007*



- ✓ *ITE Parking Generation, 4<sup>th</sup> Edition (Washington, DC: Institute of Transportation Engineers (ITE), 2010)*
- ✓ *Shared Parking, 2nd Edition (Washington, DC: Urban Land Institute (ULI), 2002)*
- ✓ *“Sunshine Coast Transit Future Plan” report prepared by BC Transit in January 2014.*

## VEHICLE PARKING REQUIREMENTS BASED ON TOWN OF GIBSONS ZONING BYLAW NO. 1065, 2007

As shown in Table 1, application of the Town’s off-street parking requirements detailed in Zoning Bylaw, 2007, No. 1065 to this development yields a requirement of 257 parking spaces. Please note that this number of required parking spaces have not taken into account the shared parking reduction required which will be described in the following sections.

Application of the Town of Gibsons off-street parking requirements detailed in Zoning Bylaw, No. 1065, 2007 to this development yields a requirement for 26 visitor and 2 disabled parking spaces. The development supplies 27 visitor parking for the townhouses and apartments in total and 2 disabled parking spaces for commercial use meeting the Town of Gibsons requirement.

*Table 1 - Town of Gibsons By-Law Parking Requirements*

Component	Number of Units / Area	By-Law Parking Ratio	Parking Requirement (Spaces)	Proposed Parking Supply (Spaces)	Required /Supplied Visitor parking <sup>2</sup>	Required /Provided Disabled Spaces
<b>Townhouse</b>	48 DU <sup>1</sup>	2 per dwelling units	96	96	15/15	-
<b>Apartment</b>	49	1.5 per dwelling units	74	70	11/12	-
<b>Retail</b>	460 m2 GFA <sup>1</sup>	1 per 45 m2 of GFA	11			
<b>Restaurant</b>	100 Seats	1 per each eight seats for an attached inn and 1 per each eight seats for consumption of alcohol for attached inn	26	66		2/2 <sup>3</sup>
<b>Tourist Accommodation Use</b>	50 rooms	1 per sleeping unit	50			
<b>Car Sharing 15 Min Limit on Plaza</b>				4 2		-
<b>TOTAL</b>			<b>257</b>	<b>238</b>		-

1 DU – dwelling unit; GFA – Gross Floor Area

2 Of the parking spaces required for a townhouse or apartment use, 15% must be available for visitor use.

3 For all commercial, industrial, entertainment, recreation and public assembly uses, parking spaces sized and marked for the exclusive use of physically disabled persons must be provided in at least 2 to the total 49-99 of accessory parking spaces required on the lot.

Based on Town of Gibsons Zoning Bylaw No. 1065, 2007 Section 605, where the peak use of parking spaces for two or more uses on the same lot or adjacent lots occurs at different periods of time, and accessory required parking



for such use is shared, the required number of parking spaces required for such use in total may be reduced by no more than 25%. As further elaborated in “Shared Parking” section shown below, this results in 25% hotel/restaurant/retail parking reduction requiring only 66 parking spots for the hotel building and leading to total 236 parking stalls required for the proposed hotel and residential developments. The 238 parking spots provided by Greenlane Homes Ltd. meet the required 236 parking spots as shown in Table 2.

*Table 2 - Adjusted Town of Gibsons By-Law – Vehicle Parking Requirements*

Component	Parking Requirement (Spaces)	Adjustment based on Zoning Bylaw No 1065, Section 605	Required Parking (Spaces)	Proposed Parking Supply (Spaces)
Townhouse	96		96	96
Apartment	74		74	70
Retail	11	Parking spaces required for shared parking use in total may be reduced by no more than 25%	66	66
Restaurant	26			
Tourist Accommodation Use	50			
Car Sharing				4
15 Min Limit on Plaza				2
<b>TOTAL</b>	<b>257</b>		<b>236</b>	<b>238</b>

Notes: DU – dwelling unit; GFA – Gross Floor Area

## BICYCLE PARKING REQUIREMENTS BASED ON TOWN OF GIBSONS ZONING BYLAW NO. 1065, 2007

Two types of bicycle parking facilities are to be provided for each project including Class 1 and Class 2. Long Term (Class 1) parking is intended for long-term use and may consist of attended facilities, racks in an enclosed and lockable room, indoor or outdoor bicycle lockers, or restricted-access parking facilities. Short Term (Class 2) parking is intended for short-term use and should consist of racks located in an accessible outside location, protected from weather. As shown in Table 3, application of the Town of Gibsons’ bicycle parking requirements detailed in Zoning Bylaw, 1065, No. 2007 to this development yields a requirement for 136 Class 1 and 35 Class 2 bicycle parking spaces. This finding shows the proposed number of bicycle spaces for Class 1(136 spaces) is consistent with the Bylaw requirement. However, the provided Class 2 bicycle spaces (34 spaces) is less than the Town of Gibbons’ requirement.

*Table 3 - Town of Gibsons By-Law - Bicycle Parking Requirements*

Component	Number of Units / Area	By-Law Parking Ratio	Parking Requirement (Spaces)	Proposed Parking Supply (Spaces)
Apartment/Townhouse	97 DU	Class 1- 1.25 per dwelling units	122	127

		Class 2 – 0.2 per dwelling units	20	20
Tourist Accommodation	4470 sq.m.	Class 1 - 0.27 per 100 sq.m.	12	Class 1 -14 Class 2 - 20
		Class 2 - 0.27 per 100 sq.m.	12	
Retail, Office and Service Commercial	735 sq.m.	Class 1 - 0.27 per 100 sq.m.	2	
		Class 2 - 0.4 per 100 sq.m.	3	
TOTAL		Class 1	136	141
		Class 2	35	40

## PARKING REQUIREMENTS BASED ON ITE PARKING GENERATION, 5TH EDITION

This section determines whether the proposed off-street parking supply in the Gospel Rock Village area during peak parking occupancy hours can accommodate the parking demand generated by the proposed development based on the ITE Parking Generation, 5th Edition, (ITE: Washington, DC, 2010). Findings of the peak parking demand study are summarized in Tables 4. According to the Town of Gibsons, the nature of this project resembles a resort destination. Consequently, the proposed site's parking demand was estimated based on the "Resort Hotel" land use code in ITE Parking Generation, 5th Edition, (ITE: Washington, DC, 2010). Key finding is that the total parking demand by the proposed site during peak parking occupancy hours during the weekday is expected to be 217 spaces based on the Parking Generation Manual, 5<sup>th</sup> Edition assuming no nearby transit is available. Therefore, the 231 off-street parking supply is expected to meet the demand.

Table 4 – Average Peak Period Parking Demand on a Weekday based on ITE Parking Generation Requirements

Component	Number of Units / Area	ITE Parking Ratio	Parking Requirement (Spaces)	Proposed Off-Street Parking Supply (Spaces)
Mid-rise Multifamily Housing	97 DU*	1.31 vehicles per dwelling units**	127	165
Resort including Retail and Restaurant	50	1.79 parking demand per room	90	66
TOTAL			217	231

\* DU – dwelling unit; GFA – Gross Floor Area

\*\*No nearby transit

## OPPORTUNITIES TO FURTHER REDUCE THE PARKING SUPPLY

There are a number of land-use bylaws, policy reviews and documents across Canada support a large reduction in parking requirements. These parking policies are typically introduced with the intention of lowering vehicle usage or to reflect lower parking demand.





Reducing parking supply and efficiently managing parking provided would greatly benefit the Town of Gibsons in the long-term. Accommodating the expected future regional growth by incorporation high density land-use, sustainable modes of transportation, and reduced parking requirements will encourage non-auto transportation modes. On the other hand, parking induces further driving. Automobile dependency increases driving and road congestion. Therefore, there is a range of incentives for reducing parking demands for this and other similar developments.

There are workable solutions for managing parking such as:

- Providing on-site secure cycling storage;
- Regulating on-street parking to reduce unnecessary searching for free parking space;
- Shared parking;
- Car sharing programs; and,
- Public transit growth.

The first two strategies have already been incorporated into the project, namely the provision of 141 bicycle rack space for the long term parking and more than 40 bicycle spaces for short term parking spaces and the 30 on-street parking spots. Opportunities for shared parking and car sharing programs are described below.

## SHARED PARKING

As described in the Shared Parking Manual, 2nd Edition (Washington, DC: Urban Land Institute, 2005), shared parking is the use of a parking space for two or more individual land uses, without conflict or encroachment. The ability to share parking spaces is the result of two conditions:

- ✓ *Variations in the accumulation of vehicles by hour, by day, or by season of the individual land uses; and*
- ✓ *Relationships among the land uses that result in visiting multiple land uses on the same auto trip.*

Gospel Rock Village meets both conditions. Residential visitors and hotel guests to the site could also be customers and perhaps even employees of the retail and restaurant component. Table 5 shows the peak demand for the Gospel Rock Village is expected to be at around 8-9 p.m. with 238 parking spots including 70 parking spots for the hotel, restaurant and retail. The demand for the 70 parking spots is expected to be less for these mixed land uses, as the hotel guests are expected to be the customers of both restaurant and retail. This trip-sharing reduces the need for parking.

*Table 5 - Parking Demand Requirements based on Shared Parking, 2<sup>nd</sup> Edition*

Land Use	Time of Day Factor																		
	6AM	7AM	8AM	9AM	10AM	11AM	12PM	1PM	2PM	3PM	4PM	5PM	6PM	7PM	8PM	9PM	10PM	11PM	12AM
Retail	1%	5%	15%	35%	65%	85%	95%	100%	95%	90%	90%	95%	95%	95%	80%	50%(5)	30%	10%	-
Restaurant	0%	10%	30%	10%	10%	5%	100%	100%	33%	10%	10%	30%	55%	60%	70%	67%(17)	60%	40%	30%
Hotel	95%	95%	90%	80%	70%	70%	65%	65%	70%	70%	75%	80%	85%	85%	90%	95%(48)	95%	100%	100%
Residential	100%	90%	85%	80%	75%	70%	65%	70%	70%	70%	75%	85%	90%	97%	98%	99%(168)	100%	100%	100%
Total Demand	194	204	199	182	172	164	178	187	172	166	177	202	219	232	237	238	236	231	228



## CAR-SHARE PROGRAM

Car-sharing is the practice where a number of people share the use of one or more cars that are owned by a profit or non-profit car sharing organization. To use a shared vehicle, a person must meet the membership requirements of the car-sharing organization, including the payment of a membership fee that may or may not be refundable. Cars are reserved in advance and fees for use are normally based on time and miles driven. Car-sharing organizations are typically residentially based with cars parked for convenient access within the area of the membership served by the organization.

In Metro Vancouver municipalities, developers have been able to reduce the number of parking spaces required by zoning bylaws when Shared Vehicles and Shared Vehicle Parking Spaces are provided as part of the form of development. For instance, the City of Vancouver allows for the substitution of shared vehicles and shared vehicle parking spaces for required parking spaces for secured market rental housing not Downtown, at a 1:5 ratio, to a maximum of 4 shared vehicles and 4 shared parking spaces for each 100 dwelling units. For the 97 rental units of Gospel Rock Village, this represents a reduction of 20 parking spaces if 4 shared vehicles and 4 shared parking spaces were provided.

Consequently, there is an opportunity to reduce the parking stalls required by Town of Gibsons to 237 parking spaces (= 146 resident parking spaces + 87 non-resident parking spaces + 4 shared parking spaces) if 4 shared vehicles and 4 shared parking spaces were provided.

## COAST CAR CO-OP

WSP followed up with Coast Car Co-op company about their willingness to use the 4 car share stalls for their program. Coast Car Co-op board expressed their interest and added that they have 5 vehicles right now.

Coast Car Co-op is building a strong membership and community support for the co-op, and they have the systems in place to manage shared vehicles professionally. Coast Car Co-op would be happy to keep dialogue open with Greenlane Homes Ltd. as they work on their development.

## PUBLIC TRANSIT GROWTH FUTURE IMPACT

Based on the “Sunshine Coast Transit Future Plan” report prepared by BC Transit in January 2014, a 25-year target of 1,800,000 rides per year by 2038 has been set for the Transit Future Plan, assuming continued growth. The transit mode share for all annual trips on the Sunshine Coast is estimated at 2% in 2011, assuming 2.9 trips per person per day. If future population and ridership targets are realized, assuming a population forecast of 36,900 people, the Sunshine Coast would have a 25-year transit mode share of 5.4 % by 2038.

This target is aimed to be achieved through the following implementation strategies within the town of Gibsons:

- Short Term Implementation Priorities (0-3 years)
  - a Begin to develop the Future Frequent Transit Network (FTN) - Increase frequency between Sechelt, Gibsons and Langdale Ferry Terminal to 30 minute service at peak times
  - b Develop the Local Transit Network (LTN) - Provide hourly local community bus service with smaller transit vehicles to the eastern Sunshine Coast communities (including Gibsons, Elphinstone and areas of Howe Sound) that feeds into the Frequent Transit Network
  - c Develop an exchange in upper Gibsons
- Medium Term Implementation Priorities (4-6 years)
  - a Develop a Park & Ride facility in Gibsons



- b Explore the cost benefits of providing a satellite operating facility in Wilson Creek to support local community bus service in the eastern Sunshine Coast communities (including Gibsons, Elphinstone and areas of Howe Sound)
- Long Term Implementation Priorities (7+ years)
  - a Increase the frequency to Lower Gibsons to 30 minute service all day
  - b Provide hourly two-way service on Gower Point Road in Lower Gibsons and Elphinstone

According to the “Sunshine Coast Transit Future Plan” report prepared by BC Transit in January 2014, the transit ridership is expected to increase from 2% from 2011 to 3.43 % in 2025. This results in the number of single occupancy vehicles reduction on the road, and thereby vehicle ownership and parking requirement reduction. However, since the proximity of the future bus stops to the proposed development is not determined at this stage and the 30-minute frequency service provision in the long term is not considered significant enough, parking variance due to transit impacts is not warranted for the purpose of this study.

## SUMMARY

- The developer is proposing to provide **232** off-street parking spots plus 4 car sharing spots and two (2) 15-minute limit parking stalls on plaza resulting in a total of **238** parking spots.
- Application of the Town of Gibsons off-street parking requirements detailed in Zoning Bylaw No. 1065, 2007 Section 605 to this development yields a requirement for **236** parking spaces employing the Shared Parking Zoning bylaw No. 605.
- Based on car-sharing program, there is also an opportunity to reduce the parking supply required by Town of Gibsons to **237** parking spaces (=146 resident parking spaces + 87 non-resident parking spaces + 4 shared parking spaces)) if 4 shared vehicles and 4 shared parking spaces were provided. Coast Car Co-op expressed their willingness to contribute in this car sharing program.
- Based on Shared Parking Manual, 2nd Edition (Washington, DC: Urban Land Institute, 2005), the total parking demand by the proposed site during peak parking occupancy hours during the weekday is expected to be **238** spaces. The demand for the 238 parking spots is expected to be less for these mixed land uses, as the hotel guests are expected to be the customers of both restaurant and retail. This trip-sharing reduces the need for parking.
- The total parking demand by the proposed site during peak parking occupancy hours during the weekday is expected to be 217 spaces based on the Parking Generation Manual, 5th Edition assuming no nearby transit is available.
- Application of the Town of Gibsons’ bicycle parking requirements to this development yields a requirement for 136 Class 1 and 35 Class 2 bicycle parking spaces. This finding shows the supplied number of bicycle spaces for Class 1(141 spaces) and Class 2 (40 spaces) are beyond the Bylaw requirement.
- Public transit growth is not expected to have significant impact to grant a parking variance.

## CONCLUSION

According to various strategies reviewed, parking provision of a minimum of 236 stalls for the proposed development with the car-sharing program is expected to meet the demand during the peak hours. The developer is proposing to provide 238 parking stalls including 4 car sharing spots and two (2) 15-minute limit parking spots on plaza.



## ATTACHMENT A



Gospel Rock Village Project  
Parking Calculation - Village Center

July 16, 2019

Class of Use		Units	Required as per Zoning By-Law		Provided	Required H.C Parking	Provided H.C Parking	Required Off- Street Loading Space	Provided Off-Street Loading Space
Inn	Accommodation Use	50	1 per sleeping unit	50					
	Attached Restaurant Use	100	1 per each 8 seats	13					
	Restaurant with Alcohol License (Additional)	100	1 per each 8 seats	13					
	Retail, Office and Service Commercial use	460	1 per 45 sq.m	11					
Subtotal				87	66	2	2	1	1
Apartment	Live and Work	4	1.5 per unit	6					
	3 Bedroom	8	1.5 per unit	12					
	2 Bedroom	30	1.5 per unit	45					
	1 Bedroom	7	1.5 per unit	11					
Subtotal				74	70	N.A	N.A	1	1
Townhouse	Dwelling	48	2 per dwelling unit	96					
Subtotal				96	96			0	0
Plaza	Passenger Parking (15 min. Time Limt)				2				
	Car Share				4				
Total				257	238	2	2	2	2
Street Parking (Public)	Village Crescent				22				
	Village High Street				8				
Subtotal					30				
Total				257	268				

Gospel Rock Village Project  
Bicycle Parking Calculation - Village Center

July 9, 2019

Class of Use			Bylaw	Units/Area	Required Bicycle Parking Space	Proposed Bicycle Parking Space
Inn	Tourist Accommodation	Class 1	0.27 spaces per each 100 sqm	4470	12	
		Class 2	0.27 spaces per each 100 sqm		12	
	Retail, Office, Service Commerical and Restaurant	Class 1	0.27 spaces per each 100 sqm	735	2	
		Class 2	0.4 spaces per each 100 sqm		3	
Subtotal					29	
Class 1					14	14
Class 2					15	20
Apartment		Class 1	1.25 spaces per dwelling unit	49	62	62
		Class 2	0.2 spaces per dwelling unit		10	10
Townhouse		Class 1	1.25 spaces per dwelling unit	48	60	65
		Class 2	0.2 spaces per dwelling unit		10	10
Total						
Class 1					136	141
Class 2					35	40





# STAFF REPORT

**TO:** Planning and Development Committee    **MEETING DATE:** September 17, 2019  
**FROM:** Kirsten Rawkins  
Planning Assistant    **FILE NO:** 3220-School Rd-749  
**SUBJECT:** Development Permit for Form and Character (DPA 4) for 749 School Road

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## RECOMMENDATIONS

**THAT** the report titled Development Permit for Form and Character (DPA 4) for 749 School Road be received;

**AND THAT** the Planning and Development Committee recommends issuance of DP-2019-12 for 749 School Road, subject to the adoption of amendment bylaws 985-23, 2019 and 1065-49, 2019.

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## BACKGROUND / PURPOSE

In conjunction with the Zoning and OCP amendment applications already before Council, the Town of Gibsons has received an application from the BC Housing Management Commission for a Development Permit for the form and character of its proposed 3-storey, 40-residence supportive housing facility at the site of the former Gibsons RCMP detachment site at 749 School Road.



*Figure 1 – Proposed 40-unit modular supportive housing building and landscape as seen from O'Shea Road*

The Official Community Plan and the OCP amendment designate the property in Development Permit Area 4 (DPA 4) – Multi-Unit Residential. The Multi-Unit Residential Development Permit Area is designated under Section 488(1)(f) of the Local Government Act to establish objectives for the form and character of multi-family development. Form and Character guidelines apply to all buildings and structures within a DPA and are evaluated with consideration to the intended use and project scope and context.

The purpose of this report is to review the proposed building design in relation to the Development Permit Area 4 (DPA 4) guidelines and obtain a recommendation from the Planning and Development Committee on next steps. The draft permit is attached as Attachment H.

Development plans, site plan, landscape plan, lighting plan and accessory structure design detail drawings are enclosed as Attachments A through E.

## DISCUSSION

Per the OCP, the purpose of the multi-unit residential DPA is to ensure that a high standard of design, landscaping and building form is implemented for any multi-unit residential development. The guidelines are aimed at ensuring that new development is appropriate to its surroundings, is compatible with surrounding uses or neighbourhood character and is attractive for future residents.

Through the framework of the DPA 4 guidelines, the form and character of the multi-unit development is considered with respect to the context of both existing land uses and future land uses as envisioned in the Official Community Plan.

### *Zoning and Current Land Use Context*

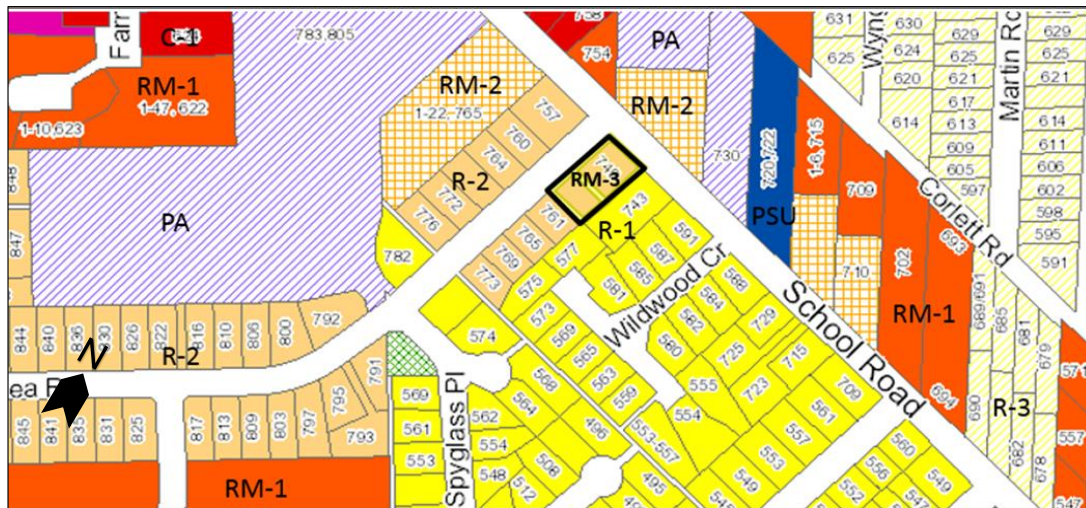


Figure 2 - Zoning context map



To the south-west of the site, along O'Shea and Wildwood Roads, current land use is largely single family residential (R-1 and R-2 zones) as reflected in Figure 2, while the School Road corridor has a mix of uses including single family homes directly uphill and downhill of the site, a three-storey, 22-unit town-house development two lots uphill of the site toward Gibsons Elementary School, as indicated in orange cross-hatching for its multi-family land use (RM-2 zone), and Public Assembly (PA) uses, including the Legion and Christian Life Assembly Church, opposite the site on School Road.

Directly across School Road, flanked by the Royal Canadian Legion and church is a property currently used as a parking lot and zoned for "High Density" (OCP) Multi-Family Residential use.

Uphill properties across School Road include two single family residences (zoned for multi-family and commercial use) along with commercial uses including a law office and Kern's Plaza.

Zoning downhill of the church and legion include the Town's public works yard followed by multifamily residential apartment uses, some of which are in construction stages.

The form and massing of the proposal is evaluated with respect the zoning requirements of the RM-3 zone in the following table:

**ZONING REQUIREMENTS**

	<b>BYLAW (RM-3)</b>	<b>PROPOSED</b>	<b>MET? Y/N</b>
SCHOOL RD. SETBACK (FRONT)	4 m	4.11 m	Y
SOUTH SIDE SETBACK	3.5 m	6.10 m	Y
O'SHEA EXTERIOR SIDE SETBACK	3.5 m	3.51 m	Y
REAR SETBACK (SW)	7.5 m	13.11 m	Y
HEIGHT	11.5 m	11.46 m	Y
LOT COVERAGE	75%	70%	Y
FLOOR SPACE RATIO	1.2	0.8	Y

*Future Development Context - Official Community Plan*

The Land Use Plan of the Official Community Plan is part of the Town's strategic framework and identifies the type of future land use that is desired for a location. As shown in Figure 3, an excerpt from the OCP Land Use Map, the majority of the School Road corridor, including the subject lot adjacent to School Road is designated by the OCP for Medium Density Residential use, as shown in dotted yellow. The intent of the *Medium Density Residential* designation is "to permit townhouses, stacked townhouses and 2 to 4 storey apartments with a FSR of 0.7 to a maximum of 1.2 (generally between 40-75 units per hectare)." The proposed development has 3 storeys and an FSR of 0.84.

Future uses of neighbouring properties, as envisioned through the OCP, include multi-family uses up and down the School Road Corridor, high density residential use directly across School Road to the east and continued detached residential to the south and west along O'Shea Road.

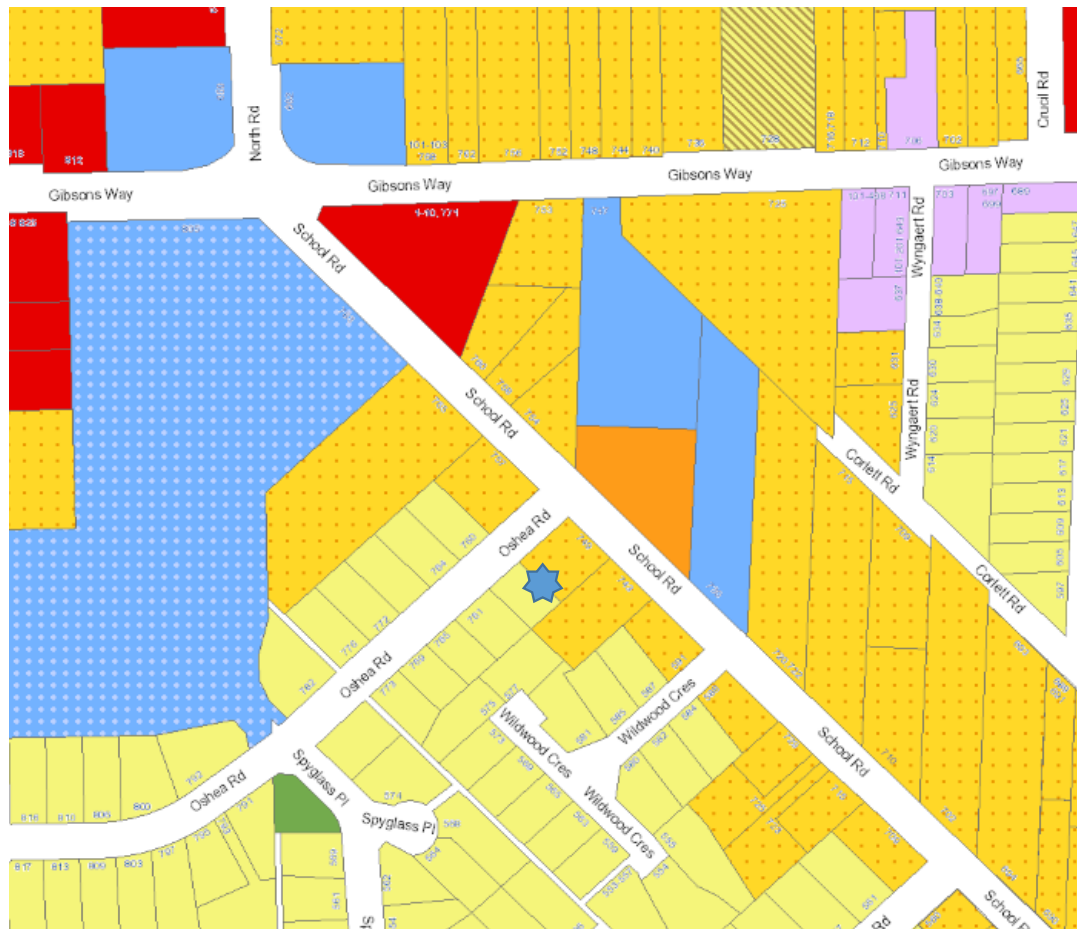


Figure 3 - OCP Land Use Map; Subject property indicated with blue star; Dotted yellow indicates Medium Density Residential; solid light yellow indicates Detached Residential; Orange indicates High Density Residential; Red indicates Mixed Use Commercial and Blue indicated Public/ Community Uses.

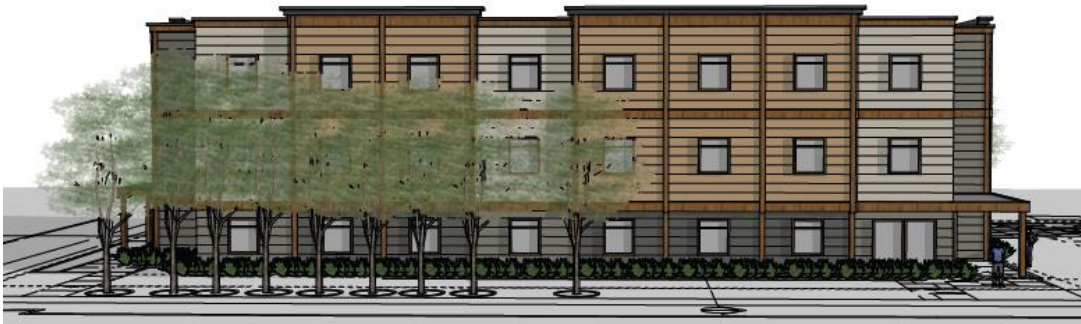


*Form and Character Evaluation per DPA Guidelines*

The figures below show renderings of the building:



*Figure 4: View from corner of School Road and O'Shea Road, showing School Road exit*


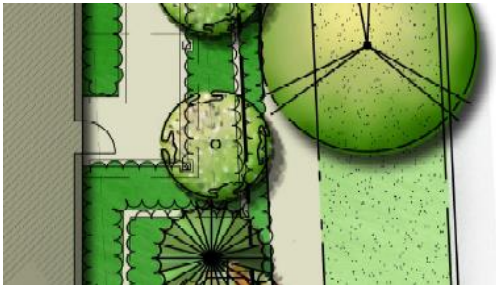


*Figure 5: View from O'Shea Road at side of property*






*Figure 6: View from O'Shea Rad at rear of property showing main building entrance*

The following table reviews the proposed design in relation to the DPA 4 form and character guidelines. A response to the DPA4 guidelines by the project architect is also attached to this report for reference as Attachment F.


DPA 4 Guidelines	Staff Evaluation – is criteria met?
Public street edges which are characterized by low (less than 3.5 feet high), neighbourly fences, combined with extensive landscape materials at the private edge.	<b>Yes</b> - The landscape plan maintains and unfenced public street edge buffered by landscape materials with fencing and landscape screening at the private edge.
Residences oriented towards the street with well-defined and welcoming entries at the street edge.	<p><b>Yes/No</b> - The primary entrance is oriented to O'Shea Road with a secondary exit to School Road. Both entrances are potentially appealing and framed with a covered front porch, though detail drawings would be required for staff to evaluate finished effect.</p>  <p><i>Figure 7: School Road (secondary) entrance</i></p> <p><i>Placement of landscape materials reflects a priority of screening rather than welcoming emphasis.</i></p>  <p><i>Figure 8: Landscaping around School Road entrance</i></p>




DPA 4 Guidelines	Staff Evaluation – is criteria met?
Construction materials should reflect the West Coast Design and setting.	<b>Yes</b> - Cladding Materials are wood-look cementitious siding in a 'west-coast inspired' palette of slates, chestnut wood grain and neutrals (see samples below).
Simple exterior detailing with earth-tone colours – and primary colours only as accents.	<p><b>Yes</b> – Detailing is simple with earth-tone colours</p>  <p>Cladding Colours (approximate to be product matched)</p>
Buildings should be oriented to maximize solar exposure while minimizing shadow impacts on adjacent buildings and common areas.	<b>Yes</b> – All units have windows oriented for natural light; half of the units are oriented to the south-east with significant solar exposure while half of the units are oriented to the north-west, with minimal evening solar exposure. Common kitchen, dining and outdoor spaces are all oriented for southern exposure. Common areas are not impacted by shadows from building and natural grade minimizes shadow impact on uphill neighbours to north. Existing trees on site and adjacent to the site are likely to cast more impactful shade than the building.

DPA 4 Guidelines	Staff Evaluation – is criteria met?
<p>Common building elements which include:</p> <ul style="list-style-type: none"> <li>pitched roof line</li> </ul>	<p><b>No</b> - flat roof proposed, as a pitched roof on top of modular construction would add significant height to the building without gain in units.</p>  <p><i>Figure 9: Proposed modulated parapets</i></p> <p>The applicant also submitted a pitched parapet detail proposal in response to early feedback, however staff determined that the small parapets proposed were less appealing in their relative scale with the building than a modulated flat roofline, and would increase the building height by 1 metre.</p>  <p><i>Figure 10: Early draft with 'pitched' parapets</i></p>
<ul style="list-style-type: none"> <li>dormers</li> </ul>	<p><b>None.</b></p>
<ul style="list-style-type: none"> <li>porches</li> </ul>	<p><b>Yes/No</b> – Provided at main entrances; common outdoor space for tenant use. Individual porches add cost to project and may present neighbourhood privacy impacts and are therefore not included.</p>



DPA 4 Guidelines	Staff Evaluation – is criteria met?
<ul style="list-style-type: none"> <li>low building profiles, simple residences, set well back from the roadway and nestled into the landscape</li> </ul>	<p><b>Yes/No</b> – The building is larger than adjacent single family uses; height is consistent with existing and proposed multi-family uses in the School Road corridor, and the profile is constrained to three stories in keeping with OCP land use designations.</p> <p>Setbacks are in keeping with existing setbacks in the neighbourhood.</p> <p>Building design is simple and effort is made to buffer building with planting and to retain trees and outdoor spaces on the remainder of the limited site.</p>  <p><i>Figure 11: Landscape plan shows strategic siting and ample landscape buffering</i></p>
<p>The inclusion of elements such as bay windows, dormers, porches and cross gables help mitigate the visual impact of larger buildings.</p>	<p><b>Yes</b> – Façade detailing, siding colour variation and modulation of roofline used to break up visual mass of building</p>
<p>Steeper roof pitches and stepping down of roof lines to vary the height and rooflines of buildings is recommended.</p>	<p><b>Yes/No</b> – Roofline is flat, but modulated vertically with variation in roofline detailing.</p>

DPA 4 Guidelines	Staff Evaluation – is criteria met?
<p>Offsetting and modulating wall lines along the building elevation to allow smaller building sections to stand out. The overall building footprints on the site shall be modulated to avoid monotony and repetition and to avoid wall-like massing.</p>	<p><b>Yes</b> – Building ends/corners are modulated (stepped) by offsetting building end units. Further modulation along the length of the building is achieved by varying siding colour to highlight smaller building sections and break the massing by appearance.</p>  <p><i>Figure 12: View from north (School Road)</i></p>
<p>The general character of the development should reflect aspects of Gibsons' semi-rural coastal setting by using natural and typical local wall materials including wood siding, wood shingles, stucco, stone and brick.</p>	<p><b>Yes</b> – Staff feels that the intent of this guideline is met with durable wood-grained cementitious siding and trim that mimics native west coast materials.</p>
<p>The number of materials used on the building exterior must achieve a balance between achieving visual interest and complexity without overpowering the surroundings.</p>	<p><b>Yes</b> – Detailing strikes a balance of creating interest and breaking up the visual mass of the building through colour blocking and trim detailing while maintaining harmony through a limited palette of neutral-toned siding and trim materials.</p>
<p>Larger developments should be separated into smaller groups or clusters of units to promote a sense of belonging and neighbourliness and to maintain a residential scale and image.</p>	<p><b>No/Yes</b> – All units contained in a single building. Detailing designed to give the impression of clustered units from the street.</p>
<p>Very large single buildings more than 70metres in length, or townhomes with more than six joined units are to be avoided.</p>	<p><b>Yes</b> – building is 35 m in length.</p>




**Staff Report to Planning and Development Committee - September 17, 2019**


**Development Permit for Form and Character (DPA 4) for 749 School Road Page 11 of 21**

<b>DPA 4 Guidelines</b>	<b>Staff Evaluation – is criteria met?</b>
The roof form should have a sloped appearance large areas of flat roof will not be acceptable in low or medium density multiple unit residential developments except in the case of a green (vegetated) roof.	<b>No</b> – The roof is not sloped. Sloping the roof would add extra height to the building. To address this intent, the profile is broken up with variations in the height of the roof façade as described above.
The roof form should be modulated and broken up with dormers, skylights and other architectural features.	<b>Yes</b> – as above.
A continuous unbroken ridge line should be avoided.	<b>Yes</b> – as above.
Roof lines should include steep pitches typical of west coast building forms.	<b>No</b> – roof is not pitched.
Secondary hipped or gabled roofs are preferable to flat roofs or mansard roofs, or segments of pitched roofs applied to the building's edge.	<b>No</b> – as above.
Roofing materials may be metal, cedar shakes, concrete tiles or asphalt shingles.	<b>n/a</b> – as not visible. Parapets are wood-look.
New residential buildings should not in general, be much larger than the surrounding buildings. A graded transition in the building height is desired to ensure adjacent properties are not confronted with a “wall”.  Additional setbacks may also be required to achieve this transition.	<b>Yes/No</b> - To the west, the neighbouring use is currently and is envisioned to remain a single family dwelling. The proposed building is sited to be set back 43' and buffered with plantings from the west side property line to provide a buffer to this transition in height and density.  Properties to the north and south are occupied by two- and one-storey single family dwellings respectively. Both properties are envisioned in the OCP to accommodate medium density residential use in the long term, which allows buildings of 2-4 stories. Impacts on these neighbours in the shorter term are considered in the design and buffering with trees and landscaping are proposed.  The size of the site, desired number of units, and 11.5 m height maximum limit the opportunity to further increase setbacks or step building back vertically.


DPA 4 Guidelines	Staff Evaluation – is criteria met?
<p>New developments should reflect elements of the existing neighbourhood and the prevailing residential streetscape. This may require recessing of parking areas, creation of gabled entries or porches, and highlighting individual front door entries to be similar to those on neighbouring lots.</p>	<p><b>Yes</b> - The existing neighbourhood has a range of development styles and types as illustrated through the examples below. Siting, setbacks and design for the proposed building reflect existing development and future buildout of the school road corridor as envisioned in the OCP while attempting to mitigate present impacts on neighbours through appropriate buffering, setbacks and limits to the height of the building.</p>  <p><i>Figure 13: examples of existing and future (bottom left, 710 School Road) development as seen from School Road (top 4 images) and O'Shea Road (bottom right)</i></p>
<p>Roof lines should be stepped down from building ends to reduce the apparent mass of the building.</p>	<p><b>No</b> – building not vertically stepped. Horizontal trim along each floor and colour blocking is intended to visually break massing.</p>



DPA 4 Guidelines	Staff Evaluation – is criteria met?
The end units of new developments at road edges should not be more than one to two stories in height to establish a single-detached residential appearance in multi-unit residences.	<b>No</b> – The end of the unit on School Road is 3 stories in height, with a porch roof at the first storey to emphasize the pedestrian entrance/exit.
All lots must have direct access to the larger pedestrian circulation system via park corridors, pathways, and/or sidewalks.	<b>Yes</b> - The lot is directly connected to Primary pedestrian routes (blue) and cycling routes (red) on School and O'Shea Road as shown in the <i>Trail and Cycling Network</i> map that is part of the Official Community Plan: 
Developments on sloped properties should be terraced with the natural slope of the land, and should avoid the use of high [over 1.2 m (4 feet)] retaining walls.	<b>Yes</b> – no retaining structures proposed. Building site is relatively flat.
The design of developments into smaller areas where residents share smaller parking areas, pathways and other common areas creates a sense of belonging within a larger development.	<b>Yes</b> – Common indoor and outdoor community spaces are proposed with shared bike parking, seating and other facilities; additional outdoor seating or garden space may be desirable.
Multi-unit homes should provide a street orientation through features such as major entry points to provide a sense of belonging to the neighbourhood. Street level landscaping creates privacy within the development. Parking areas should be recessed to allow the pedestrian entry to predominate.	<b>Yes</b> - Two major entry points are emphasized in the design – one secondary access on School Road and one a primary entry at the north-west corner of the building, accessed by O'Shea Road. Landscaping serves to screen the project for neighbour and resident privacy. Parking is recessed giving emphasis to pedestrian and cycling access. Entries could be emphasized and made more welcoming from the street rather than screened with landscaping if this is the chosen priority.

DPA 4 Guidelines	Staff Evaluation – is criteria met?
Apartments should have a minimum 3 m depth terrace or balcony, sufficiently large to create a usable outdoor “room”. Balconies should be at least half enclosed in order to give the occupant privacy, security and weather protection.	<b>No</b> – individual balconies not included.
Dwelling units to be “clustered” in smaller groups to create more resident interaction and neighbourly surveillance.	<b>No</b> – Building is a single unit. Within the building, amenity spaces (main kitchen, dining area, lounge, etc) are on the ground floor to create resident interaction.
Changes in grade can provide for private areas between street edges and the development units.	<b>n/a</b> – No change in grade between street level and units. Building site is flat.
All Development Permit applications must provide a professional landscape plan.	<b>Yes</b> – Landscape Plan enclosed as Attachment C.
Trees should be planted and maintained by the property owners along street frontages of new multi-unit developments to create a mature treed “boulevard” type of streetscape. (Spacing will vary by species used, however, a rule of thumb for tree spacing is a minimum of 8.0 metres.) This may be supplemented by other lower ornamental plantings.	<p><b>Yes</b> – Mature conifers to be retained and new Autumn Brilliance Serviceberry, Nootka Cypress, Pink Kousa Dogwood and Flowering Cherry and Pear are proposed to be added along both School Road and O’Shea boulevards.</p>  <p>Figure 14: Kousa dogwood (left) and Serviceberry (right)</p>
Native or hardy landscape species are preferred over exotic species; a mix of coniferous and deciduous species is recommended to provide effective landscaping though the seasons. Willows, bamboo and other invasive species are not recommended.	<b>Yes</b> - Cultivars of native species including serviceberry and hardy Nootka cypress are used to provide year round interest to the landscape along with hardy species of flowering cherry, ornamental pear and Kousa dogwood. No invasive plants are indicated.



DPA 4 Guidelines	Staff Evaluation – is criteria met?
Trees, or a combination of landscape and architectural features shall be used to define the gateway or entrance to a development. Landscaped entrances however, should be low-level for better security at entrances.	<b>Yes</b> – shrubs around entrances are at grade or kept to a maximum for 4 feet in height while trees at access points, which include existing conifers, flowering cherry and Kousa dogwood have or can be pruned to maintain a high canopy for visibility. A boulevard Amelanchier (serviceberry) fronting the O'Shea entrance provides light screening and can also be maintained with a higher canopy once larger and mature.
Clusters of trees, ponds, or other landscape features should be used within the development to create a meaningful common area. Central areas or courtyards should be usable and inviting to residents as a meeting place, rather than random plantings of grass and shrubs. Seating areas and appropriate lighting should be provided within these common areas. Landscaping should also create a sense of enclosure and privacy for these spaces.	<b>Yes</b> – Outdoor patio area is well screened for privacy and enclosure with a variety of plantings. The outdoor space is limited somewhat by the need to provide facilities such as outdoor bicycle lockers, parking, garbage and storage. Outdoor gathering spaces are located adjacent to communal indoor spaces to prevent noise and privacy impacts to residents units.  Note: Detail drawings for garbage enclosure and bicycle lockers are enclosed as Attachment E.
Wherever possible, natural vegetation should be retained or enhanced as a feature of the development.  This is particularly important where natural features such as streams or steep slopes are a component of the development.	<b>Yes</b> – Existing conifers on O'Shea boulevard and memorial spruce on School Road frontage are to be retained.   <i>Figure 15: Image of existing building shows conifers, front left and far right, to be retained</i>
All public and semi-public areas should be landscaped, including entrance driveways, areas surrounding parking spaces or structures.	<b>Yes</b> – thorough landscaping design per landscape plan enclosed as Attachment C.


**Staff Report to Planning and Development Committee - September 17, 2019**

**Development Permit for Form and Character (DPA 4) for 749 School Road Page 16 of 21**

<b>DPA 4 Guidelines</b>	<b>Staff Evaluation – is criteria met?</b>
Large areas of uncharacteristic materials such as bark mulch, gravel, river rock and ground cover are to be avoided, and should be combined with a variety of plant materials.	<b>Yes</b> – landscape dominated by patio and sidewalk pavers and well buffered by tree, shrub and hedge plantings around common outdoor gathering spaces; the remaining landscape, not including the parking area at the rear of the building, is comprised of grass lawn with ample tree and shrub plantings.
Additional landscaping depth, denser vegetation and noise barriers such as earth berms should be used where a development abuts a major roadway.	<b>n/a</b>
Fences along streets should not provide a continuous wall or high barrier to the street, but should be lower profile and broken at intervals to provide pedestrian linkage and views to the street.	<b>Yes</b> – fencing is used at rear of property only, set back from Road.
Any fencing located along a street edge should not exceed a length greater than 20 metres without a substantive break or jog.	<b>n/a</b>
Fencing should not exceed a height of 1.2 metres within any part of the required front yard setback.	<b>Yes</b> – Front yard setback is from School Road; no fencing is proposed on this frontage.
Fencing along the street edge should be supplemented with low profile landscape plantings.	<b>Yes.</b>
Walkways and surface parking areas should be well lit and located in an area which is observable by residents.	<b>Yes</b> – The parking area is visible from the street and from the main building entrance; both walkways and parking are thoroughly lit with bollard lighting. See lighting plan enclosed as Attachment D.
Where possible, parking areas should be located in underground structures. Small groups of parking spaces throughout the development, located near to entry doors are preferable to large, central parking areas.	<b>Yes</b> – Underground parking not practical for site; proposed parking is limited in extent, well screened and near entry door. Limited parking relies on restriction on tenant car ownership.



<b>DPA 4 Guidelines</b>	<b>Staff Evaluation – is criteria met?</b>
Developments should be designed to prevent parking areas, carports or garages from dominating the internal open space areas; parking should be recessed from the main building edges.	<b>Yes</b> – Parking recessed as much as possible on the site and used to buffer living spaces from neighbours.
Parking areas should be landscaped and screened, but sufficiently visible to provide security to residents and vehicles.	<b>Yes</b> – as above.
Distinct, visible visitor parking areas should be provided near the entry to the development.	<b>Yes/no</b> – visitor parking not distinguished from general spaces; all spaces are easily visible, accessible and near entry.
Site design should provide for emergency vehicles, moving vans and service vehicles, and should locate this use to minimize noise impacts on residents and adjacent uses.	<b>Yes</b> – On-site space for loading and emergency access in parking lot is accessed from O'Shea Road and fronts main and emergency doors. The loading area is largely buffered from the street and neighbours by plantings on the O'Shea Road boulevard.
The size, siting and style of signage shall not be obtrusive or present a cluttered image.	<b>n/a</b> - no signage proposed.
Entry signs shall be placed at or below eye level and shall be integrated with landscaping or other feature.	<b>n/a</b>

DPA 4 Guidelines	Staff Evaluation – is criteria met?
Site lighting of all developments should be designed so that it avoids “light-spill” upon adjoining low density residential lands and of the night sky.	<p>Ceiling mounted pot lighting to light entrances reduces upward and outward light spill; sidewalk lighting is provided with capped bollard lights, as shown below, also reducing glare and light spill. The proposed lighting plan and lighting details are enclosed as Attachment D.</p>  <p><b>303-B1-LEDB2</b> EON LED</p>

#### Conclusion

Staff feels that given the constraints of the site and the mandate of providing 40 affordable units and associated supports, the design incorporates significant and appropriate measures to meet the form and character guidelines for Development Permit Area 4, to fit with the character and context of the neighbourhood and to mitigate potential neighbour impacts with conscious building design and orientation and with strategic landscape and planting design.

#### COMMUNICATION

An early draft of the architectural plans for the Supportive Housing proposal were shared with the public at a pre-application community dialogues held on April 3<sup>rd</sup> and 4<sup>th</sup>, 2019. A revised plan and the landscape plan were then shared at the two Public Information Meetings held on July 31<sup>st</sup>, 2019 at the Gibsons Public Market. The general outcomes and summary reports of these meetings were shared with Council at its [September 3<sup>rd</sup> Meeting](#). Comments relating to the form and character of the proposed development are summarized in the following excerpts from the two reports:

*Community Dialogues, April 3rd and 4th, 2019:*

Comments from participants of the Community Dialogues are summarized as follows in a report dated May 16, 2019 and prepared by Livable City Planning Ltd. No landscape plan was shared



with the public at the time of this meeting and several of the comments have been addressed in the updated design.

*“Comments were received related to the design of the development:*

- Provide landscaping for privacy and to mitigate impacts on neighbouring properties;*
- Restrict Smoking, or Locate outdoor smoking area away from neighbouring properties;*
- Add detail and character in keeping with the Gibsons OCP;*
- Consider adding an elevator to promote accessibility;*
- Consider indoor and outdoor amenity needs of residents;*
- Consider more parking; and*
- Consider Green Designs to reduce energy / material use.*

*Eight written responses (12.9% of written responses) suggested the development was too large and that:*

- the three-storey building was too high;*
- there are too many units; and*
- development was not consistent with neighbourhood character.*

*In response to feedback about the design of the housing development, BC Housing commits to the following measures:*

- Size / Height: Based on the homelessness count in Gibsons and the experience of the shelter-operator in Gibsons, forty units is estimated to meet the need for homes by the homeless living in Gibsons. A 4 storey building with an elevator would be more typical of supportive housing building funded by BC Housing, and allow for 52-55 units. This proposal limits the height of the building to 3 storeys, consistent with several other buildings along School Road, and in keeping with typical multi-family building form per the multi-family land use designation of the site in the Official Community Plan. BC Housing will review detail and character for the modular design through the design development process and strive to align with the Seaside Village form and character detailed by the Town;*
- Landscaping & Aesthetics: BC Housing will review landscaping plans to consider neighbouring properties and impacts on privacy, aesthetics and overlook;*
- Smoking Area: Landscape Plans will designate an outdoor smoking area away from neighbouring properties to limit impacts of smoke and noise;*
- Resident Amenity Space and Accessibility: The plan includes indoor amenity and office space as well as outdoor amenity space. As Residents will be housed in private studio apartments, they will have personal space to store their belongings. The design includes common amenity space for bike storage. A minimum of 5% of units are required to be accessible based on BC Housing's experience working with this type of housing population. More than this 5% minimum will be provided;*

- Parking: The demographics of the anticipated resident population does not warrant additional parking;
- Sustainability: Modular construction has a high standard of energy efficiency (meeting BC “Step Code 3” standards, which is roughly equivalent to LEED standards).”

*Public Information Meetings, July 31st, 2019:*

Comments on the form and character of the proposed building and landscape plan as shared at the July 31<sup>st</sup> Public Information Meetings include the following, as summarized in a report dated August 20<sup>th</sup>, 2019 and prepared by BC Housing. Comments from the report relating to building and landscape design are summarized as follows:

*“About 9% of the specific comments – including a few in favour of the project – referred to the building’s design.*

*Comments included:*

- *Scale, form and character in relation to the neighbourhood and area’s seaside village character;*
- *Amount of green space, trees and vegetable gardens;*
- *The design detail and impact;*
- *Concerns about smoking wafting over to neighbour’s yards;*
- *Concerns about lack of parking;*

*In response to this feedback, BC Housing commits to the following:*

- Size / Height: Based on the homelessness count in Gibsons and the experience of the shelter-operator in Gibsons, 40 units is estimated to meet the need for supportive housing in Gibsons. A four-storey building would be more typical of supportive housing building funded by BC Housing, and allow for 52-55 units. This proposal limits the height of the building to three storeys, consistent with several other buildings along School Road, and in keeping with typical multi-family building form per the multifamily land use designation of the site in the Official Community Plan. BC Housing would review detail and character for the modular design through the design development process and strive to align with the Seaside Village form and character detailed by the Town;
- Resident Amenity Space: The plan includes indoor amenity and office space as well as outdoor amenity space. As residents would be housed in private studio apartments, they would have personal space to store their belongings. The design includes common amenity space for bike storage.
- Parking: The demographics of the anticipated resident population does not warrant additional parking.”

#### *Referrals*

The Zoning and OCP Amendment application was referred to multiple agencies as reported at the September 3<sup>rd</sup> meeting. The responses to the referral did not address the form and



character of the proposal, with the exception of a belated response from the O'Shea/ Oceanmount Community Association specifically addressing its view on the form and character of the building. The report is attached as Attachment G.


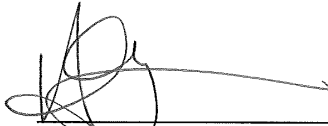
**RECOMMENDATIONS / ALTERNATIVES**

Staff recommendations are on page 1 of this report. Alternatively, the Planning and Development Committee may request changes to the design or recommend that Council denies the issuance of the permit.

**NEXT STEPS**

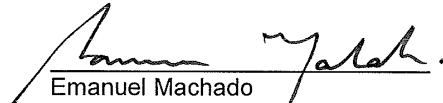
Upon issuance of a Development Permit, a Building Permit may be obtained subject to approval of the OCP and Zoning Bylaw Amendment applications.

Respectfully Submitted,

  
Kirsten Rawkins, MLA  
Planning Assistant  
Lesley-Ann Staats, MCIP, RPP  
Director of Planning

**CHIEF ADMINISTRATIVE OFFICER'S COMMENTS:**

I have reviewed the report and support the recommendation(s).

  
Emanuel Machado  
Chief Administrative Officer

**ATTACHMENTS**

- Attachment A – Development Plans
- Attachment B – Site Plan
- Attachment C – Landscape and Planting Plan
- Attachment D – Lighting Plan & Details
- Attachment E – Bicycle shelter and garbage enclosure details
- Attachment F – Form and Character review by project architect
- Attachment G – O'Shea/ Oceanmount Community Association form and character report
- Attachment H – Draft Permit

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002	16 JUN 19	ISSUED FOR REDZPOCP
003	08 JUL 19	RE-ISSUED FOR REDZPOCP
004	11 JUL 19	RE-ISSUED FOR REDZPOCP

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PROJECT NAME  
Gibsons, Supportive  
Housing  
CLIENT NAME  
BC Housing

PROJECT ADDRESS  
739-749 School Rd  
Gibsons, BC

PROJECT NUMBER  
19-007

STATUS

SCALE  
NTS

DATE  
04 June 2019

SHEET DESCRIPTION  
Cover Page - Project Information

**A**<sub>0.01</sub>

# New Supportive Housing Facility

739-749 School Road, Gibsons, BC

## Zoning Analysis

Civic Address	739-749 School Road Gibsons, BC
Legal Description	Lots 1 & 2 of Block 2 D.L. 686 G1 N.W.D., Plan 9933 P.I.D. 009-612-084, 009-612-122
Site Area	17 048 sq. ft. (1583.8 sq. m)
Current Zoning Proposed Zoning	R-2 Comprehensive Development
Approved Principal Use Proposed Principal Use	Single Family Residence 40 Single-Occupant Apartments
Density	253 Units / Hectare
Minimum Lot Area Existing Lot Area	7169 sq. ft. (666 sq. m) 17 048 sq. ft. (1583.8 sq. m)
Minimum Lot Width Existing Lot Width	59.1 ft. (18 m) 96.8 ft. (29.5 m)
Minimum Lot Depth Existing Lot Depth	98.4 ft. (30 m) 175.8 ft. (53.6 m)
Minimum Front Setback Proposed Front Setback	24.6 ft. (7.5 m) 13.5 ft. (4.1 m)
Minimum Rear Setback Proposed Rear Setback	24.6 ft. (7.5 m) 43.0 ft. (13.1 m)
Minimum Exterior Side Setback Proposed Exterior Side Setback	9.8 ft. (3 m) 11.5 ft. (3.5 m)
Minimum Interior Side Setback Proposed Interior Side Setback	4.9 ft. (1.5 m) 20.0 ft. (6.1 m)
Maximum Site Coverage Proposed Site Coverage	45% Building 7126 sq. ft. (662.0 sq. m) Overhangs & Canopies 771 sq. ft. (71.6 sq. m) Storage 154 sq. ft. (14.3 sq. m) Garbage & Recycling 195 sq. ft. (18.1 sq. m) Parking 2381 sq. ft. (221.2 sq. m) Walkways & Terraces 2024 sq. ft. (188.1 sq. m) TOTAL 12 651 sq. ft. (1175.7 sq. m) = 74%
Average Grade Maximum Building Height Proposed Building Height	315.0 ft. (96.1 m) 26.2 ft. (8 m) 37.8 ft. (11.5m)
Minimum Unit Area Proposed Unit Area	753 sq. ft. (70 sq. m) 359 sq. ft. (33.4 sq. m)
View Corridor	Not Applicable

## Parking, Loading, Bicycles

Required Car Stalls Residential Office & Kitchen TOTAL Handicap Provided	1.5 per unit = 60 1 per 45 sq. m = 100 / 45 = 2.2 62 Not Required 5 (including 1 Handicap)
Required Loading Provided	1 for 2.5 x 9 m vehicle 0
Required Class 1 Bicycles Residential Office & Kitchen TOTAL Provided	1.25 per unit = 50 0.27 per 100 sq. m over 100 = 0 50 37
Required Class 2 Bicycles Residential Office & Kitchen TOTAL Provided	0.2 per unit = 8 0.4 per 100 sq. m over 100 = 0 8 13

## Floor Areas & F.S.R.

Level One	2880 sq. ft. (267.8 sq. m)
Level Two	5750 sq. ft. (534.2 sq. m)
Level Three	5750 sq. ft. (534.2 sq. m)
TOTAL	14 380 sq. ft. (1336.2 sq. m)
F.S.R.	14 380 / 17 048 = 0.84

## Drawings List

### Architectural:

A0.01	Cover Page - Project Information
A0.02	Survey
A0.03	Renderings
A1.01	Site Plan
A2.01	Crawlspace and Level 1 Plans
A2.02	Level 2 and 3 Plans
A2.03	Roof Plan
A3.01	Elevations
A4.01	Building Sections

### Project Directory

#### Owner:

BC Housing  
Sara Jellicoe, Development Manager  
1701 - 4555 Kingsway  
Burnaby, BC V5H 4V8  
778-452-2737  
sjellicoe@bchousing.org

#### Design Builder:

Metric Modular  
Tom Faliszewski, BA, BArch, CPHD, Senior Manager, Innovative Solutions  
1825 Tower Road  
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tfaliszewski@metricmodular.com

#### Architect:

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003	08 JUL 19	RE-ISSUED FOR REZ/CP/CP



View Along School Road



View Along O'Shea Road



Cladding Colours (approximate to be product matched)

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Gibsons, Supportive  
Housing  
CLIENT NAME  
BC Housing  
PROJECT ADDRESS  
739-749 School Rd  
Gibsons, BC

PROJECT NUMBER  
19-007  
STATUS

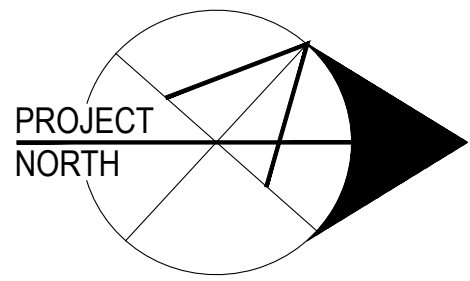
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DATE  
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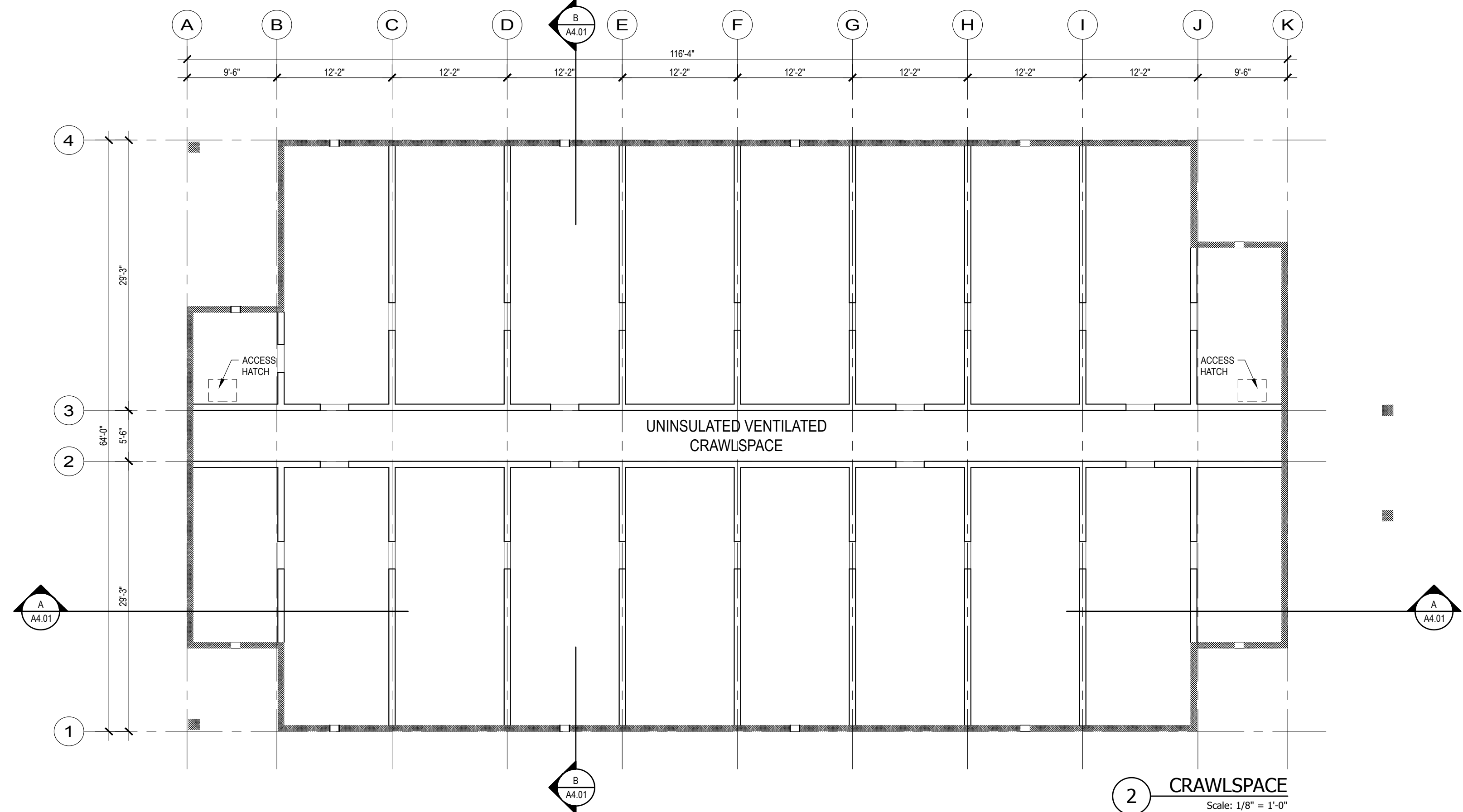
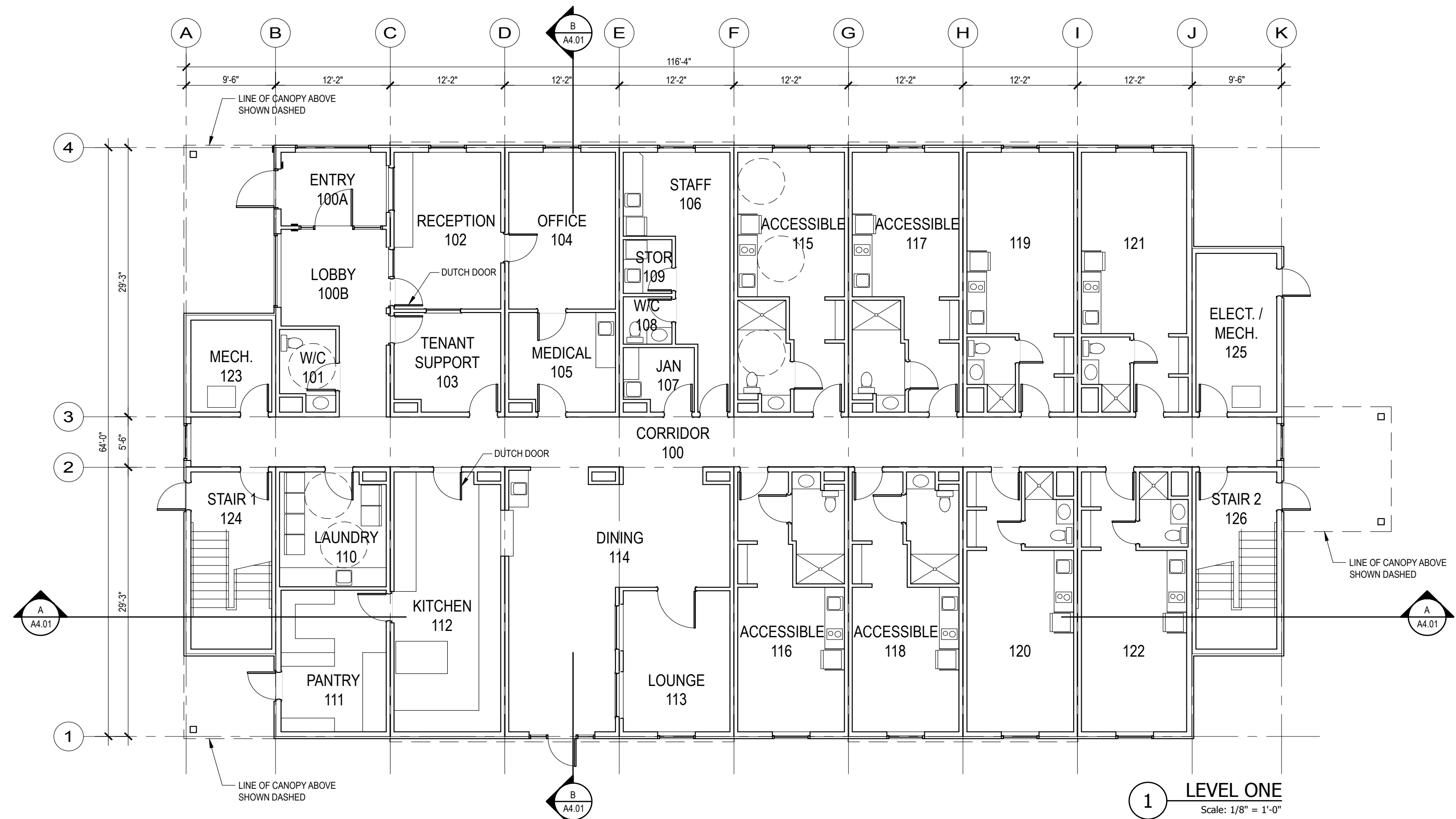
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002	14 JAN 19	ISSUED FOR REDDROOP
003	08 JUL 19	RE-ISSUED FOR REDDROOP



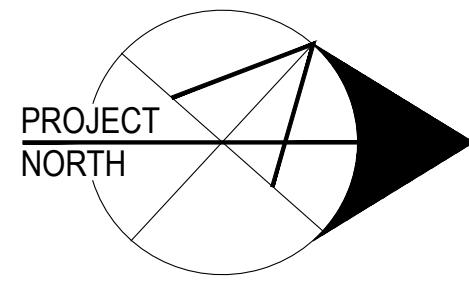
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PROJECT NUMBER  
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STATUS  
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DATE  
27-05-2019  
SHEET DESCRIPTION  
Crawlspace  
Floor Plan - Level 1

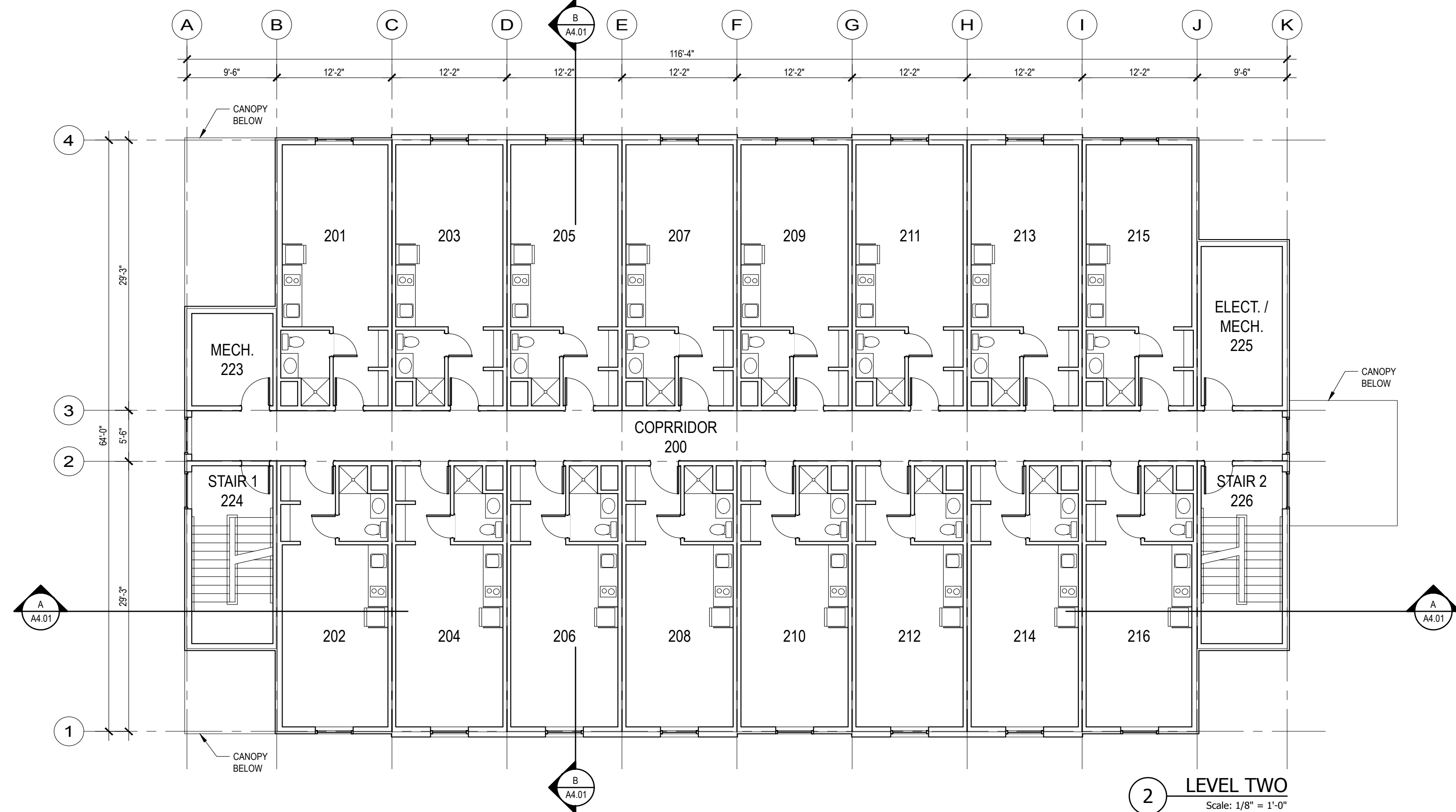
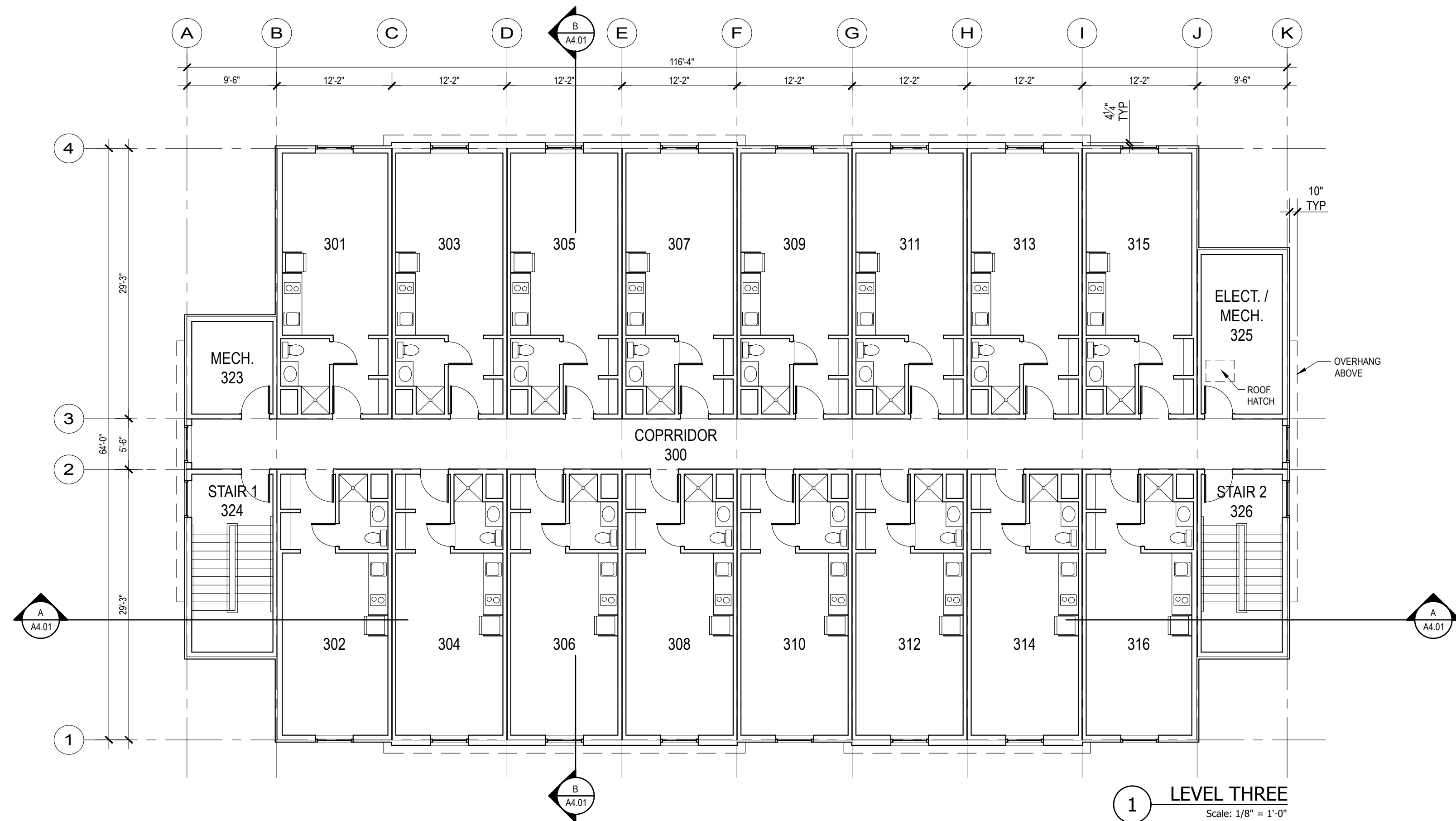
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003	08 JUL 19	RE-ISSUED FOR REDDROOP



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Gibsons, Supportive  
Housing  
CLIENT NAME  
BC Housing  
PROJECT ADDRESS  
739-749 School Rd  
Gibsons, BC

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19-007

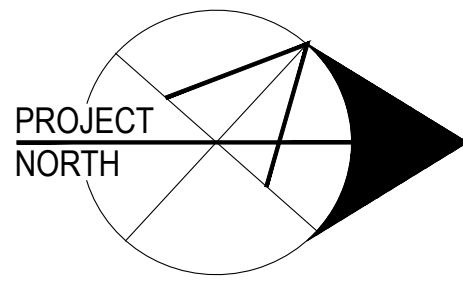
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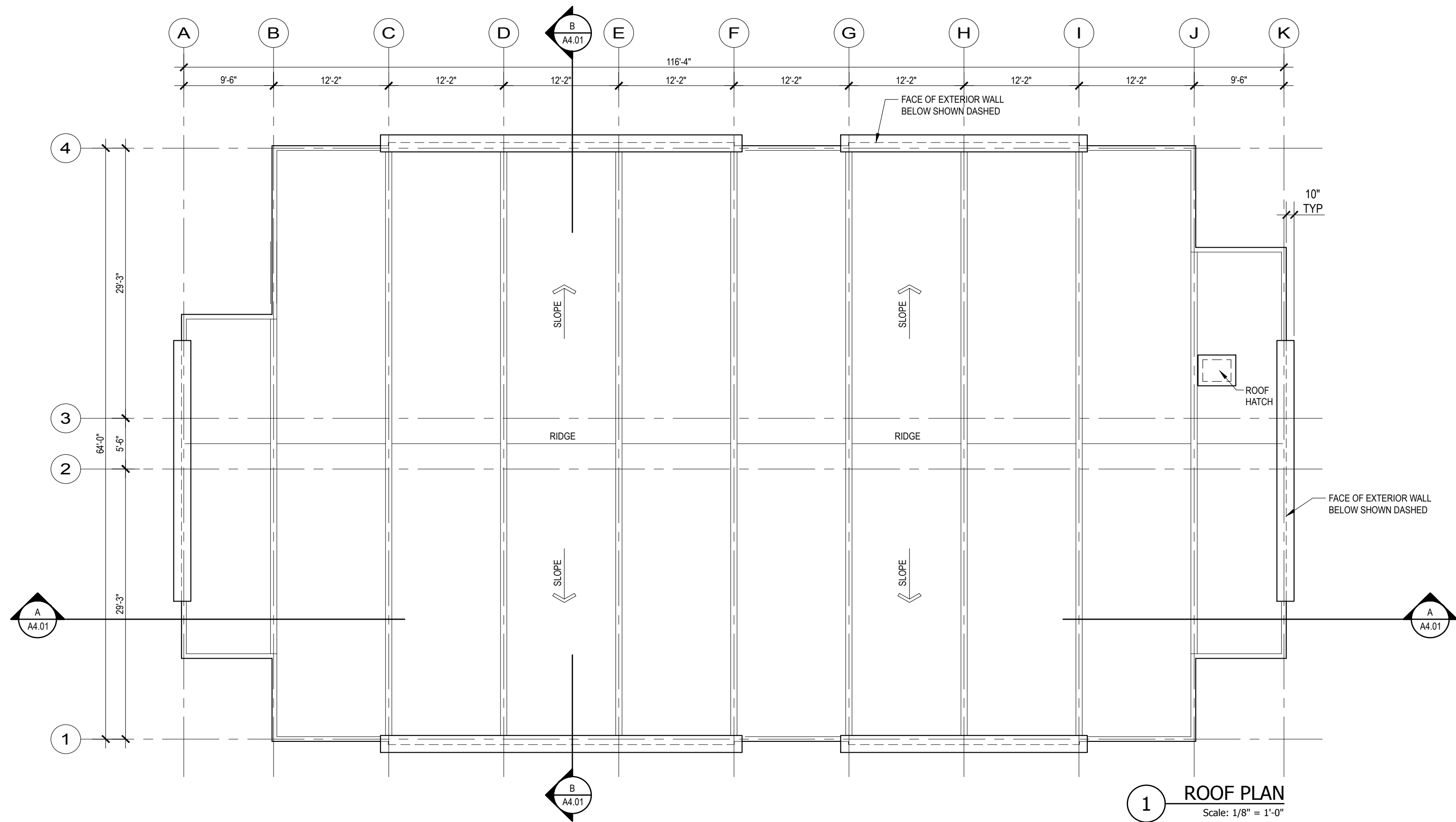
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Floor Plans  
Levels 1 & 2

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003	08 JUL 19	RE-ISSUED FOR REZDPROOFP



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Gibsons, Supportive  
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739-749 School Rd  
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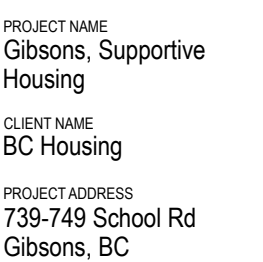
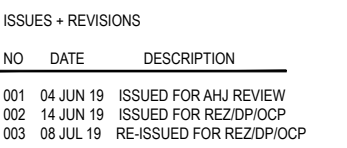
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DATE  
27-05-2019

SHEET DESCRIPTION  
Roof Plans

A2.03





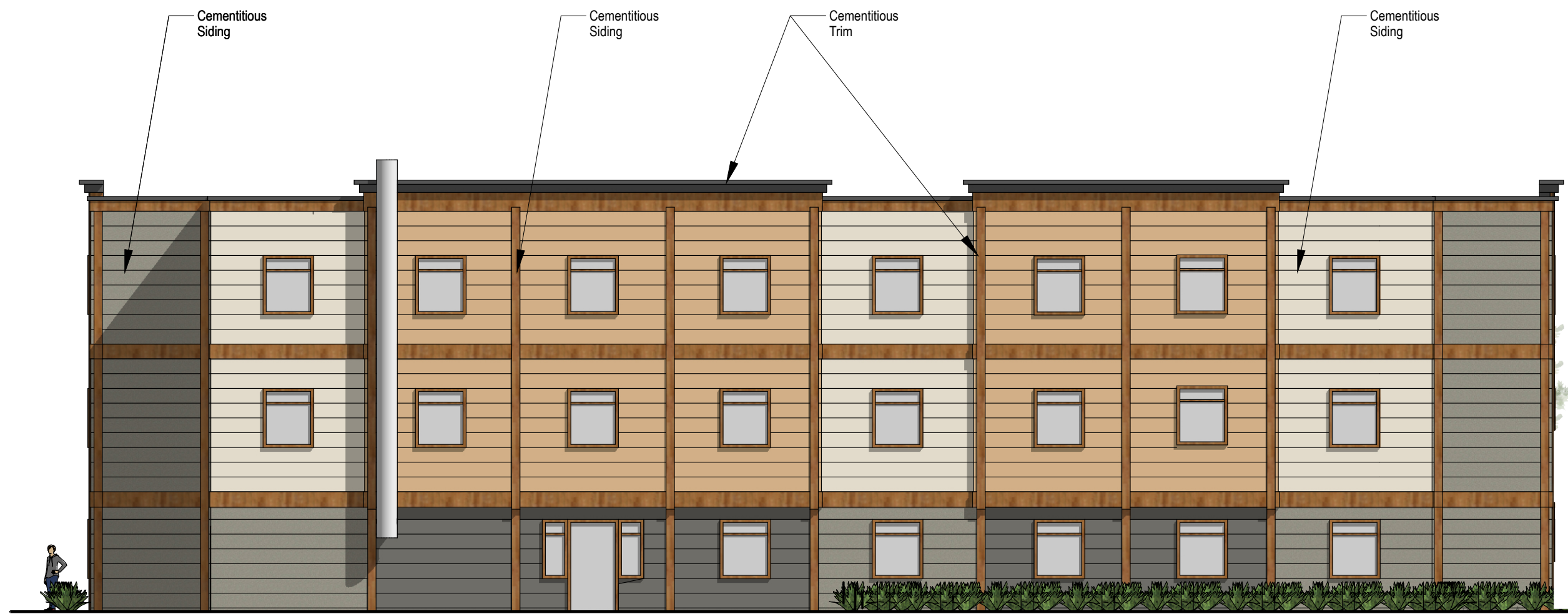


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North Elevation



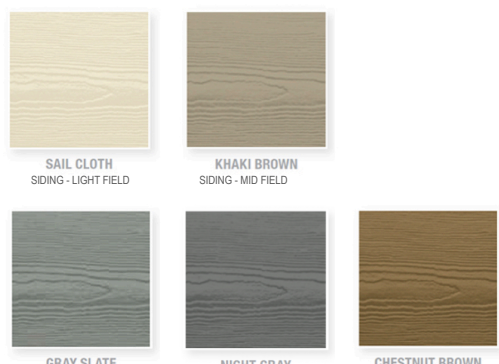
East Elevation



South Elevation



West Elevation



Cladding Colours (approximate to be product matched)

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Housing  
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739-749 School Rd  
Gibsons, BC

PROJECT NUMBER  
19-007

STATUS

SCALE  
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DATE  
04 June 2019

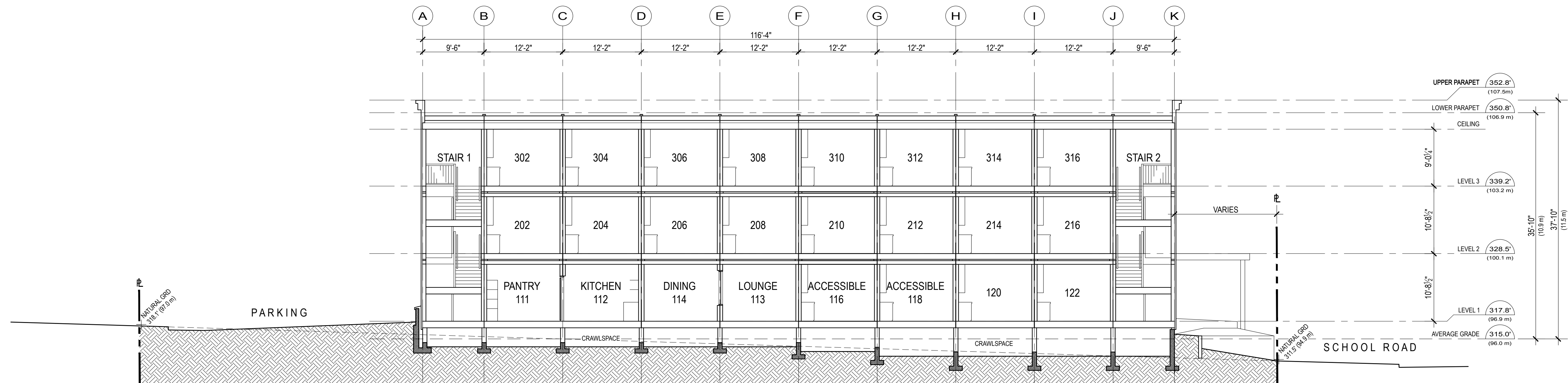
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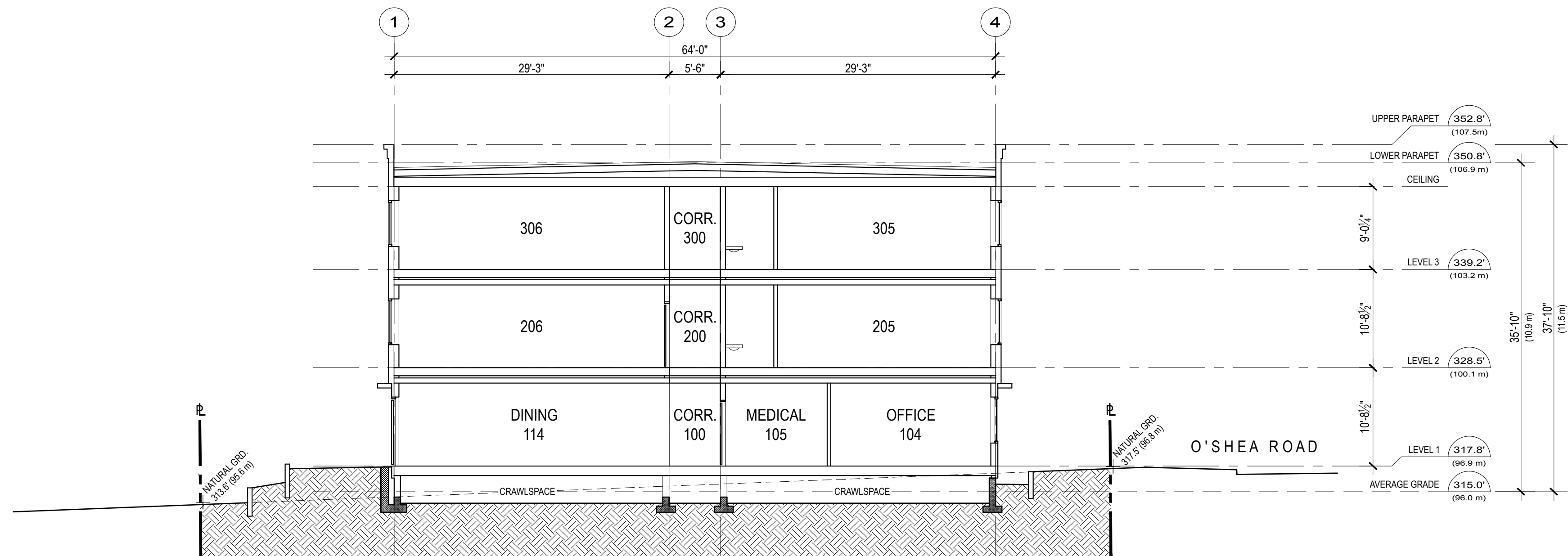


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003	08 JUL 19	RE-ISSUED FOR REZDPROCP



1 BUILDING SECTION A-A  
Scale: 1/8" = 1'-0"



2 BUILDING SECTION B-B  
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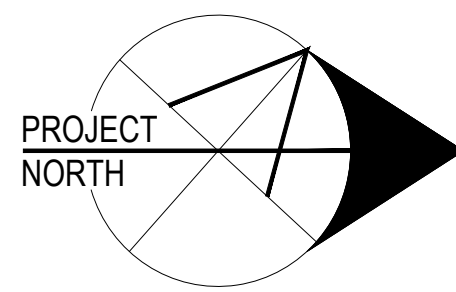
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19-007  
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DATE  
27-05-2019  
SHEET DESCRIPTION  
Building Sections

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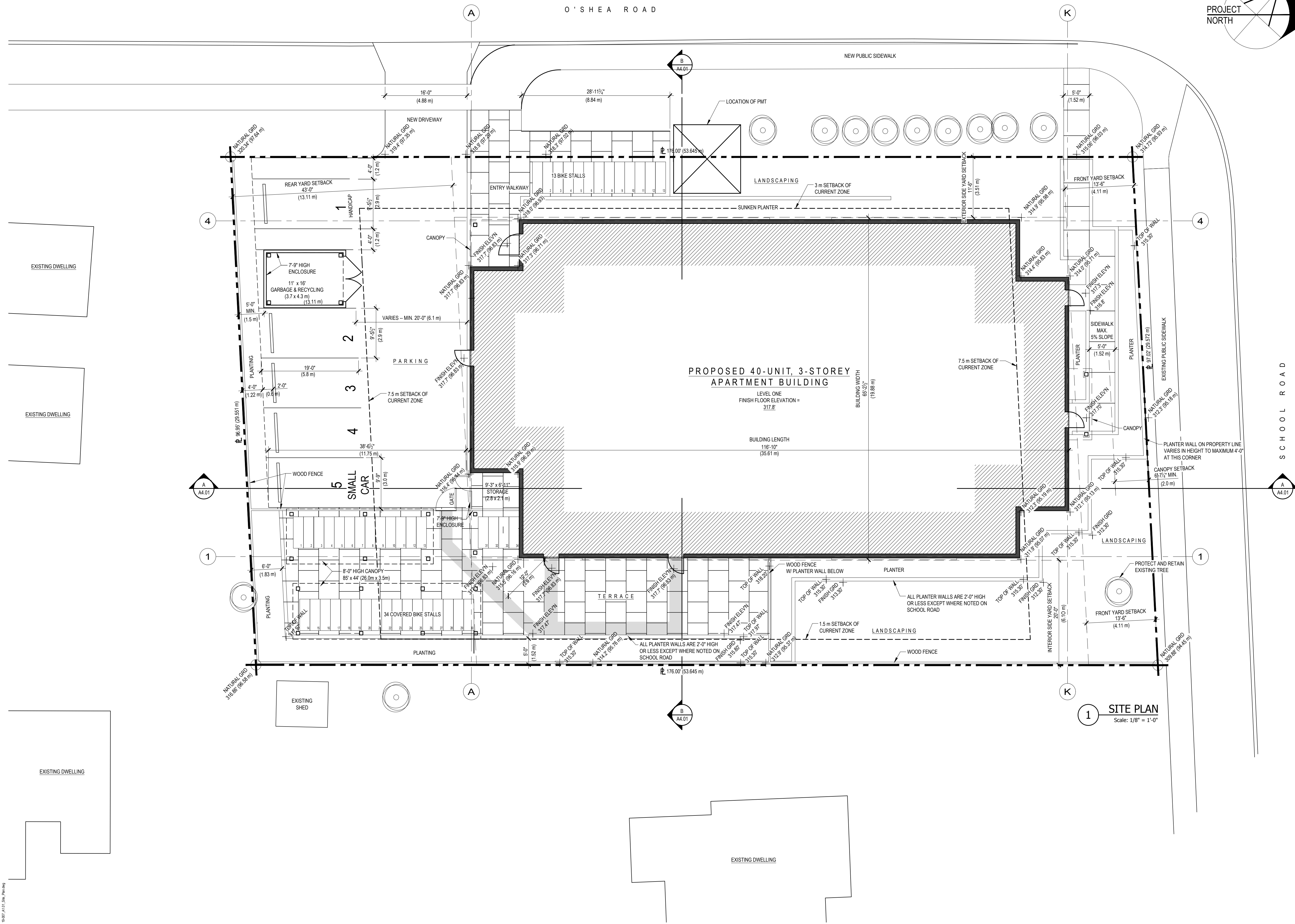
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NO.	DATE	DESCRIPTION
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002	14 JUN 19	ISSUED FOR REZPOPCP
003	08 JUL 19	RE-ISSUED FOR REZPOPCP

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PROJECT NUMBER  
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SCALE  
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DATE  
27-05-2019  
SHEET DESCRIPTION  
Site Plan

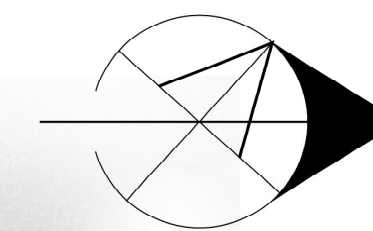
A1.01



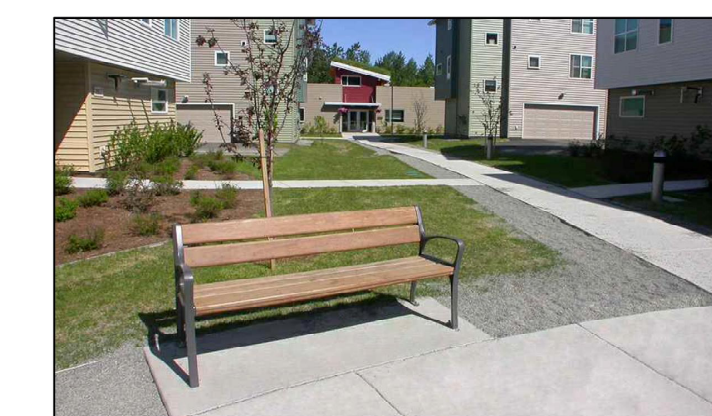
1 SITE PLAN  
Scale: 1/8" = 1'-0"



SEAL:



**MAGLIN MLPT720**  
**CLUSTER SEATING SERIES**



MAGLIN MLB870 BENCH SERIES

3	19.AUG.07	REVISED PER NEW SITE PLAN	MC
2	19.JUL.24	UPDATE PER NEW SITE PLAN	CLG
1	19.JUL.12	REVISED PER NEW SITE PLAN	JR
NO.	DATE	REVISION DESCRIPTION	DR.

CLIENT:

PROJECT:

739 - 749 SCHOOL ROAD  
GIBSONS

DRAWING TITLE:

DATE: 19.JUL.08 DRAWING NUMBER:

DATE: 19 JUL 08

SCALE: 1"=10'-0"

DRAWN: J

**DESIGN:**

CHK'D:

PMG PROJECT NUMBER:

DRAWING NUMBER

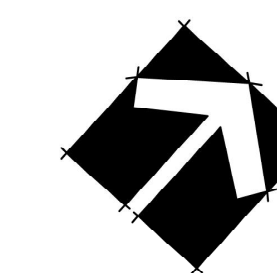
14

11







11

OF 3

19-116



19116-3.ZIP

TREE SCHEDULE				PMG PROJECT NUMBER: 19-116	
KEY		BOTANICAL NAME	COMMON NAME	PLANTED SIZE / REMARKS	
     	4	AMELANCHIER x AUTUMN BRILLANCE	PRINCESS DIANA SERVICEBERRY	5CM CAL.	1.5M STD. B&B
	4	CHAMAECYPARIS NOOTKATENSIS	NOOTKA CYPRESS	3.5M HT.	B&B
	5	CORNUS KOUSA 'SATOMI'	PINK KOUZA DOGWOOD	2.5M HT.	B&B
	4	LIQUIDAMBAR STYRACIFLUA 'SLENDER SILHOUETTE'	SLENDER SILHOUETTE SWEETGUM	6CM CAL.	2M STD. B&B
	3	PRUNUS SERRULATA 'SHROFPOUGH'	WHITE GODDESS FLOWERING CHERRY	6CM CAL.	1.5M STD. B&B
	3	PYRUS CALLERYANA 'CHANTICLEER'	CHANTICLEER PEAR	6CM CAL.	1.5M STD. B&B
	10	EXISTING TREE			

NOTES: \* PLANT SIZES IN THIS LIST ARE SPECIFIED ACCORDING TO THE BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD, LATEST EDITION. \* CONTAINER SIZES ARE SPECIFIED AS PER CNA STANDARD. \* PLANT PLANT SIZE AND CONTAINER SIZE ARE THE MINIMUM ACCEPTABLE SIZES. \* REFER TO SPECIFICATIONS FOR DEFINED CONTAINER MEASUREMENTS AND OTHER PLANT MATERIAL REQUIREMENTS. \* SEARCH AND REVIEW MAKE PLANT MATERIAL AVAILABLE FOR OPTIONAL REVIEW BY LANDSCAPE ARCHITECT AT SOURCE OF SUPPLY. AREA OF SEARCH TO INCLUDE LOWER MAINLAND AND FRASER VALLEY. \* SUBSTITUTIONS: OBTAIN WRITTEN APPROVAL FROM THE LANDSCAPE ARCHITECT PRIOR TO MAKING ANY SUBSTITUTIONS. TO THE SPECIFIED MATERIAL UNAPPROVED SUBSTITUTIONS WILL BE REJECTED. ALLOW A MINIMUM OF FIVE DAYS PRIOR TO DELIVERY FOR REQUEST TO SUBSTITUTE. SUBSTITUTIONS ARE SUBJECT TO BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD. \* DEFINITION OF CONDITIONS OF AVAILABILITY. \* ALL LANDSCAPE MATERIAL AND WORKSMANSHIP MUST MEET OR EXCEED BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD LATEST EDITION. \* ALL PLANT MATERIAL MUST BE PROVIDED FROM CERTIFIED DISEASE FREE NURSERY. \* BIO-SOLIDS NOT PERMITTED IN GROWING MEDIUM UNLESS AUTHORIZED BY LANDSCAPE ARCHITECT.



[illegible]

CLIENT:

DRAWING TITLE:

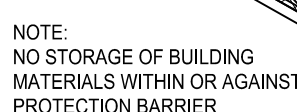
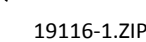
**TREE MANAGEMENT  
PLAN**

PMG PROJECT NUMBER: 19-116

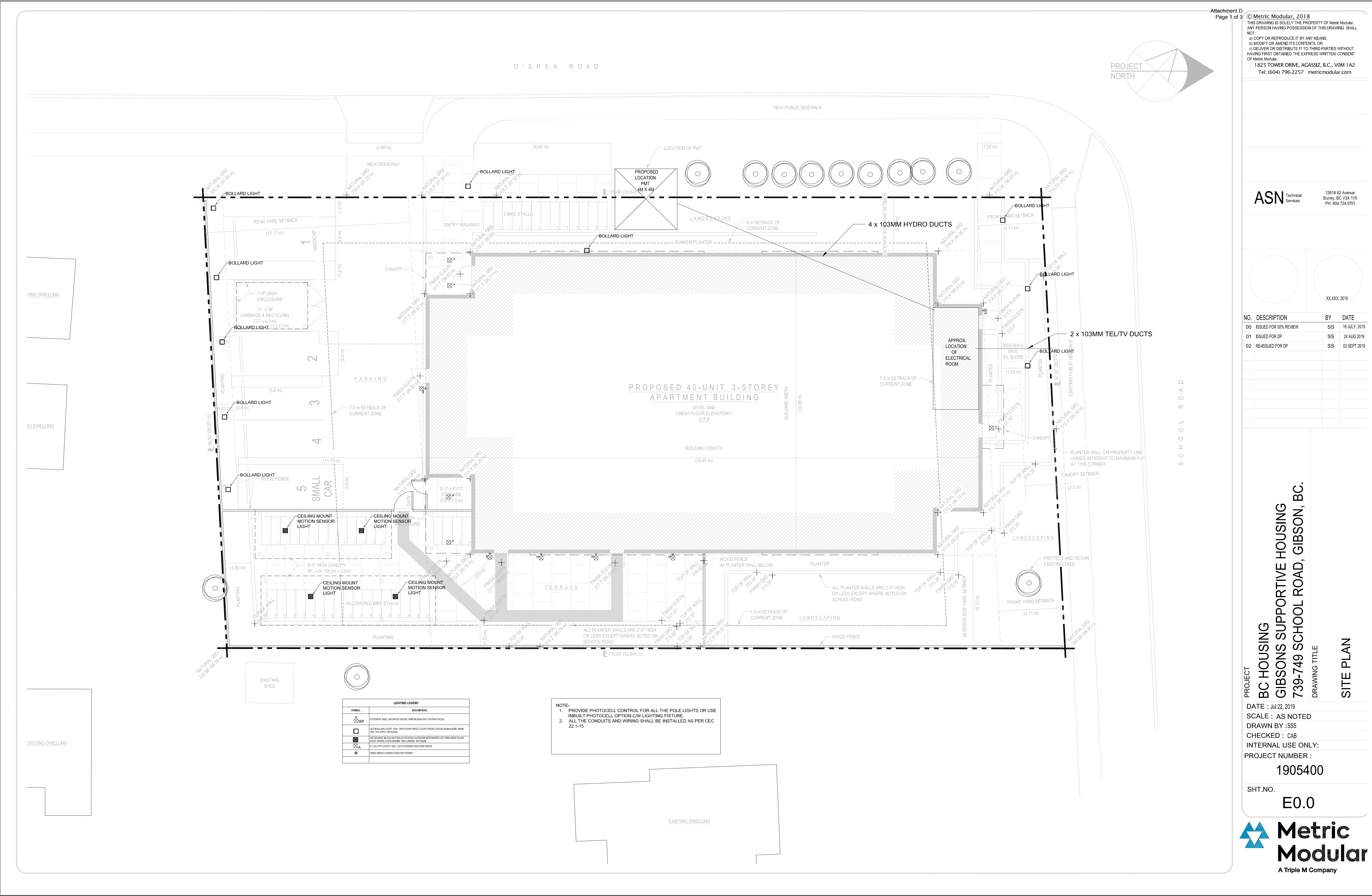


\* A LANDSCAPE PLAN IS TO BE SUBMITTED FOR REVIEW TO ENGINEERING SERVICES A MINIMUM OF 8 WEEKS PRIOR TO START OF ANY CONSTRUCTION PROPOSED FOR PUBLIC PROPERTY. NO WORK ON PUBLIC PROPERTY MAY BEGIN UNTIL SUCH PLANS RECEIVE "FOR CONSTRUCTION" APPROVAL AND RELATED PERMITS ARE ISSUED. PLEASE CONTACT KEVIN CAVELL AT 604-873-7773 OR FRANK BATTISTA AT 604-873-7317 FOR DETAILS.

TRUNK DIAMETRE (CM)	DISTANCE FROM TRUNK (M)	TOTAL DIAMETER (M)
20	1.2	2.60
25	1.5	3.25
30	1.8	3.90
35	2.1	4.55
40	2.4	5.20
45	2.7	5.85
50	3.0	6.50
55	3.3	7.15
60	3.6	7.80
75	4.5	9.75
90	5.0	10.90
100	6.0	13.00


$$\frac{1}{L2}$$






# Lumière

## DESCRIPTION

Eon 303-B1-LEDB2 is a compact, low profile, dimmable, LED bollard that provides downlight only via a fixed head. 303-B1-LEDB2 has a single head on one side of the luminaire. The bollard comes standard with universal input LED drivers (120-277V, 50/60 Hz). Dimming is achieved with a standard ELV, reverse phase dimming driver or an optional 0-10V dimming driver. Eon fixtures may be used indoors or outdoors and carry an IP66 rating. The patented LumaLevel™ leveling system provides quick installation, easy adjustment, secure mounting and protection from vibration.

Catalog #	Type
Project	
Comments	Date
Prepared by	

## SPECIFICATION FEATURES

### Construction

The head of the 303-B1-LEDB2 is precision machined from corrosion-resistant 6061-T6 aluminum. Body is extruded aluminum and adjustable mounting base is cast from corrosion resistant aluminum alloy. Stainless steel hardware is included. Four (4) 3/8" x 12" galvanized anchor bolts and a galvanized steel anchor bolt template are standard. Specify option -LAB and order the anchor bolt/template kit separately (Catalog: 7581-01PK).

### Optical

LightBAR™ and optical assembly are sealed by a clear, impact resistant tempered glass lens. The optical assembly is available in three distributions: T2 (lateral throw), T4 (forward throw) and T5X (Flood). Available in several color temperatures: 2700K, 3000K, 3500K,

4000K and TSAM (Amber). Both color temperature and distribution must be specified when ordering – see catalog logic for details. An edge-lit option is available.

### Electrical

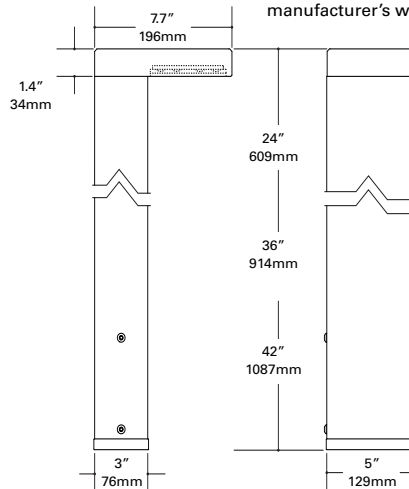
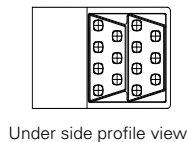
The bollard is standard with an ELV trailing edge phase dimmable driver that accepts a universal input (120-277, 50/60Hz). The standard driver is ELV trailing edge phase dimmable. An optional 0-10V dimming driver is also available. Both driver options incorporate surge protection. The receptacle option incorporates a specification grade, 120V, 15A tamper proof and weather resistant duplex GFCI. The photocell option comes in either a 120V or 277V. Please see Option section for more detail.

### Finish

Luminaire and mounting base are double protected by a RoHS compliant chemical film undercoating and polyester powdercoat paint finish. The mounting base is painted black. The luminaire housing and head are available in a variety of standard colors. RAL and custom color matches are available upon request. As an option, the Eon bollards are also available in colors to match other outdoor Eaton product lines, such as Invue. See the Finish section in the ordering detail for more detail. The LightBAR™ cover plates are standard white.

### Warranty

Lumière warrants the EON series of fixtures against defects in material and workmanship for five (5) years. Auxiliary equipment such as LED drivers carries the original manufacturer's warranty.



## 303-B1-LEDB2 EON LED

### APPLICATIONS: BOLLARD

### CERTIFICATION DATA

UL and cUL Wet Location Listed  
LM79 / LM80 Compliant  
ROHS Compliant  
IP66 Ingress Protection Rated

### TECHNICAL DATA

50°C Maximum Temperature Rating  
External Supply Wiring 90°C Minimum



## ORDERING INFORMATION

Sample Number: 303-B1-LEDB2-2700-120-T2-DIM10-BK-42-EDGE-PC1-RFL-LAB

Series <sup>8</sup>	Color Temperature	Input Voltage	Optics	Dimming	Finish <sup>3</sup>	Height <sup>4</sup>	Options <sup>5</sup>
303-B1-LEDB2  Head contains two (2) Mini LightBAR™	2700=2700K 3000=3000K 3500=3500K 4000=4000K TSAM=Turtle Safe Amber (585-595nm)	UNV=120-277V <sup>1</sup> 120=120V 277=277V <sup>2</sup>	T2 = Type II, Lateral Throw T4 = Type IV, Forward Throw T5X = Type V, Extra Wide Flood	DIMELV=Trailing Edge Phase Dimming Driver DIM10=0-10V Dimming Driver	Painted BK=Black BZ=Bronze CS=City Silver WT=White Premium Paint AP=Grey DP=Dark Platinum GM=Graphite Metallic	24=24" 36=36" 42=42"	EDGE=Edge lit glass lens PC1=Photocontrol 120V <sup>6</sup> PC2=Photocontrol 208-277V <sup>6</sup> RIU=Receptacle - In Use (120V Only) <sup>6</sup> RFL=Receptacle - Flip-Lid (120V Only) <sup>6</sup> LAB=Less Anchor Bolts & Template <sup>7</sup>

NOTES: 1 Universal Voltage (UNV) is standard unless specifying Photocontrol or Receptacle (RIU or RFL - 120V) options. 2 Specify for PC2 option only. 3 Custom and RAL color matching available upon request. Consult factory for further information. 4 Bollard heights are nominal (shown in inches). 5 Add suffix in the order shown. 6 Must specify voltage when ordering. 7 When specifying LAB option the anchor bolts and template need to be ordered separately 7581-01PK. 8 DesignLights Consortium™ Qualified and classified for DLC Standard. Refer to [www.designlights.org](http://www.designlights.org) for details on exact qualified EON 303-B1-LEDB2 product as not all configurations are DLC classified.



ADL121470  
March 15, 2016



## LUMEN MAINTENANCE

Ambient Temperature	TM-21 Lumen Maintenance (72,000 Hours)	TM-21 Reported L70(10k) (Hours)	Theoretical L70 (Hours)
25°C	> 94%	> 60,000	365,000
40°C			
50°C			

## CURRENT DRAW

Model	Line Voltage	Current Draw
303-B1-LEDB2	120-277V, 50/60Hz	0.13A

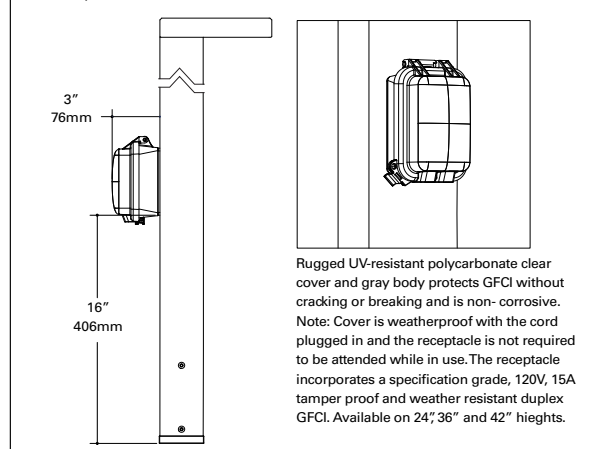
## MAX LOAD RATING

Options	Line Voltage	Max Load Rating
PC1	120V, 50/60Hz	1000VA, 8.3A
PC2	208-277V, 50/60Hz	1800VA, 15A
RIU or RFL	120V, 50/60Hz	1800VA, 15A

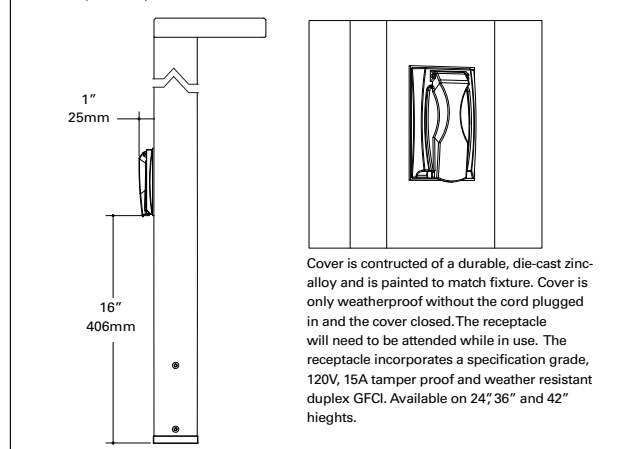
## OPTIONS

### Receptacle Options (120V Only)

#### RIU - Receptacle In-Use

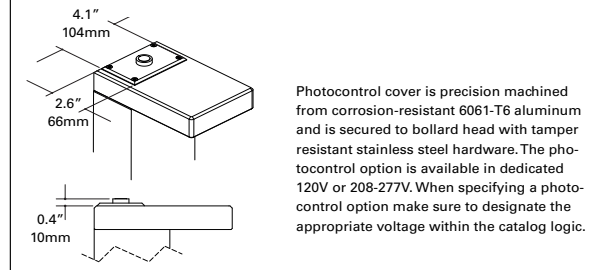


#### RFL - Receptacle Flip Lid

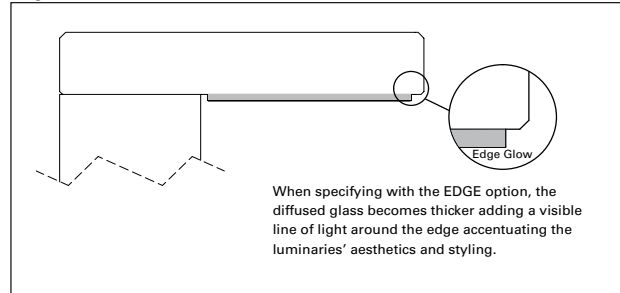


## Photocontrol

### PC1 (120V) or PC2 (277V)

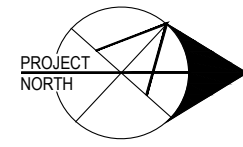


## Edge



## TECHNICAL NOTES:

- Adjustable mounting base - Cast aluminum mounting base is equipped with the patented LumaLevel™ leveling system that includes mounting base, 70 shore neoprene base, stainless steel hardware and a slot to accommodate two inbound and outbound 3/4 inch conduits. It provides quick installation, easy adjustment, secure mounting and protection from vibration.



NOTES  
THIS DOCUMENT HAS BEEN DIGITALLY  
CERTIFIED WITH DIGITAL CERTIFICATE  
AND ENCRYPTION TECHNOLOGY  
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INSTITUTE OF BC AND THE ASSOCIATION  
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FILE PROVIDED BY THE ARCHITECT.

#### LEGEND & NOTES

ASSEMBLY TYPE DESIGNATION  
SEE DRAWING A6.01 and A6.02  
Assemblies  
REFER TO SITE PLAN FOR SETBACK  
AND LOCATION REQUIREMENTS  
FOR CANOPY DETAILS, SEE DRAWING  
AX.XX

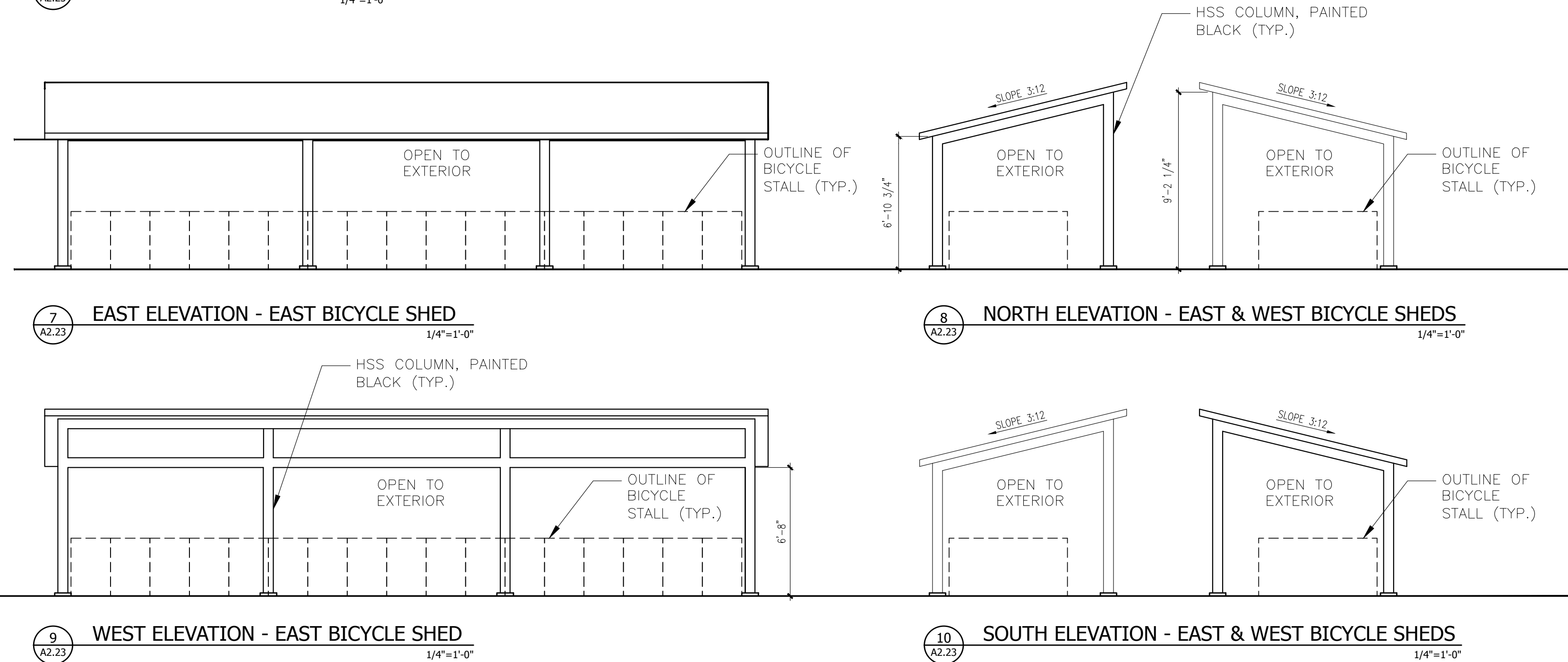
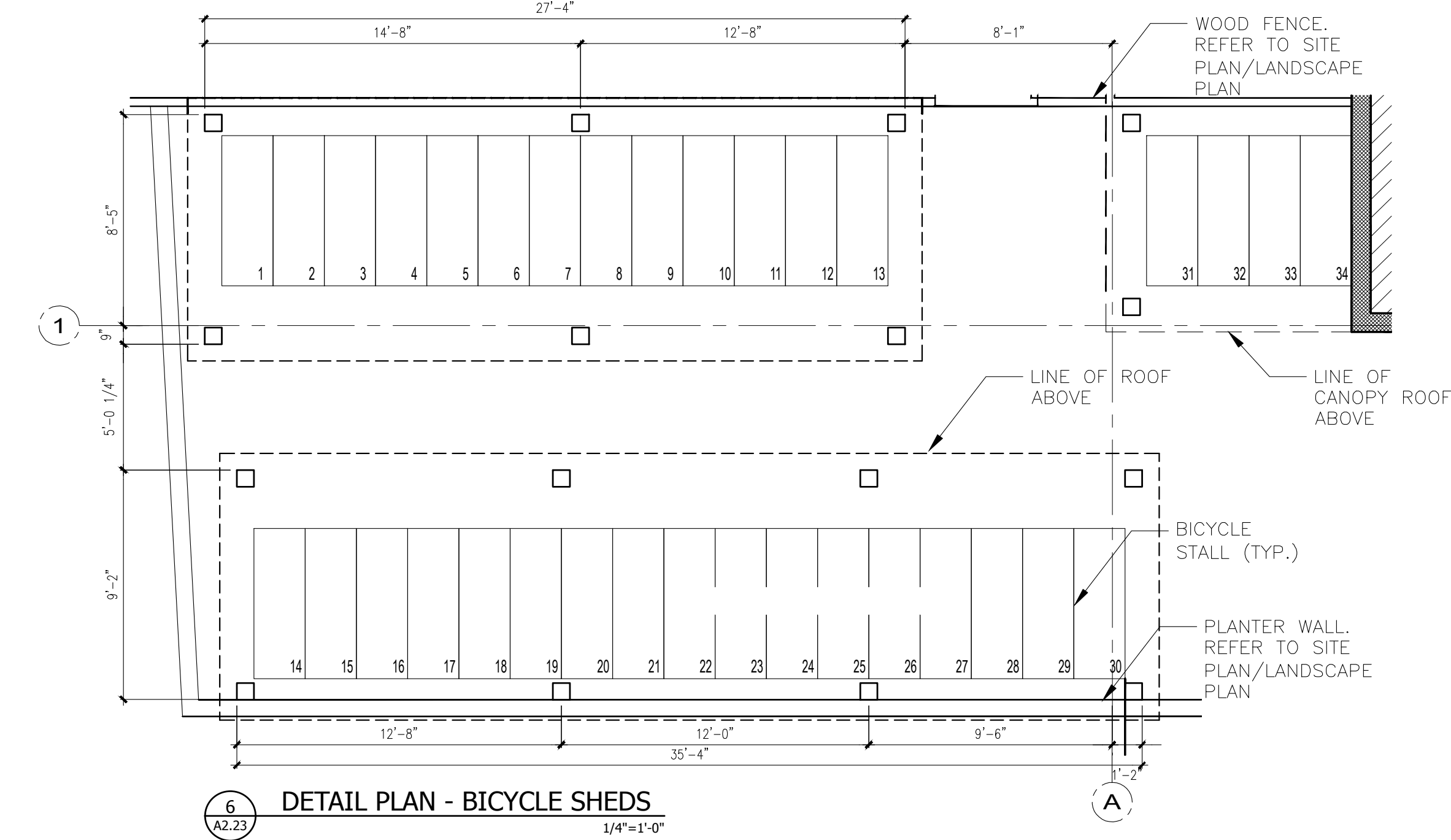
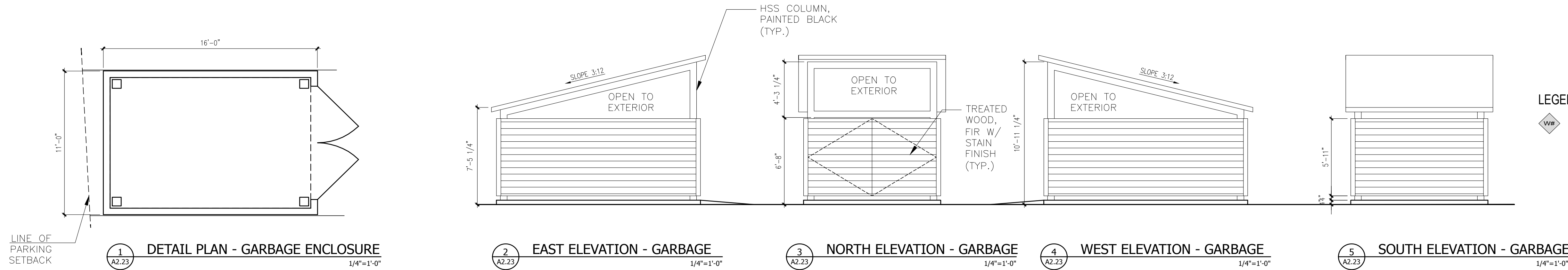
ISSUES & REVISIONS		
NO.	DATE	DESCRIPTION
001	08/03/19	ISSUED FOR

**MartinPykalArchitect**  
1113 Lovers Road  
Bowen Island, BC, V0N 1G0  
(604) 346-9595  
info@martinpykalarchitect.com  
www.martinpykalarchitect.com

PROJECT NAME  
Gibsons Supportive  
Housing  
CLIENT NAME  
BC Housing  
PROJECT ADDRESS  
739-749 School Rd  
Gibsons, BC

PROJECT NUMBER  
19-007  
STATUS  
-  
SCALE  
1/4"=1'-0"  
DATE  
27-05-2019  
SHEET DESCRIPTION  
Detail Exterior Plans

**A2.23**





## **Proposal Summary**

### **Project Within the Urban Context**

The proposed building is designed to compliment and blend into the adjacent residential neighbourhood. This is achieved through on-site program organization and the use of soft landscaping elements, including tall trees and vegetation screening. These elements are intended to provide a buffer between the on site activities as well as minimizing the impact of a 3 storey and regular shaped modular construction building on this site.

### **Environmentally, Socially, and Economically Sustainable**

This building is designed to comply with latest energy efficiency requirements of the BC Energy Step Code. It is targeted to meet Step Code level 4. Compliance is planned through meeting the required step 4 heating performance limits as well as total energy use limits. This will be achieved through implementation of effective building mechanical, electrical and building envelope systems. The project will be 'energy modelled' during design phases and upon completion to verify that targets are met. On site airtightness testing will also take place to conform compliance. The building envelope will be designed to minimize thermal bridging and control air passage.

### **Character: General Design Guidelines**

The proposed design incorporates numerous architectural and landscape features which are intended to compliment the desired small town character of this neighbourhood. Extensive landscaping, including hedges, planters, shrubs and trees will be incorporated along public street edges to soften and screen the mass of the building structure.

### **Building Scale and Massing**

By definition, modular construction lends itself to efficient linear massing. The proposed design follows this approach to address constructibility, scheduling and energy efficiency issues. With this approach taken as the starting point, every effort is made to 'soften' the impact of the massing on this neighbourhood's residential character.

### **Building Wall Design**

The longer Eastern and Western facades will be articulated with slight projections and colour differentiation to increase visual interest and de-emphasize the overall simple building massing. The shorter Southern and Northern facades will be stepped in order to accommodate stair cores and service spaces. These facades will be screened with tall vegetation.

### **Roof Design**

The building will be covered with a flat roof as is inherent to modular construction. The proposed design incorporates 'gable-end' roof edge articulations that terminate the facade stepping and introduce visual interest to the building's sky edge profile.

### **Integration with Surrounding Areas**

The site design proposes pedestrian and vehicle access via the South end of O'Shea Road. The intent of this approach is to de-emphasize entry and service use patterns away from neighbouring residences and the School Road artery. A new sidewalk will be provided along O'Shea road linking the building entrance to the intersection with School Road.

The new building is proposed to be positioned as close as possible to the intersection in order to manage the transition to the lower neighbouring single family residences.

**Sense of Place, Amenity Space and Private Areas**

This site allows for a logical separation of resident exterior activities from the street and the neighbouring properties. With a single controlled entrance and no practical gathering space on the street side, residents will be encouraged to make use of abundant outdoor gathering spaces on the property's Southern-Eastern side. Furthermore, the Southern exposure to daylight and sunshine will make these gathering spaces more appealing.

**Landscaping**

The proposal includes abundant 'soft' landscaping for the site with numerous trees, hedging, shrubs, ground cover as well as planters over hard areas. Outdoor equipment and furniture such as a moveable pet run enclosure, shade shelters, picnic tables, and benches will also be provided. All of these significant landscape elements are intended to provide privacy screening as well as allowing this building to fit well into its urban context.

**Fencing**

The site will be screened from adjacent properties by means of opaque and secure wood fencing, as well as planted hedging in key areas.

**Parking Areas and Vehicle Access**

Parking and service access is proposed to be located at the Southern end of the property, with redeveloped driveway access from O'Shea Road. The intent of this approach is to de-emphasize entry and service use patterns away from neighbouring residences and the School Road artery.

**Signage and Lighting**

Safety and security lighting will be provided at the main entrance, including the service and outdoor amenity spaces. Subtle lighting will also be provided at the building exit doors, including the North side along School Road. Landscape features will be used to screen lighting from adjacent properties and the street as much as possible.

**Central Recycling Area**

A central garbage and recycling structure is proposed to be located near the O'Shea Road driveway entrance, at the South-Western corner of the site. This structure will be landscape screened from the street.

**Stormwater Management and Drainage**

Stormwater management and drainage will be coordinated by a civil engineer.



## **739 – 749 School Road, Supportive Housing Project**

### **Multi-unit Residential Development Permit Area No. 4**

#### ***Land Use Assessment Matrix***

#### ***Submitted by the O'Shea/Oceanmount Community Association***

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## Introduction

As part of its submission to Gibsons Town and Council regarding the proposed Supportive Housing Project on School Road, the OOCA has prepared a Risk Assessment of the Project using the Town's Development Permit 4 guidelines. The project is partly within DP4 with a portion of the lands in DP8. An OCP amendment seeks to change the current DP8 area into DP4, thus we will only assess the project as a DP4.

The compliance with the DP4 criteria have been given a score from 0 (Low Risk – completely compliant) to 5 (High Risk, Non-Compliant). The criteria (extracted from the DP4 document) is stated first and then a rating is provided in a table following the criteria. We have rated 12 risk areas and at this stage have not provided a weighting to reflect areas that are more important than others. Risk ratings are a means of quantifying the project's adherence to the guidelines laid out in the DP4 document.

Naturally a more in-depth consultation would be useful to ensure all the constituents of this project had an opportunity to provide input regarding the risk ratings.

## Summary of Risk Matrix

See detailed assessments and explanations for this summary in the following document.

DPA-4 Rating Summary			
739 - 749 School Road Supportive Housing Facility			
<b>Overall Average Risk Rating</b>		<b>3.5</b>	
Risk	Rating	Weighting	Weighted Rating
1. Character: General Design Guidelines	4.5	1.0	4.5
2. Building Scale and Massing	4.7	1.0	4.7
3. Building Wall Design	4.1	1.0	4.1
4. Roof Design	4.6	1.0	4.6
5. Integration with Surrounding Areas	4.4	1.0	4.4
6. Gradual Change in Height	4.5	1.0	4.5
7. Sense of Place; Development Identity	3.8	1.0	3.8
8. Amenity Space; Private Areas	2.5	1.0	2.5
9. Landscaping	2.0	1.0	2.0
10. Fencing	3.0	1.0	3.0
11. Signage and Lighting	2.5	1.0	2.5
12. Central Recycling Area	1.0	1.0	1.0
<b>Totals</b>	<b>41.6</b>		<b>41.6</b>



## Recommendations

It is apparent from the risk analysis that the proposed development may not comply with many of the DP4 criteria. With half the criteria in the red zone (high risk), the development appears to have contravened the guidelines. We therefore recommend that the size or location of the building be changed. Further, we recommend that the architecture be modified to encompass more of the design criteria and common building elements required in the DP4 area.

Alternatively, the Town could propose a special Development Permit Area specifically created for this site and eliminate the criteria outlined in DP4.

## Proposed Project Design



*Existing building on the property*



Figure 1. 439 - 449 School Road Proposed Design



Figure 2. 439 - 449 School Road Proposed Design





1 SITE PLAN OPTION 1  
Scale: 1/16" = 1'-0"

Figure 3. 439 - 449 School Road Site Layout

## 16.5 Multi-unit Residential Development Permit Area No. 4

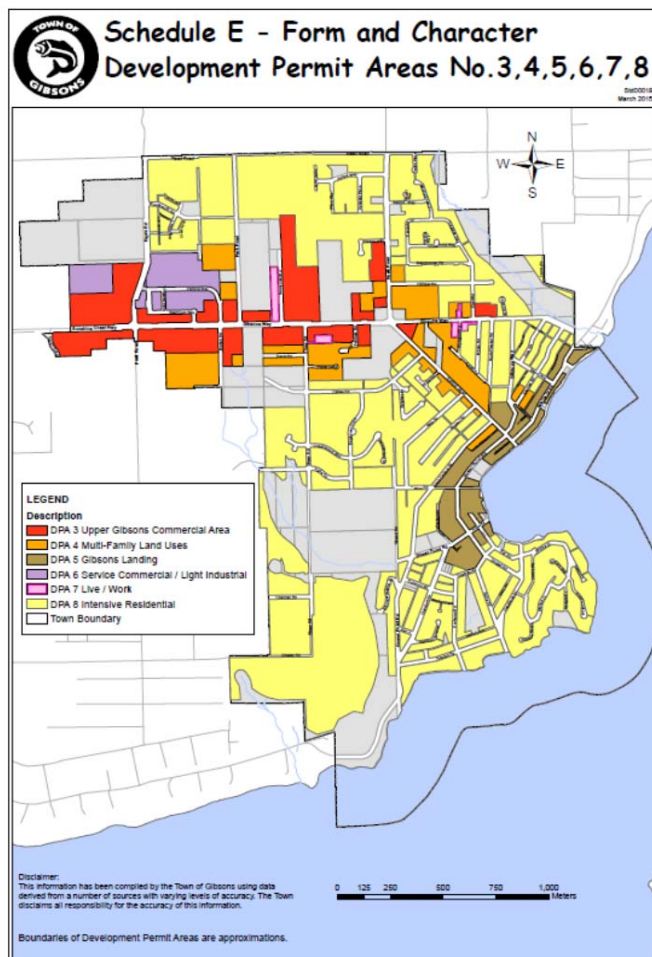
### Purpose

The Multi-unit Residential Development Permit Area is designated under Section 919.1(1)(f) of the Local Government Act to guide the form and character of multi-unit development.

### Area

The Multi-unit Development Permit Area is shown as Development Permit Area No. 4 on Schedule E. NOTE: These guidelines do not apply to single-detached or duplex homes.

### Form and Character Development Permit Areas Map





## Justification

The objective of the Multi-unit Residential Development Permit Area designation is to ensure that a high standard of design, landscaping and building form is implemented for any multi-unit residential development.

The guidelines are aimed at ensuring that new development is appropriate to its surroundings, and is compatible with surrounding uses or neighbourhood character. The Development Permit guidelines are also intended to ensure that multi-unit residential development is attractive for future residents.

## Guidelines

### 1. Character: General Design Guidelines

Multi-unit developments should reflect the following design elements which are key components contributing to the form and character of development which “creates” the Gibsons character:

- *Development should promote a small town character by encouraging architecture, landscape design and environmental settings that respect the surrounding context.*
- *Public street edges which are characterized by low (less than 3.5 feet high), neighbourly fences, combined with extensive landscape materials at the private edge.*
- *Residences oriented towards the street with well-defined and welcoming entries at the street edge.*
- *Construction materials should reflect the West Coast Design and Setting.*
- *Each building should appear unique or easily distinguishable from neighbouring buildings.*
- *Simple exterior detailing with earth-tone colours – and primary colours only as accents.*
- *Buildings should be oriented to maximize solar exposure while minimizing shadow impacts on adjacent buildings and common areas.*
- *Common building elements which include:*
  - ☐ *pitched roof line*
  - ☐ *dormers*
  - ☐ *porches*
  - ☐ *low building profiles, simple residences, set well back from the roadway and nestled into the landscape*

Changes in the building facades and the massing of buildings add a human scale and visual richness to the development. Long, unbroken building lines and rooflines are to be avoided. New developments should create visual interest by providing variations in building height and massing as follows:

- *The inclusion of elements such as bay windows, dormers, porches and cross gables help mitigate the visual impact of larger buildings.*
- *Steeper roof pitches and stepping down of roof lines to vary the height and rooflines of buildings is recommended.*
- *Offsetting and modulating wall lines along the building elevation to allow smaller building sections to stand out. The overall building footprints on the site shall be modulated to avoid monotony and repetition and to avoid wall-like massing.*

○

Assessment Remarks	Rating Range	Rating
<p>This design has the general appearance of a large box with an institutional look. It is a 3 storey apartment building located next to single family homes and as such overpowers the immediate neighbourhood. It doesn't provide any of the common building elements suggested in the guidelines.</p> <p>The wall-like massing of the building specifically goes against the guidelines of DP4</p> <p>The proposed cladding colours and the design feature on the top do add some character but it is clear there was no attempt to comply with Gibsons vision of a seaside village character.</p> <p>See Figures 4 and 5 which are examples of buildings that provide more seaside character.</p>	0 – 5	4.5



Figure 4. Stonehurst Classic Architecture



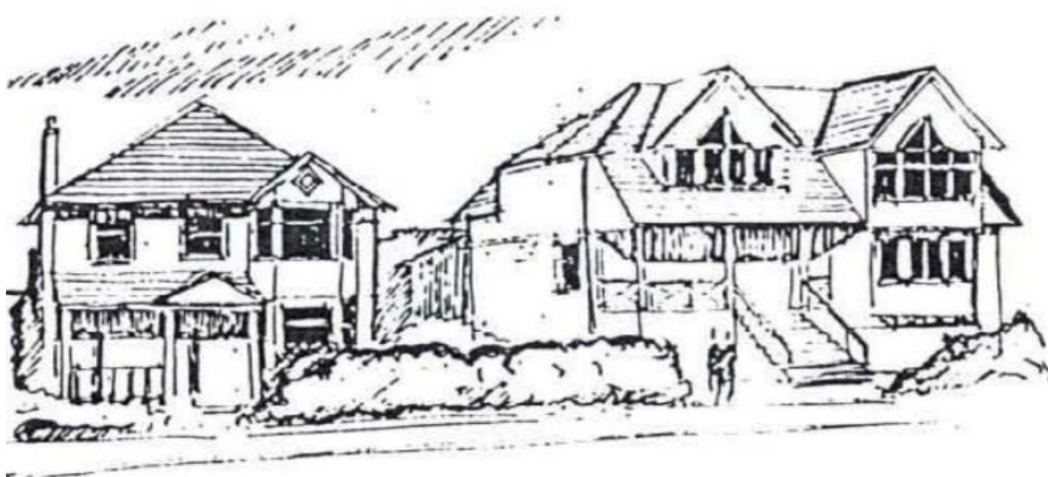
Figure 5. Marina House Classic Architecture



## 2. Building Scale and Massing

To compliment adjacent single-detached neighbourhoods and reduce impacts of building massing, the following guidelines shall apply:

- *Larger developments should be separated into smaller groups or clusters of units to promote a sense of belonging and neighbourliness and to maintain a residential scale and image.*
- *Townhomes should be designed in clusters of 25 units or less based on a single entry point.*
- *Apartment-style developments based on a single entry should have 60 units or less.*
- *Very large single buildings more than 70metres in length, or townhomes with more than six joined units are to be avoided.*



Respect for the existing streetscape

Assessment Remarks	Rating Range	Rating
The building scale and massing contravene the guidance that indicates large single buildings should be avoided. In this case there are two large 3 storey buildings proposed to be built side by side.	0 – 5	4.7

## 3. Building Wall Design

The general character of the development should reflect aspects of Gibsons' semi-rural coastal setting by using natural and typical local wall materials including wood siding, wood shingles, stucco, stone and brick.

The number of materials used on the building exterior must achieve a balance between achieving visual interest and complexity without overpowering the surroundings.

Assessment Remarks	Rating Range	Rating
The building as presented would be out of balance with the neighbourhood and its difficult to see the semi-rural coastal setting from the wall materials proposed. It appears there will be large blocks of wall board in solid colours. Given the boxlike nature of the design, it's difficult to detect visual interest or complexity. .	0 – 5	4.1

#### 4. Roof Design

The design of the roof of multiple-unit residential dwellings has a major influence on the overall character of the development. The “roofscape” is a key design feature, which is of critical importance to Gibsons, especially areas of lower Gibsons, because of its potential to be viewed from above (as a result of the varied topography of Gibsons). The following guidelines shall apply:

- *The roof form should have a sloped appearance large areas of flat roof will not be acceptable in low or medium density multiple unit residential developments except in the case of a green (vegetated) roof.*
- *The roof form should be modulated and broken up with dormers, skylights and other architectural features.*

*A continuous unbroken ridge line should be avoided.*

- *Roof lines should include steep pitches typical of west coast building forms.*
- *Secondary hipped or gabled roofs are preferable to flat roofs or mansard roofs, or segments of pitched roofs applied to the building's edge.*
- *Roofing materials may be metal, cedar shakes, concrete tiles or asphalt shingles.*

Assessment Remarks	Rating Range	Rating
The roof architecture is non-compliant in that it is a large area of flat roof which is specifically prohibited. The top design feature appears to be an attempt to break up the very straight flat lines. The roofing materials used in the flat roof (usually tar and gravel) are not listed items.	0 – 5	4.6

#### 5. Integration with Surrounding Areas

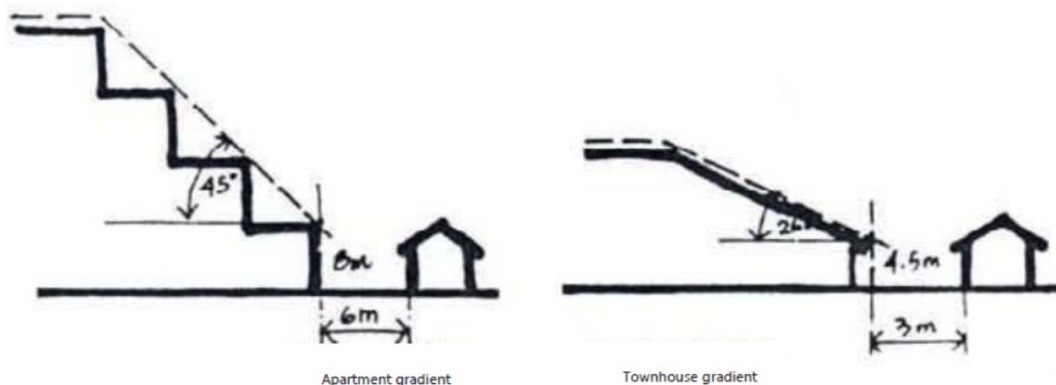
- *New developments should reflect elements of the existing neighbourhood and the prevailing residential streetscape. This may require recessing of parking areas, creation of gabled entries or porches, and highlighting individual front door entries to be similar to those on neighbouring lots. For larger developments, this may require separating the units into smaller components.*



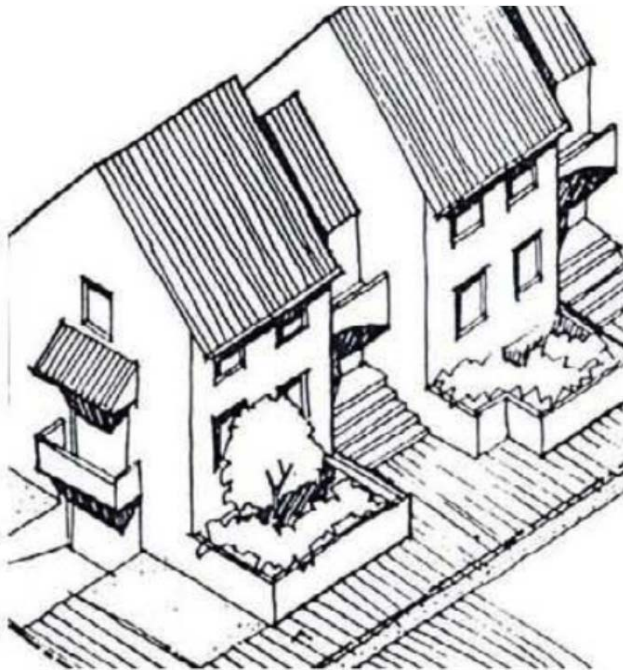
- New residential buildings should not in general, be much larger than the surrounding buildings. A graded transition in the building height is desired to ensure adjacent properties are not confronted with a “wall”.
- Additional setbacks may also be required to achieve this transition.
- Roof lines should be stepped down from building ends to reduce the apparent mass of the building.
- The end units of new developments at road edges should not be more than one to two stories in height to establish a single-detached residential appearance in multi-unit residences.
- All lots must have direct access to the larger pedestrian circulation system via park corridors, pathways, and/or sidewalks.
- Developments on sloped properties should be terraced with the natural slope of the land, and should avoid the use of high [over 1.2 m (4 feet)] retaining walls.
- New developments should be oriented to best utilize natural light, southern exposure, and views of adjacent natural features, and to minimize loss of views and shadows cast on adjacent uses. This may require increased setbacks or terracing of buildings.

Assessment Remarks	Rating Range	Rating
<p>The building is much larger than the surrounding buildings which are mostly single family homes. Roof lines are not stepped down and the end units at road edges are more than two stories tall.</p> <p>Elements of the existing neighbourhood are missing. The front door entry is at the side of the building and there appears to be just one entry point as opposed to “highlighting individual door entries”.</p> <p>The buildings don’t integrate with the surrounding neighbourhood due to the modular flat roof design, building massing and height.</p>	0 – 5	4.4

## 6. Gradual Change in Height



Front entrances create a sense of belonging to the neighbourhood



Apartment gradient Townhouse gradient

Assessment Remarks	Rating Range	Rating
This is a modular building without any change in height or front entrances that create a sense of belonging.	0 – 5	4.5

## 7. Sense of Place: Development Identity

With the increased density of multi-unit housing; creating a sense of “place”, of neighbourhood, and of privacy within the development are importance design features. New multi-unit developments will be reviewed for features which help create these qualities. The following guidelines apply to new developments:

- *The design of developments into smaller areas where residents share smaller parking areas, pathways and other common areas creates a sense of belonging within a larger development.*
- *Multi-unit homes should provide a street orientation through features such as major entry points to provide a sense of belonging to the neighbourhood. Street level landscaping creates privacy within the development. Parking areas should be recessed to allow the pedestrian entry to predominate.*



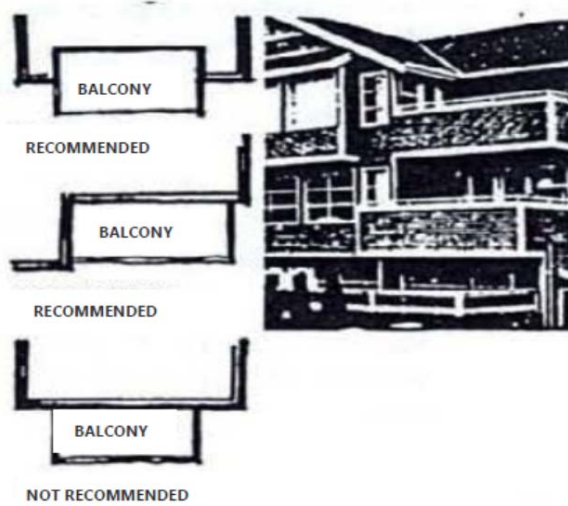
- *Townhome projects on major arterials may have private rear yards facing the street, but should create a streetscape of entrances within the private roadway or courtyard area.*

Assessment Remarks	Rating Range	Rating
This development could produce a sense of place for the residents due to the rear courtyard. There is no parking for the residents and with just the one entrance, the streetscape of entrances doesn't exist.	0 – 5	3.8

## 8. Amenity Space; Private Areas

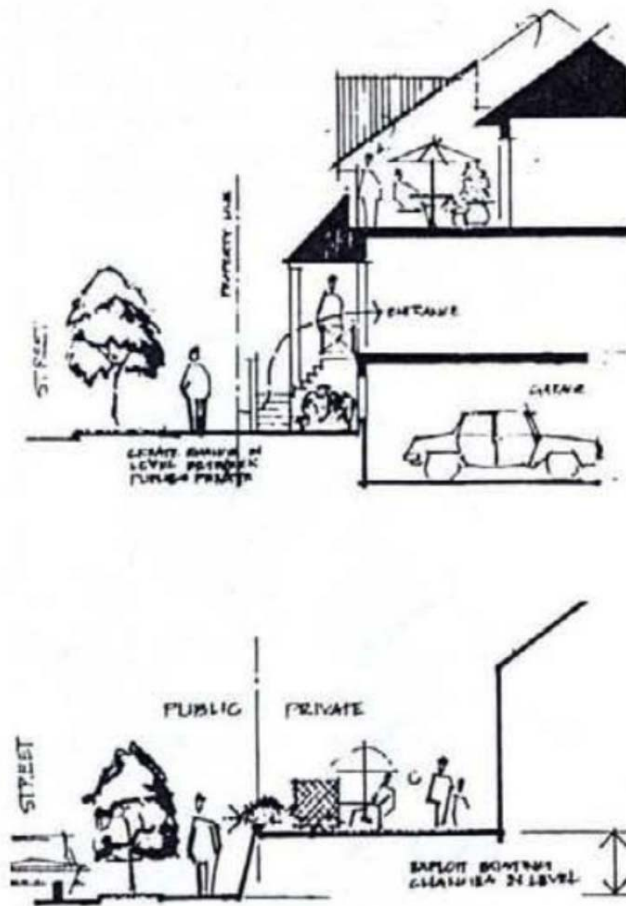
The location and size of outdoor spaces such as patios and balconies have considerable effect on the sense of privacy. New residential buildings should be designed to provide privacy for each resident; through windows, private outdoor spaces or balconies, or through changes in grade or elevation as follows:

- *An outdoor living space of minimum 5 m depth for townhouses and minimum size of 37 m<sup>2</sup> (400 square feet) is recommended.*
- *Apartments should have a minimum 3 m depth terrace or balcony, sufficiently large to create a usable outdoor “room”. Balconies should be at least half enclosed in order to give the occupant privacy, security and weather protection.*
- *Dwelling units to be “clustered” in smaller groups to create more resident interaction and neighbourly surveillance.*
- *Changes in grade can provide for private areas between street edges and the development units.*



Balconies to create outdoor rooms





Changes in grade separate the public and private areas

Projects should provide meaningful and appropriate amenity space areas, and may consist of indoor or outdoor recreation areas, landscape features such as benches, gardens or plazas, children's play areas, social meeting rooms, or specific recreations features such as tennis, swimming or walking trails. These common areas contribute significantly to the quality of life in multi-unit developments, where private yard areas are not available for these activities.

Assessment Remarks	Rating Range	Rating
There is some compliance with amenity space and private areas criteria. A cafeteria and common community spaces, including a back garden/sitting area are planned. The buildings do not have balconies to provide a useable outdoor room.	0 – 5	2.5

## 9. Landscaping

Landscaping and open space areas are an essential part of the livability of multi-unit residential developments, and their integration into neighbourhoods. The following guidelines refer to the landscaping surrounding the development, and the common areas within the development:

- *All Development Permit applications must provide a professional landscape plan.*
- *Trees should be planted and maintained by the property owners along street frontages of new multi-unit developments to create a mature treed "boulevard" type of streetscape. (Spacing will vary by species used, however, a rule of thumb for tree spacing is a minimum of 8.0 metres.) This may be supplemented by other lower ornamental plantings.*
- *Native or hardy landscape species are preferred over exotic species; a mix of coniferous and deciduous species is recommended to provide effective landscaping though the seasons. Willows, bamboo and other invasive species are not recommended.*
- *Trees, or a combination of landscape and architectural features shall be used to define the gateway or entrance to a development. Landscaped entrances however, should be low-level for better security at entrances.*
- *Clusters of trees, ponds, or other landscape features should be used within the development to create a meaningful common area. Central areas or courtyards should be usable and inviting to residents as a meeting place, rather than random plantings of grass and shrubs. Seating areas and appropriate lighting should be provided within these common areas. Landscaping should also create a sense of enclosure and privacy for these spaces.*
- *Large areas of uncharacteristic materials such as bark mulch, gravel, river rock and ground cover are to be avoided, and should be combined with a variety of plant materials.*
- *Wherever possible, natural vegetation should be retained or enhanced as a feature of the development. This is particularly important where natural features such as streams or steep slopes are a component of the development.*
- *All public and semi-public areas should be landscaped, including entrance driveways, areas surrounding parking spaces or structures.*
- *Additional landscaping depth, denser vegetation and noise barriers such as earth berms should be used where a development abuts a major roadway.*

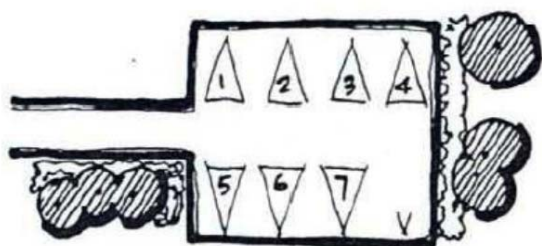
Assessment Remarks	Rating Range	Rating
Figure 3 indicates some compliance with landscaping criteria. Detailed landscaping plans would provide more data. The existing trees along O'Shea add to the privacy and should be retained.	0 – 5	2

## 10. Fencing

Fencing design for multi-unit residential areas should provide privacy to the individual units or developments without creating solid walls along the street edge. The following guidelines respecting fencing and landscape shall apply:



- *Fences along streets should not provide a continuous wall or high barrier to the street, but should be lower profile and broken at intervals to provide pedestrian linkage and views to the street.*
- *Any fencing located along a street edge should not exceed a length greater than 20 metres without a substantive break or jog.*
- *Fencing should not exceed a height of 1.2 metres within any part of the required front yard setback.*
- *Fencing along the street edge should be supplemented with low profile landscape plantings.*
- *Parking Areas and Vehicle Access Parking and driveways entrances should be designed to minimize impact on surrounding uses, the pedestrian character of the street and the internal appearance of the development, according to the following guidelines:*
- *Walkways and surface parking areas should be well lit and located in an area which is observable by residents.*
- *Entrances to parking garages should be located in areas visible from habitable room windows and well lit.*
- *Where possible, parking areas should be located in underground structures. Small groups of parking spaces throughout the development, located near to entry doors are preferable to large, central parking areas.*



Small, screened parking

- *Developments should be designed to prevent parking areas, carports or garages from dominating the internal open space areas; parking should be recessed from the main building edges.*
- *Parking areas should be landscaped and screened, but sufficiently visible to provide security to residents and vehicles.*
- *Distinct, visible visitor parking areas should be provided near the entry to the development.*
- *Site design should provide for emergency vehicles, moving vans and service vehicles, and should locate this use to minimize noise impacts on residents and adjacent uses.*

Assessment Remarks	Rating Range	Rating
Figure 3 does not indicate fencing. Parking is adjacent to the neighbouring property. Discussions indicated that in lieu of fencing, trees would be planted. Final configuration is unknown.	0 – 5	3.0

## 11. Signage and Lighting

Signage and lighting shall meet the following guidelines:

- *The size, siting and style of signage shall not be obtrusive or present a cluttered image.*
- *Entry signs shall be placed at or below eye level and shall be integrated with landscaping or other feature.*
- *Site lighting of all developments should be designed so that it avoids "light-spill" upon adjoining low density residential lands and of the night sky.*

Assessment Remarks	Rating Range	Rating
Figure 3 did not provide sufficient detail to evaluate this as positive or negative compliance.	0 – 5	2.5

## 12. Central Recycling Area

New developments are encouraged to provide residents with Central Recycling Areas suitable for the disposal of a variety of materials.

Assessment Remarks	Rating Range	Rating
Assumed compliant.	0 – 5	1.0

## 13. Stormwater Management and Drainage

For developments in the Upper Gibsons Neighbourhood Plan area the following guidelines regarding stormwater management apply:

- *Building foundations must be chosen and designed such that they do not require a subsurface connection to the municipal system. They must be designed for on-site drainage, or day-lighting into the fronting municipal surface conveyance system.*
- *No subsurface habitable floor space (MBE) is permitted because it would affect drainage infrastructure requirements and cost.*
- *Roof leaders / downspouts shall drain to ground surface on-site. No direct piping is permitted to the municipal drainage system.*
- *Lot grading shall ensure drainage away from the building and towards the municipal drainage system, or adjacent greenbelts. Where required, side / rear yard swales shall be provided to prevent cross lot drainage.*
- *Where on-site stormwater detention is required, both surface and sub-surface techniques may be used. The provision of stormwater detention does not reduce the requirement for Best Management Practices (BMP). Runoff must first be treated / managed by BMP techniques prior to entering the detention facility, according to applicable standards under the Subdivision Bylaw.*

Assessment Remarks	Rating Range	Rating
Not Upper Gibsons Neighbourhood Plan area – Not Applicable	0 – 5	N/A





# DEVELOPMENT PERMIT

NO. **DP- 2019-12**

TO: **BC Housing Management Commission**

ADDRESS: **Suite 1701-4555 Kingsway  
Burnaby, B.C. V5H 4V8  
(Permittee)**

- 1) This Development Permit is issued subject to compliance with all of the Bylaws of the Town of Gibsons applicable thereto, except those specifically varied or supplemented by this Permit.
- 2) The Development Permit applies to those "lands" within the Town of Gibsons described below:

**Parcel Identifier: 009-612-084**

**Legal Description: LOT 1 BLOCK 1 DISTRICT LOT 686 PLAN 9933**

**Civic Address: 749 School Road**

- 3) The lands are within Development Permit Area No. 4 for form and character.
- 4) The "lands" described herein shall be developed strictly in accordance with the terms and conditions and provisions of this Permit, and any plans and specifications attached to this Permit which shall form a part thereof.
- 5) The building form and character is required to conform to the following plans:
  - Development Plans titled: *New Supportive Housing Facility 739-749 School Road, Gibsons, BC*, dated June 4, 2019 and prepared by Martin Pykalo, Architect.
  - Site Plan titled: *Site Plan A1 – Gibsons, Supportive Housing*, dated May 27, 2019 (received August, 2019) by Martin Pykalo
  - Landscape Plans titled: *Landscape Plan (L1) 739-749 School Road*, dated July 8, 2019 and prepared by PMG Landscape Architects
- 6) In conjunction with the plans outlined under 5, the following further specifications apply:
  - *< list conditions or last minute changes approved for the permit >*
- 7) This Development Permit applies to the form and character on the site. For details shown in off-site areas the plans may be subject to change following the provisions of a Servicing Agreement.

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- 8) Minor changes to the aforesaid drawings that do not affect the intent of this Development Permit or the general appearance of the buildings and character of the development may be permitted, subject to the approval of the Director of Planning.
- 9) If the Permittee does not commence the development permitted by this Permit within twenty four months of the date of this Permit, this Permit shall lapse.
- 10) This Permit is NOT a Building Permit.
- 11) As a condition of the issuance of the Building Permit, Council requires that the Permittee provide security for the value of \$ **XX,XXX** to ensure that the on-site landscaping component of the development is carried out in accordance with the terms and conditions set out in this permit.
  - (a) The condition of the posting of the security is that, should the Permittee fail to carry out the development hereby authorized according to the terms and conditions of this Development Permit within the time provided, the Town may carry out the development or any part of it by its servants, agents or contractors and deduct from the security all costs of so doing, it being understood that the surplus, if any, shall be paid over to the Permittee.
  - (b) If on the other hand, the Permittee carries out the landscaping component of the development permitted by this Development Permit within the time set out herein, the security shall be returned to the Permittee.
  - (c) Prior to issuance of a Building Permit, the Permittee is to file with the Town an irrevocable Letter of Credit or Certified Cheque as security for the installation of hard and soft landscaping in accordance with approved plans, such Letter of Credit to be submitted to the Town at the time of the Building Permit application.
  - (d) The Permittee shall complete the landscaping works required by this permit within six (6) months of issuance of the Building Permit.
  - (e) If the landscaping is not approved within this six (6) month period, the Town has the option of continuing to renew the security until the required landscaping is completed or has the option of drawing the security and using the funds to complete the required landscaping. In such a case, the Town or its agents have the irrevocable right to enter into the property to undertake the required landscaping for which the security was submitted.
  - (f) Upon completion of the landscaping, a holdback of 10% of the original security, plus any deficiencies, will be retained for a 1-year period, to be returned upon approval from the Landscape Architect and the Director of Planning.
  - (g) The following standards for landscaping are set:
    - (i) All landscaping works and planters and planting materials shall be provided in accordance with the landscaping as specified on the Site Plan and Landscaping Plan which forms part of this Permit.



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- (ii) All planting materials that have not survived within one year of planting shall be replaced at the expense of the Permittee.

AUTHORIZING RESOLUTION PASSED BY COUNCIL

THIS THE XX DAY OF <month>, 2019.

ISSUED THIS \_\_\_\_ DAY OF \_\_\_\_\_, 2019.

\_\_\_\_\_  
Bill Beamish  
Mayor

\_\_\_\_\_  
Lindsey Grist  
Corporate Officer

DRAFT