



Render from Gibsons Way

Aerial Context Photo



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1 Background

Project Overview

The proposed development is located at 1057 Gibsons Way at the South-East corner of Gibsons Way (Hwy #101) and Pratt Road in Upper Gibsons. This general corridor is comprised of a mix of established commercial and light industrial businesses as well as more recent mixed-use residential and commercial developments. Notably, the site is cross-corner to the Gibsons Park Plaza shopping centre and is immediately accessible via Highway #101, the #1 Langdale Ferry/Sechelt Transit Route and #90 Langdale Ferry/Sechelt Express Transit Route which provide convenient connectivity throughout Gibsons and beyond.

The proposed development provides much needed housing units, commercial/retail business, and Live-Work opportunities in a key growing area of the Town of Gibsons where a wide variety of services is accessible within walking distance and via transit. Elphinstone Secondary School and Gibsons Elementary, the Gibsons & Area Community Centre, White Tower Park, and Brothers Park are all conveniently located within approximately 1.0 km of the subject site.

Site Description

The project site is comprised of:

Building A

- Five-storey mixed-use building
- 3 Commercial Retail Units (total gross area 3,061 sq. ft.)
- 3 Live-Work units
 - total commercial area 3,086 sq. ft.
 - total residential area 2,864 sq. ft.
- 64 residential apartment units.
 - 26 two-bedroom units
 - 23 one-bedroom units
 - 15 studio/bachelor units
- 36 residential underground parking stalls in P1.
- 38 residential surface parking stalls
- 6 surface visitor parking stalls
- 14 surface commercial/live-work parking stalls
- 1 Loading stall
- 2 Accessible parking stalls (included in above calculations)
- 83 Class 1 Bicycle spaces and 14 + 4 Class 2 Bicycle spaces
- 9 EV Chargers as per bylaw in addition to rough-in for additional EV chargers in parkade

Building B

- Five-storey mixed-use building
- 2 Live-Work units
 - total commercial area of 2,934 sq. ft.
 - total residential area of 1,962 sq. ft
- 77 residential apartment units
 - 24 two-bedroom units
 - 43 one-bedroom units
 - 10 studio/bachelor units
- 36 residential underground parking stalls in P1
- 51 residential underground parking stalls in P2
- 5 visitor surface parking stalls
- 5 surface live-work parking stalls
- 1 Loading stall
- 4 Accessible parking stalls (included in above calculations)
- 101 Class 1 Bicycle spaces and 16 Class 2 Bicycle spaces
- 10 EV Charges as per bylaw in addition to rough in for additional EV chargers in parkade

The proposed development will provide much needed housing to the community, as well as commercial and Live-Work spaces at the ground floor suitable for local businesses, retail, professional services or medical practices, and restaurants. Residents will benefit from the diversity of unit mix, including studios, one bedroom and family-sized two-bedroom units. There will also be features such as a shared fitness center and two amenity rooms, one in each building.

2 Design Rationale

Site Planning

Various scenarios were examined in determining the site layout, in balance of several goals: ease of access from main roads, visibility of commercial uses, mature tree retention, using the slope to advantage, and providing residents with privacy and views. Commercial units were focused along the northern axis of the site, parallel to Gibsons Way.

The main points of entry to the site are off of Pratt Road and Gibsons Way, at the appropriate distance from the main intersection to avoid congestion. The access and parking directly off of Gibsons Way facilitates parking for commercial uses, while the other access provides a direct route to the parkade entries and surface parking for residents at the lower levels of both buildings. The significant grade change on the lot also facilitates the entry for the second level of Building B's parkade to be at the south end of the lot. The shape of the lot lends itself to the proposed configuration, with the length parallel to Gibsons Way for Building A and perpendicular for Building B.

Pedestrian connections to Gibsons Way are provided and are set back from being directly by traffic through means of a landscape buffer, with the exception of the existing bus stop lay-by connection. This allows for both the public to walk to the commercial uses and residents to have easy walking access to other services located nearby. Bicycle lockups are conveniently located to allow residents easy access to and from the site. Waste and Recycling collection will take place entering off Gibsons Way and stopping and proceeding south.

Mature trees on the site to be retained are located at the corner feature of Pratt Road and Gibsons Way, as well as at the entry off Pratt Road, and also occupy the south end of the lot.



Architecture

The buildings are designed with a two-storey commercial and live/work element fronting Gibsons Way and the residential suites set back from this, providing both separation for the residents and a break in the massing. Stonework and cement board paneling is used as cladding to distinguish the commercial and live/work areas from the residential suites, which are clad with lap siding. Additionally, the façades are stepped at different heights over the commercial portions and at the residential entry to distinguish the different suites and break up the massing. The corner façade which faces both Gibsons Way and Pratt Road was raised the most to highlight this important intersection and create a sense of place, grounded by the retained trees and the proposed plaza area (see Landscape Plan) The arrangement of residential suites dictates the shape of the upper floorplates, allowing for jogs in the building which break up the massing and add private nooks for residents to enjoy their outdoor spaces.



The two buildings are articulated with subtle differences in colour. Building A commercial areas have white paneling while Building B have a cream-coloured paneling and more stonework. The residential entries are signalled by the navy-blue paneling on each entrance façade, and the building masses are broken up with cladding colour changes. Horizontally, the commercial areas have a banding of dark grey just above their awnings, to which their unit signage is affixed.



Rendered View of Building A from Gibsons Way & Pratt Road



Rendered View of Building B from Gibsons Way



Views of Existing Neighbourhood along Gibsons Way:

North Side of Gibsons Way at Payne Road: IGA/Starbucks/Mall



North Side of Gibsons Way, East of Payne Road: Wendy's/Tim Hortons/Veterinary Clinic



South Side of Gibsons Way, West of Pratt: Industrial



Pratt Road, South of Site: The Brick



South Side of Gibsons Way, East of Site: Auto parts & Dance



North Side of Gibsons Way, East end of Site: Industrial



Existing Site Viewed from Gibsons Way



Existing Site Viewed from Pratt Road



Project Colours & Finishes:

Natural tones were used in the following colours:

White, Brown, Dark Blue, Beige, Dark Gray



FINISHES

	<p><u>BUILDING A - Commercial entry</u></p> <p>Lap siding - White</p> <p>Lap siding - Dark Blue</p> <p>Lap siding - Brown</p> <p>Panel siding - Dark Blue</p> <p>Stone veneer cladding - Beige</p> <p>Panel siding - Dark Gray</p> <p>Panel siding - White</p> <p>Stone veneer cladding - Beige</p>
	<p><u>BUILDING A - East side</u></p> <p>Lap siding - White</p> <p>Lap siding - Brown</p> <p>Panel siding - Dark Gray</p> <p>Panel siding - White</p> <p>Stone veneer cladding - Beige</p>

	<p><u>BUILDING B - Residential entry</u></p> <p>Panel siding - Dark Blue</p> <p>Lap siding - White</p> <p>Lap siding - Brown</p> <p>Stone veneer cladding - Beige</p>
	<p><u>BUILDING B - Live/Work</u></p> <p>Soffit - Brown</p> <p>Panel siding - Dark Blue</p> <p>Panel siding - Dark Gray</p> <p>Panel siding - Beige</p> <p>Stone veneer cladding - Beige</p>



<p>Infrastructure servicing</p> <p>Servicing for this development project is proposed to extend from offsite municipal infrastructure located within both Pratt Road and Gibson Way. A gravity sanitary service connection is proposed to extend across the street to the property line from the existing sanitary main located on the north side of Gibsons Way. Grade of the site necessitates that an onsite low pressure sanitary system will be required to pump flows up to discharge to the service connection at the property line. A mechanical pump is anticipated to be required to pump flow from both buildings.</p> <p>Water servicing to the site is proposed to extend south from Seamount Way across Gibsons Way to the site. A water meter chamber is anticipated at the property line after which water servicing is proposed to split into dedicated fire and domestic service mains extending to each of the two buildings. An upgrade to the existing offsite water mains connecting between the site and the intersection of Seamount Way and Industrial Way is anticipated to be required to provide adequate fire flow for the development.</p> <p>Runoff from the site is proposed to be managed in two different ways. Grade of the site does not permit collection of runoff from the entire site to be directed to the property via gravity. The portion of runoff that can be discharged via gravity is anticipated to be collected and directed towards a detention tank located under the drive aisle in the southwest corner of the site. Flow from the site will be controlled using a flow control system and limited to pre-development levels. All flow leaving the site will pass through an oil separator water quality device prior to discharge. Flow discharged offsite is anticipated to be directed to the existing ditch on the east side of Pratt Road. Runoff from the area of the site that cannot be discharged from the site via gravity is proposed to be directed to a rock pit infiltration area located in the southeast corner of the site.</p>	<p>Landscape</p> <p>PMG Landscape Architects has developed the overall landscape design at 1057 Gibsons Way with respect to the natural context and conditions of the site, while also recognizing the Sunshine Coast’s changing climate by planning for landscape resiliency going into the future.</p> <p>A key priority for the landscape design was to save as many of the existing trees on site as possible. Trees such as these will be invaluable as land along Gibsons Way continues to develop, providing shade and contributing to the space’s sense of place, preserving the natural history of the Sunshine Coast. The planting design supports the goal to retain 23 on-site trees, and to prioritize native plants where appropriate. Our planting design also recognizes the quickly changing climate of the Sunshine Coast. We have limited the use of lawn. In areas where the planting plan deviates from native selections, we have specified plants that are drought tolerant, and will thrive in future climate models for the area. The planting throughout the site also seeks to support pollinators, particularly through the seeding of a native grass and wildflower mix that will cover the South and East landscape buffers.</p> <p>For visitors and residents of the site, the design includes ample bike parking facilities nearby all major entrances and gathering areas. There is an informal plaza at the Northwestern corner of the site, giving the public an opportunity to sit and rest along Gibsons Way. For residents, we have provided two pet relief areas, which will support those residents with active pet-oriented lifestyles, as well as provide informal opportunities for interaction between residents.</p>
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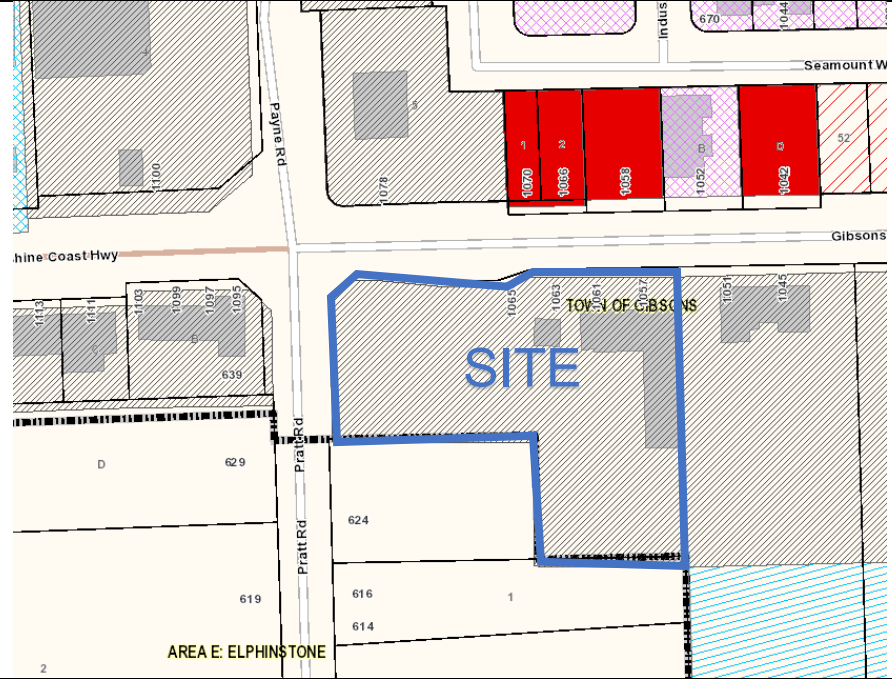
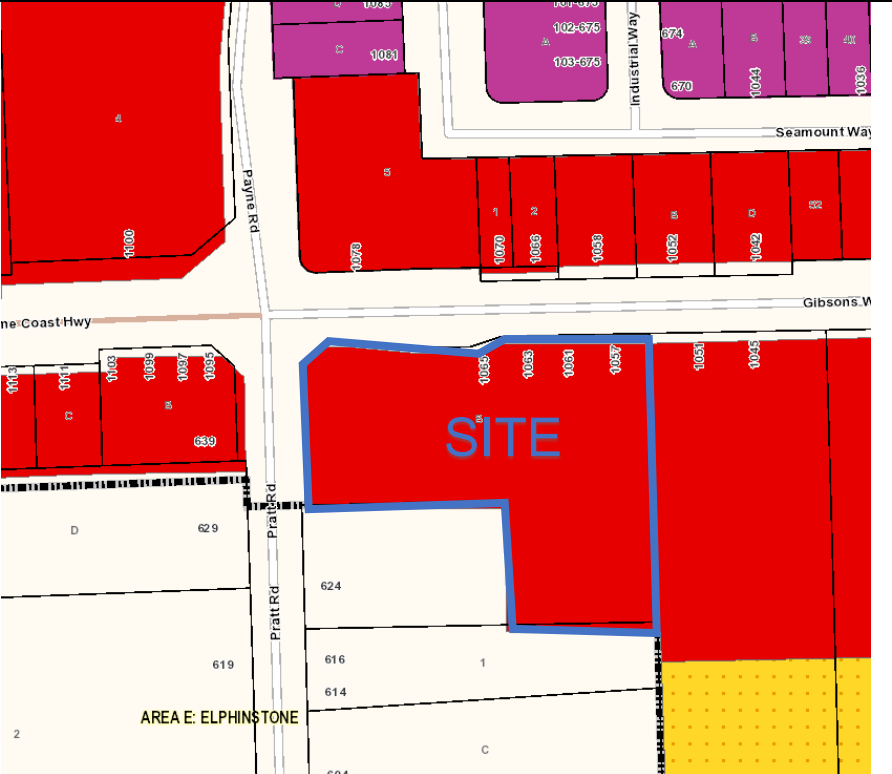
3 Sustainability

The project incorporates various strategies for achieving a sustainable footprint:

- Vehicle usage - The project proposes lower parking ratios (based on the mixed-use transit-oriented nature of the development site) and offers a variety of secure, indoor-bicycle parking stalls, including some oversized spaces and outlets for e-bikes.
- A multifamily project, through densification, allows for lower land, energy, and water usage per occupant.
- The location and mixed-use nature of this development (including Live-Work), as well as proximity to amenities, groceries, schools, employment, and transit routes, along with the lower parking utilization, are inherently sustainable as they reduce reliance on personal vehicles. There are numerous examples of communities who are relaxing, if not removing entirely, minimum parking requirements in order to support housing and help address climate change.
- Landscape Planting – Preference for indigenous and drought tolerant plantings; create an environment that is an attractive habitat.
- Storm water management will be designed by a professional engineer and will include on-site stormwater detention as well as oil separators.
- Reduce urban heat island effect - through landscaping and mature tree retention.
- Water conservation and efficiency – use low maintenance native plantings, low flush toilets, low-flow plumbing fixtures.
- Energy – All units will be constructed with use of high-performance window glazing and envelope assembly, and optimized heating and cooling strategies through energy modeling under the advice of consulting Engineers. The goal is to achieve Step 3 of BC's Energy Step Code.
- Materials – use of durable materials, avoid use of disposable finishes, use of formaldehyde free and low VOC products.
- Avoid Light Pollution – minimize glare towards sky and neighbours by using downcast lighting fixtures.
- Site Waste Management – implement construction waste recycling program.
- Air Quality – Natural cross ventilation and filtered conditioned air, carbon monoxide monitoring.
- Indoor Environment – Low-emitting materials, Controllable ventilation and lighting systems, natural day lighting, access to views.

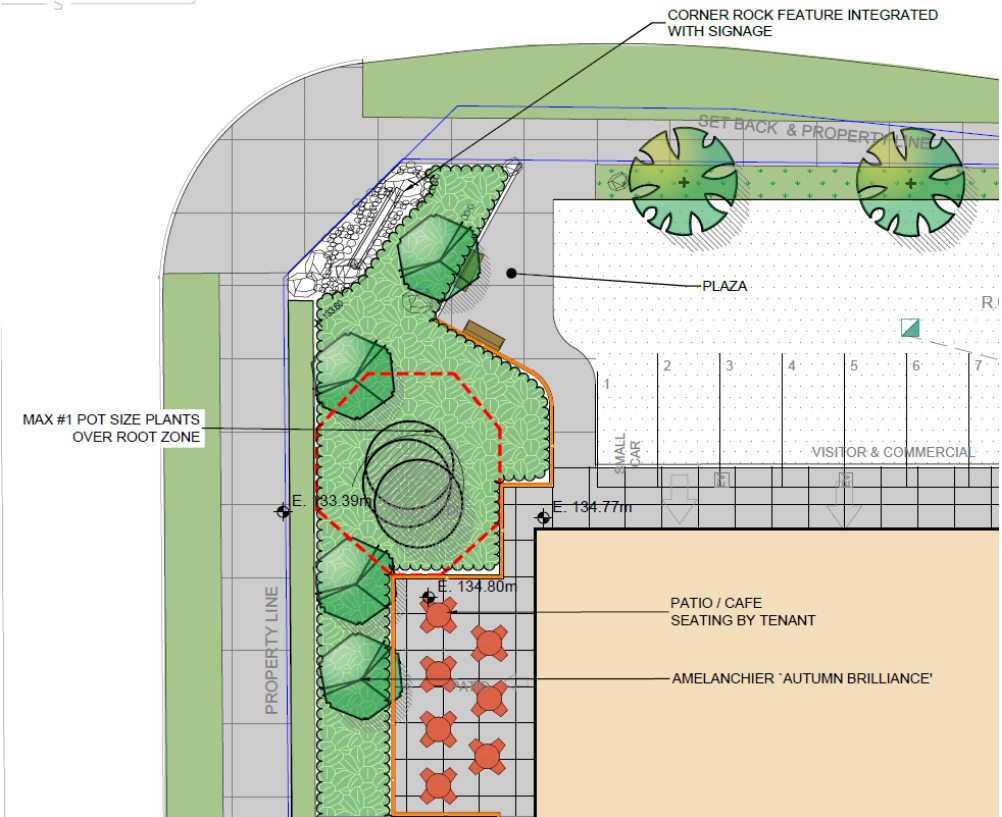
4 Zoning & OCP Review

General Form and Character of Development

<p>Land Use:</p> <p>The site is located at the border of the Town of Gibsons and Electoral Area E: Elphinstone of the Sunshine Coast Regional District (SCRD), on the traditional territories of the Sk̓wx̓wú7mesh Úxwumixw (Squamish) Nation. The approximate property area is 112,600 square feet or 2.58 acres (1.04 hectares). When this site was amalgamated into the Town of Gibsons, it retained the C-5 zoning designation per SCRD Bylaw 310 and a Mixed-Use Commercial Land Use Plan designation per the Town of Gibsons Official Community Plan. Furthermore, the site is part of the Upper Gibsons Commercial Development Permit Area No. 3.</p> <p>Our proposal, which includes a zoning change to bring the lot into conformance with current Town of Gibsons zoning, and a proposed development of two mixed use commercial and multifamily residential buildings is in keeping with the intent of the goals and policies of the Official Community Plan and DPA-3 Area. The Upper Gibsons Commercial Area is a focal point of commercial activity and is envisioned to support a strong mix of residential development that will support local retail. Specifically, some of the major policies for the area that are relevant and aligned with our proposal are as follows:</p>	
<p>OCP:</p> <p>10.1.3 Encourage the development of buildings and developments that mix commercial uses on the ground floor facing the street with residential or office uses above or behind.</p> <p>10.2.1 Create an Upper Gibsons “town centre” focused on the area between School/North Roads and Payne/ Pratt Roads with the following guidelines:</p> <ul style="list-style-type: none">• Emphasis on commercial uses, including retail stores, banks, restaurants, office uses, visitor accommodation, entertainment facilities and personal services.• A greater pedestrian orientation than currently exists, with improvements to sidewalks and landscaping emphasized with future developments.• Incorporation of multi-unit housing adjacent to the commercial areas as well as in mixed use buildings with commercial uses on the lower floors and residential above.	

DP A-3

- Significant landscaping adjacent to public roadways and integrated within the site, use of both architectural and landscape features to provide a “gateway” or distinct entrance.
- On sloped sites, building forms should step down gradually to follow the slope of the site.
- Where commercial development incorporates a residential or multi-unit use on upper floors, a graded transition in the building height is desired to ensure adjacent properties are not faced with a massive wall.
- Buildings should be sited with the entrance to the buildings facing the street (Gibsons Way) to encourage creation of an interest and access for pedestrians. Buildings on corner sites should have façade interest facing both streets.
- The buildings or structures should be used to reinforce the definition of street corners.
- Natural landscape which includes significant tree stands should be retained and incorporated into site development plans when feasible.
- Providing pedestrian amenities within the 3.0 metre transition zone between the sidewalk and the building or parking edge such as benches, shelters, alcoves, seating, walls, trellises and arbors and pathways to the adjacent businesses.
- Parking should not visually dominate a development. Parking areas should be integrated into developments by such means as incorporating significant landscaping, coordination of outdoor elements, and linking of buildings with parking by distinctively paved walkways.
- Parking lots should be paved and shall include landscaped areas within the lot. No more than 10 parking stalls in a row should be allowed without a landscaping break.
- Low dense screening of street fronting stalls is required.

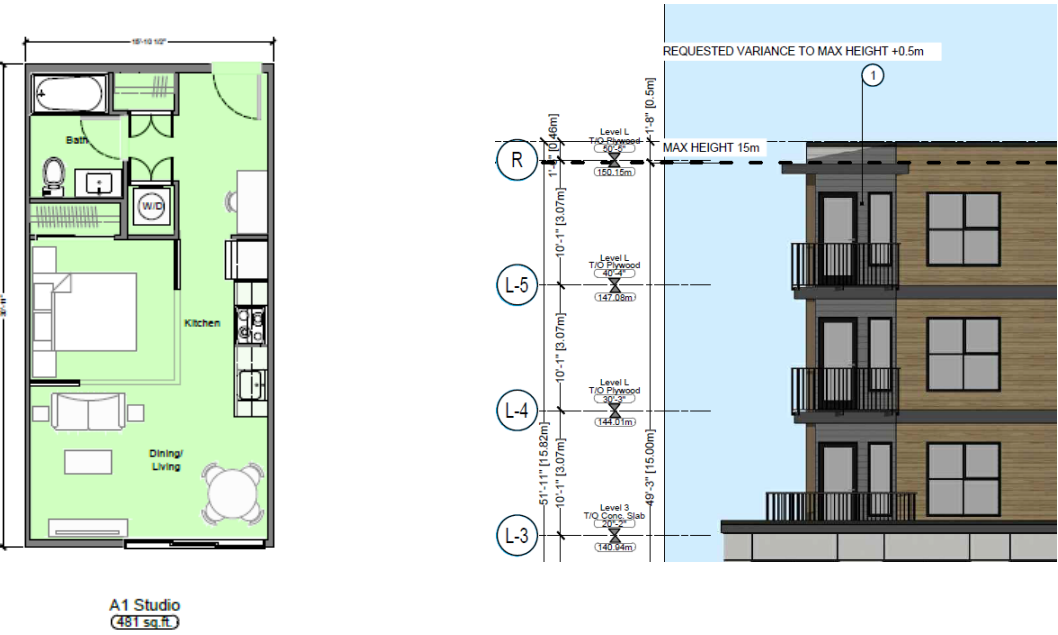


Portion of
Landscape Plan
at Corner of
Gibsons Way &
Pratt Road

Zoning

While the proposed development is generally comparable and consistent with the existing C1 - Upper Gibsons Commercial District 1 and C1A - General Commercial zoning designations, due to the unique characteristics of our proposal as we will outline below, we are requesting a Zoning Amendment for the site to a **Comprehensive Development (CD) Zone** to accommodate the proposed design elements, uses, and unique characteristics of the project. Further details are described in the Development section below, however, the main factors for this request include the following:

- Height: The proposed height is 15.5m vs the 15m allowed in C1A
- Uses: C1/C1A do not support Live Work uses.
- Apartment units: We are proposing to include bachelor suites to accommodate the growing renter demographic. Unit sizes start at 45m² (481 sq. ft.) which is below the 55.0 m² (592.0 sq. ft.) minimum outlined in the C-1 and C-1A zones.
- Parking: As demonstrated in the Transportation Report enclosed with our application and conducted by Bunt & Associates Transportation Engineers, we are seeking parking requirements within the proposed CD zone that are in keeping with the Institute of Transportation Engineers (ITE) parking utilization rates and similar past projects in Gibsons and other parts of BC. The proposed parking ratio of 1.1 stalls/unit is actually higher than the observed parking demand and vehicle ownership ratios from 3 comparable residential developments in Gibsons (see TIA by Bunt & Associates). Commercial parking is in keeping with the Bylaw Rate of 1 space per 45 m², and consistent with the Mixed-Use Gateway Zone 1 and Live-Work Zone 1 when it comes to commercial spaces that are intended to be shared with residential visitors.



Example of smallest suite size.

Height variance for Building A (Height calculated per average natural grade at front Property Line per C-1A method)
(Building B meets the 15.0m calculation)

5.0 The Need for Housing

At the July 18, 2023, Committee of the Whole meeting, Town of Gibsons Council voted unanimously to commit to a new housing growth target of 90 new housing units per year (current average of 58 units/year over the past 10 years). Several relevant considerations are mentioned in the associated Staff Report:

- The purpose of the associated Staff report was “to obtain Council endorsement on an application to CMHC for the Housing Accelerator Fund, for approximately \$3 million in funding to increase the housing supply at an accelerated pace. There are 5 proposed actions as part of the Town’s Housing Accelerator Fund application:
 - Develop a Gentle Density Program
 - Review Parking Program
 - Review and Expand Density Bonusing
 - Develop a Comprehensive Land Strategy
 - OCP Update and Implementation
- Staff have been working with Urban Matters and Urban Systems to prepare a Housing Action Plan and housing supply growth target projections for the application to CMHC.

On January 19, 2021, Council endorsed the recommendations of the Housing Needs Report Implementation Framework (December 2020). This was based on the Sunshine Coast Housing Needs Assessment Report (November 2020) prepared by Urban Matters for the Town of Gibsons, District of Sechelt, and Sunshine Coast Regional District. Key determinations of the reports including the following:

- The median age in Gibsons was 54.7 in 2016 and is projected to increase to 59.6 by 2025. Between 2016 and 2025, the average household size is projected to decrease from 2.09 to 1.97. As the population continues to age, there will be increased need for smaller units in accessible housing forms and supports to meet the needs of seniors living alone, or older couples whose children have left home who are looking for diverse housing options.
- Gibsons is growing faster than the Coast as a whole. Between 2006 and 2016, the population of Gibsons grew by 10% compared to 8% population growth across the Coast as a whole. Projections suggest this rate of growth will be sustained in the coming years.
- The population in Gibsons grew from 4,605 in 2016 to 4,758 in 2021 or by 3.3%.

The table below is an excerpt from the November 2020 Housing Needs Assessment Report, identifying the need for 178 units between 2016-2020 and 197 units between 2020-2025, in a variety of bedroom types (from studio to 3+ bedrooms). The figures below are prior to the new housing targets endorsed by Council in 2023, as part of its application to the Housing Accelerator Fund.

Table 39: Anticipated Units for Gibsons, 2016-2025

Household Type	2016-2020					2020-2025				
	Number of Bedrooms					Number of Bedrooms				
	0	1	2	3+	Total	0	1	2	3+	Total
Couples without Children	30	15	15	0	59	25	12	12	0	49
Families with Children and Other Families	0	0	2	4	6	0	0	1	3	4
Non-Census Families	34	34	34	11	113	43	43	43	14	144
Total	63	49	51	15	178	68	55	57	17	197
% by bedrooms	36%	27%	28%	9%	100%	34%	28%	29%	9%	100%

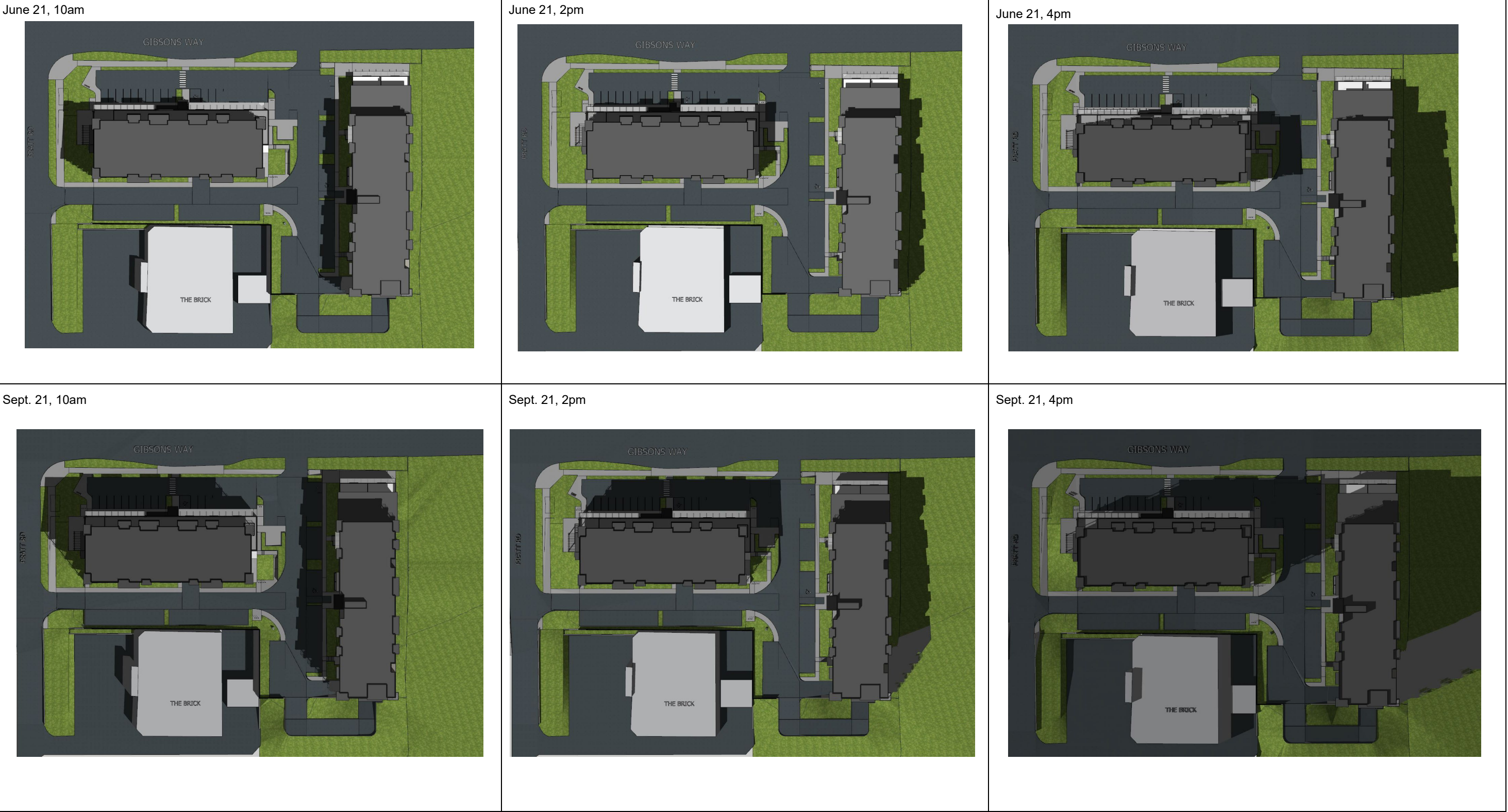
Source: Derived from Statistics Canada Census Program and BC Stats Custom Projections

To conclude, this project will add to the much-needed diversity of housing stock in the Town of Gibsons and will provide additional housing choice to residents at varying stages of their lives. Due to the current volatility of interest rate and inflationary pressures, it is not possible to commit to a rental structure at this time, however we will be monitoring this situation closely throughout the application and design process.

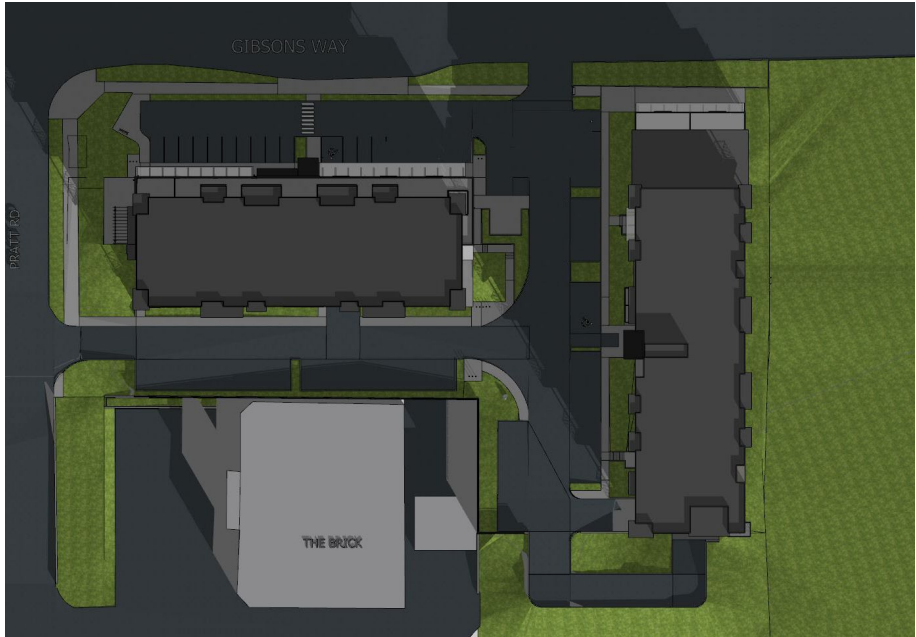
Image of typical floor (Building B)



Shadow Studies
Proposed development.



Dec. 21, 10am



Dec. 21, 2pm



Dec. 21, 4pm



5 Attachments

- Architectural Drawings
- Civil Drawings
- Landscape Drawings
- Transportation Impact Assessment
- Tree Evaluation Report
- Site Disclosure Statement
- Archaeological Review Letter