



ACTIVE TRANSPORTATION NETWORK PLAN

ROUND 2 ENGAGEMENT SUMMARY



Town of Gibsons

474 SouthFletcher Road, Box 340
Gibsons, BC V0N 1V0

January 2024

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Appendix A: Stakeholder Strategies and Actions Activity

INTRODUCTION

In May 2023, the Town of Gibsons launched the planning process for developing an Active Transportation Network Plan (ATNP). The ATNP will identify and fill gaps in the active transportation network and guide how the Town prioritizes and spends money on active transportation improvements.

During the first round of engagement in Summer 2023, we heard from community members about the challenges for walking, biking, and rolling in Gibsons and where improvements are most needed. Input collected was used to develop draft elements of the ATNP, including themes, strategies and actions, active transportation network, and priority projects.

The second round of engagement was launched in December 2023 and was designed to gauge community support for the preliminary recommendations and to understand which projects community members would like to see prioritized.

This document provides a summary of the second round of engagement, including activities and what we learned from community members.

ENGAGEMENT OPPORTUNITIES

RAISING AWARENESS

Town of Gibsons ATNP StoryMap

Round 2 engagement launched in November 2023 with an update to the Gibsons Active Transportation Network Plan project page, hosted on ArcGIS StoryMaps. The project updated included:

- Where we are in the planning process;
- Information about what has been done since the first round of engagement;
- An online survey to gather community feedback; and
- Upcoming opportunities for engagement.

Outreach and Promotion

The Town promoted the community survey and virtual open house on social media channels in December, including:

- 6 posts on Facebook
- 7 posts on Instagram
- 4 posts on X (formerly Twitter)



- Newsletter
- Posters on community boards
- Homepage of the website
- 2 newspaper ads

Community Pop-ups

Town of Gibsons staff hosted two pop-up booths to promote the Active Transportation Network Plan and encourage community members to participate in the second round of engagement. Pop-up events were held at:

- Sunnycrest Mall on December 5, 2023
- Gibsons & Area Community Centre on December 6, 2023

In total, Town staff engaged with more than 30 people.

ENGAGEMENT ACTIVITIES

The second round of engagement was held between December 4 and 17 and stakeholder meetings were held in January 2024. Engagement focused on gauging the community's support for the recommended actions and priority projects.

Round 2 engagement opportunities included:

- **Community Survey #2**
 - Open between December 4 and December 17, 2023.
 - 276 responses
- **2 Virtual Open Houses**
 - December 12, 2023
 - 20 attendees
- **Meeting with Community Stakeholders**
 - January 15, 2024
 - 10 attendees
- **Gibsons Elementary School Classroom Presentation – Grade 6/7 class**
 - January 15, 2024
 - Approximately 20 attendees
- **Elphinstone Secondary School Activity Sheets**
 - Activity sheets about the active transportation network plan were distributed to Elphinstone Secondary Students on January 23, 2024



WHAT WE LEARNED

The following is a summary of what we learned from community members through each engagement activity.

COMMUNITY SURVEY

An online survey was available on the project website between December 4 and 17, 2023. In total, 276 people completed the survey.

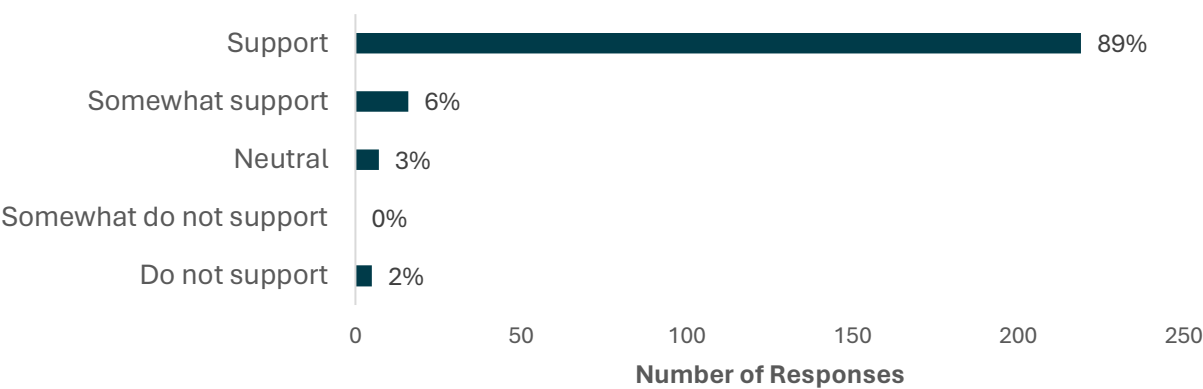
What Matters Most

Based on what we heard from community members and stakeholders in the first round of engagement, four key themes emerged:

- **Connected** – Active transportation is direct and consistent. This theme focuses on expanding and enhancing the active transportation network in Gibsons.
- **Safe** – Active transportation is safe. This theme focuses on addressing transportation safety in the community and making it safe to use active transportation year-round.
- **Convenient** – Active transportation is convenient. This theme focuses on ensuring active transportation is a convenient, easy, and fun way to travel by making it accessible and enjoyable.
- **Equitable** – Active transportation is for everyone. This theme focuses on ensuring active transportation is inclusive through planning, design, and outreach.

Participants were asked to share their level of support for the themes:

Figure 1 On a scale of 1 to 5 (1 being do not support and 5 being support), please tell us your level of support for the overarching themes of the plan. (247 responses)



Most survey participants (95%) indicated that they support or somewhat support the guiding principles, and 2% of survey participants said that they do not support the guiding principles (Figure 1).

Within these four themes, the draft plan includes 11 strategies and 34 actions. Survey participants were presented with the highest priority actions under each theme and asked to share their level of support.

Connected

Active transportation is direct and consistent.

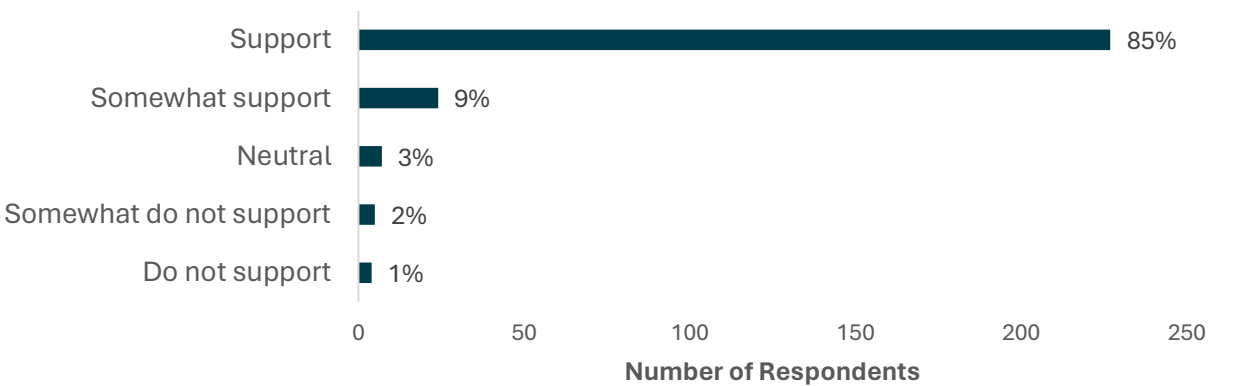
Under the *Connected* theme, there are three strategies and nine actions. Of these, three actions have been identified as the highest priority by Town staff. Participants were asked to identify their level of support for the priority actions under each theme.

Fill in the gaps in the pedestrian network. This includes implementing proposed sidewalk projects and updating the process for prioritizing sidewalk upgrades to fill in gaps in the sidewalk network. For roads where sidewalks are not feasible or appropriate, implementing walkable shoulders or pathways will be explored.

Continue to provide more active transportation pathways and connections between blocks and developments. This includes preserving and enhancing existing connections and seeking opportunities to create new connections through redevelopment to increase neighbourhood permeability and help shorten walking distances.

Fill in the gaps in the cycling network to ensure connections to key destinations. This includes implementing new cycling network projects to fill gaps in the cycling network. Filling gaps in the cycling network will provide safe, consistent, and connected cycling facilities to key destinations

Figure 2 On a scale of 1 to 5 (1 being do not support and 5 being support), please tell us your level of support for the Connected priority actions. (267 responses)



Most survey participants (94%) indicated that they support or somewhat support the priority actions identified under the *Connected* theme, and 3% said that they do not support or somewhat do not support the priority actions (Figure 2).

Safe

Active transportation is safe.

There are two strategies and eight actions included in the *Safe* theme. Of these, four actions have been identified as the highest priority by Town staff.

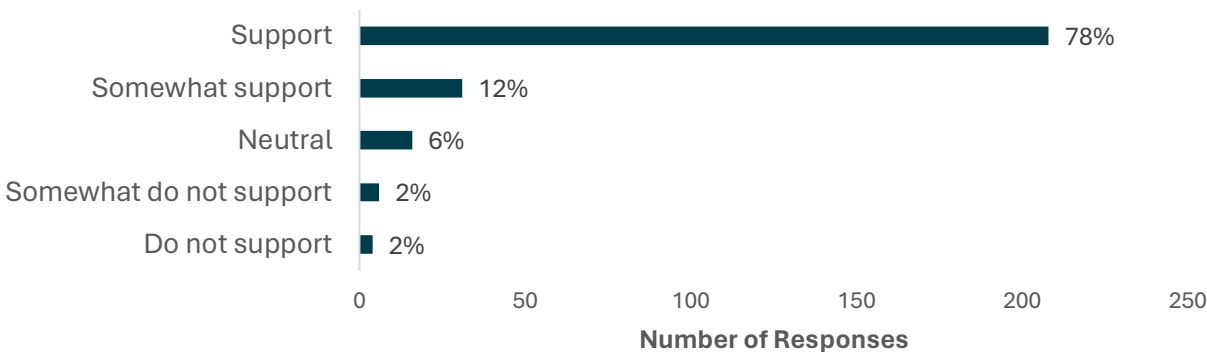
Address safety and accessibility concerns at crossing locations. This includes working with the province to make safety and network connectivity improvements at intersections, through improvements such as providing accessible push-buttons, curb ramps with tactile features, well-marked crosswalks, appropriate crossing times, and other treatments that shorten pedestrian crossing distance.

Review, identify, and implement strategies to reduce speeding throughout the community. This includes identifying infrastructure features such as speed humps, curb extensions, traffic circles, and traffic diversion to help reduce motor vehicle speeds and volumes on active transportation routes in the community.

Monitor and mitigate user conflicts on multi-use pathways. This includes identifying, monitoring, and reviewing locations of concern and providing treatments to address speed differential on multi-use pathways.

Ensure active transportation infrastructure is inventoried and maintained. This includes ensuring that active transportation facilities are inventoried as a part of the Town's Asset Management Program. This also involves developing an approach to prioritize and implement maintenance and repairs as well as ensuring there are adequate resources to maintain the active transportation network.

Figure 3 On a scale of 1 to 5 (1 being do not support and 5 being support), please tell us your level of support for the Safe priority actions. (265 responses)



Most survey participants (90%) said that they support or somewhat support the priority actions identified under the *Safe* theme, and 4% said that they do not support or somewhat do not support the priority actions (Figure 3).

Convenient

Active transportation is convenient.

There are three strategies and eight actions included in the *Convenient* theme. Of these, four actions have been identified as the highest priority by Town staff.

Provide more active transportation route wayfinding and signage throughout the Town.

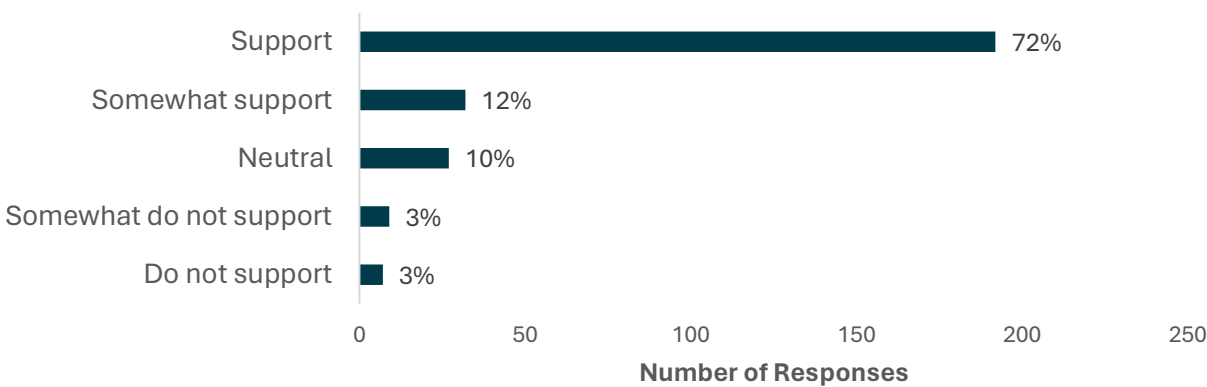
This includes developing a Wayfinding Strategy, conducting a wayfinding and signage review, and finding opportunities to add signage on new and existing facilities.

Develop a central hub for active transportation with a network map and information kiosk in community centres. This includes implementing a kiosk that could provide amenities such as covered bicycle parking, a bicycle repair station, an e-bike charging station, maps, and information about the cycling network.

Develop a program to install more bicycle parking within the public right-of-way and at facilities operated by the Town. This includes conducting an inventory of existing bicycle parking facilities and e-bike charging stations within Gibsons and developing a program to install more bicycle parking throughout the community.

Improve transit connections and experience. This includes ensuring there are direct active transportation connections provided to transit stops along transit routes, working with BC Transit to improve transit stops, and continuing to support programs and initiatives that encourage transit.

Figure 4 On a scale of 1 to 5 (1 being do not support and 5 being support), please tell us your level of support for the Convenient priority actions. (267 responses)



Most survey participants (84%) indicated that they support or somewhat support the priority actions identified under the *Convenient* theme, and 6% said that they do not support or somewhat do not support the priority actions (Figure 4).

Equitable

Active transportation is for everyone.

There are three strategies and ten actions included in the *Equitable* theme. Of these, four actions have been identified as the highest priority by Town staff.

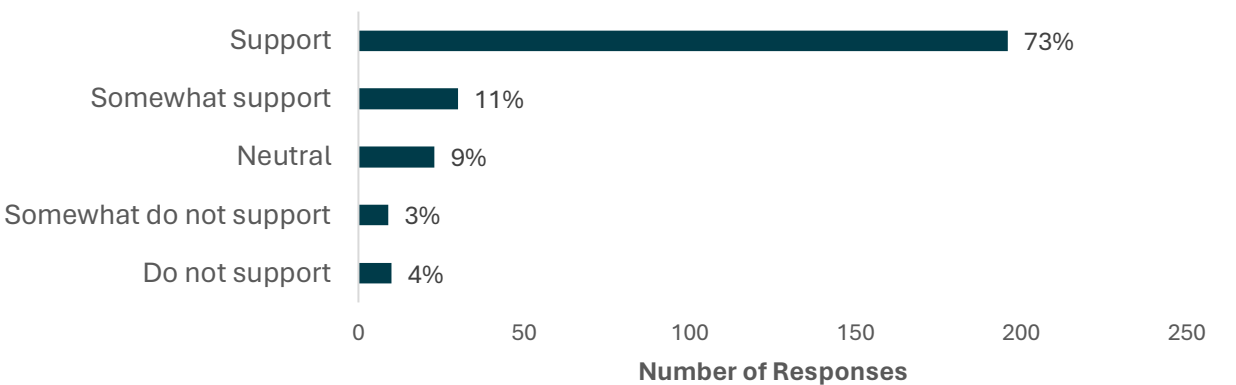
Leverage existing relationships and build new relationships with community-based organizations to distribute information and encourage engagement and feedback on active transportation projects. This includes partnering and collaborating with community-based organizations such as the Gibsons Senior Society to seek targeted engagement and feedback on active transportation projects.

Support the installation of public amenities such as seating, garbage, washrooms, and recycling bins. This includes developing a strategy for the installation of public amenities.

Incorporate street trees, landscaping, stormwater treatments and other “green street” infrastructure as part of active transportation projects where feasible. This includes seeking opportunities to incorporate green infrastructure, retain existing trees and plant new trees during the planning and design of active transportation projects where possible.

Support education and awareness programs that are tailored to different community groups. This includes working with key partners to pilot and continue programs such as the Walking School Bus to encourage active travel to school, working with partners to support bicycle skills training for elementary students and adults, and working with community organizations to empower seniors with tools and services to meet transportation needs through active modes.

Figure 5 On a scale of 1 to 5 (1 being do not support and 5 being support), please tell us your level of support of the Equitable priority actions. (268 responses)



The majority of survey participants (84%) said that they support or somewhat support the priority actions identified under the *Equitable* theme, and 7% said that they do not support or somewhat do not support the priority actions (Figure 5).

In addition to the questions above, survey participants were able to provide additional feedback about key themes and priority actions. In total, there were 133 responses which have been grouped into themes based on similar sentiments or ideas. Themes with two or more responses are summarized below:

Walking and Cycling Routes

- Include actions related to maintaining the network year-round. (11 responses)
- Need to prioritize separating cyclists and pedestrians. Shared facilities do not feel safe. (5 responses)
- Prioritize installing more bike lanes, specifically around the ferry terminal, in Lower Gibsons, and along Reed Road. (5 responses)
- Provide protected active transportation facilities to the ferry terminal. (2 responses)
- School Road is too steep to be a recommended pedestrian or cycling route. (2 responses)
- Need to clearly mark on-street cycling facilities and paved shoulders. (2 responses)
- Unsure if cycling facilities will be used enough for them to be prioritized. (2 responses)
- Need to implement a safe and connected cycling network. (2 responses)
- Need to make active transportation more attractive and convenient than driving. (2 responses)

Transit

- The Town needs to expand existing transit service and routes. Specifically, would like a transit route that conveniently connects Lower Gibsons and Upper Gibsons. (5 responses)
- The Town should invest in smaller buses. Could better service smaller neighbourhoods and the large buses are often empty. (3 responses)
- Need to improve transit between coastal communities. (2 responses)
- Install amenities at bus stops, including shelters and seating. (2 responses)

Safety

- Prioritize actions related to improving safety. Specific examples include installing lighting along pedestrian and cycling routes, safe pedestrian crossings, and prioritizing improvements along the most dangerous sections of roads. (8 responses)
- Need safe pedestrian crossings on Gower Point Road and to cross the highway. (3 responses)



Connections

- Include actions related to working with neighbouring communities to create a continuous active transportation network. (6 responses)
- Create walking and cycling connections between neighbourhoods and to schools. (2 responses)

Traffic Calming

- Include actions related to increasing traffic calming in neighbourhoods, specifically Gower Point Road and Marine Drive. (5 responses)
- Do not support implementing more traffic calming. (2 responses)

Environment and Sustainability

- Include actions related to implementing green streets, including street beautification and landscaping with plants that support pollinators. (4 responses)
- Include actions related to prioritizing ecological sustainability, avoiding or enhancing wildlife and aquatic habitats. (2 responses)

Implementing the Plan

- Need to ensure the Plan can be implemented. (4 responses)
- Include actions related to promoting the benefits of and encouraging active transportation. (2 responses)

Roads

- Fix potholes throughout Gibsons. (3 responses)
- Increase road signage for speed limits and sharing the road, especially around schools and high-risk intersections. (3 responses)

Accessibility

- Include actions that prioritize, and explicitly state, improvements will be accessible for everyone, including those using assistive mobility devices. (5 responses)

Trails and Multi-use pathways

- Include actions related to extending existing trails, such as the Reed Road Walking Path. (2 responses)

Continued Engagement

- Engage directly with seniors, people with diverse abilities, and low-income families. (2 responses)

Approach

- Participants questioned the survey approach, with some feeling the information presented was too general. (11 responses)
- General supportive comments (i.e., looks good, great work, fully support). (10 responses)



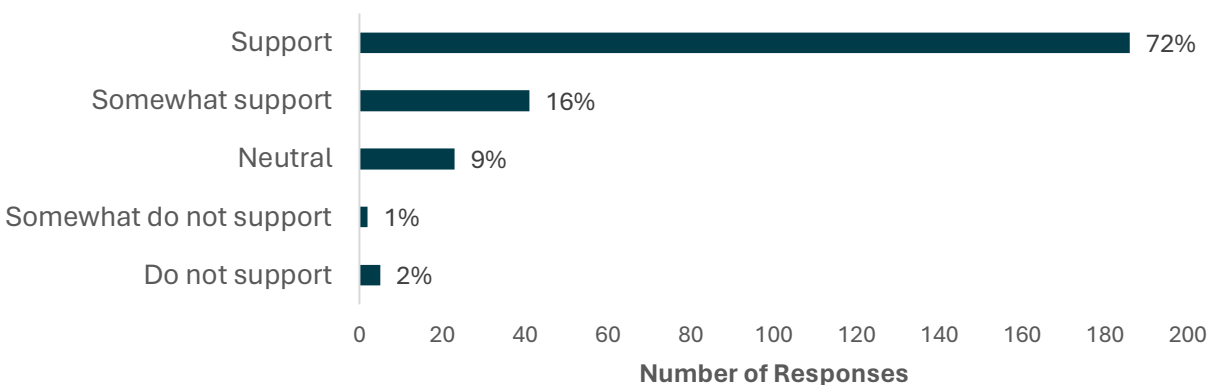
How We Will Make Decisions

A series of criteria was identified to guide the development of the network plan to fill gaps in the pedestrian and cycling networks. The criteria will further be applied to prioritize the implementation of the improvements recommended in the Active Transportation Network Plan. The criteria include:

- Proximity to transit
- Proximity to schools
- Proximity to community facilities (parks, library, senior care facility, recreation facility, and beach access)
- Proximity to commercial and retail areas
- Safety improvements (level of comfort improvement and the presence of ICBC report collisions)
- Network connectivity (fills in pedestrian or cycling network gaps)

Survey participants were asked to share their level of support for the approach to prioritizing active transportation improvements:

Figure 6 On a scale of 1 – 5 (1 being do not support and 5 being support), please tell us your level of support for our approach to prioritizing the recommendations of the Active Transportation Network Plan. (257 responses)



Most survey participants (88%) indicated that they support or somewhat support the approach to prioritization, and 3% said that they do not support or somewhat do not support the prioritization criteria (Figure 6).

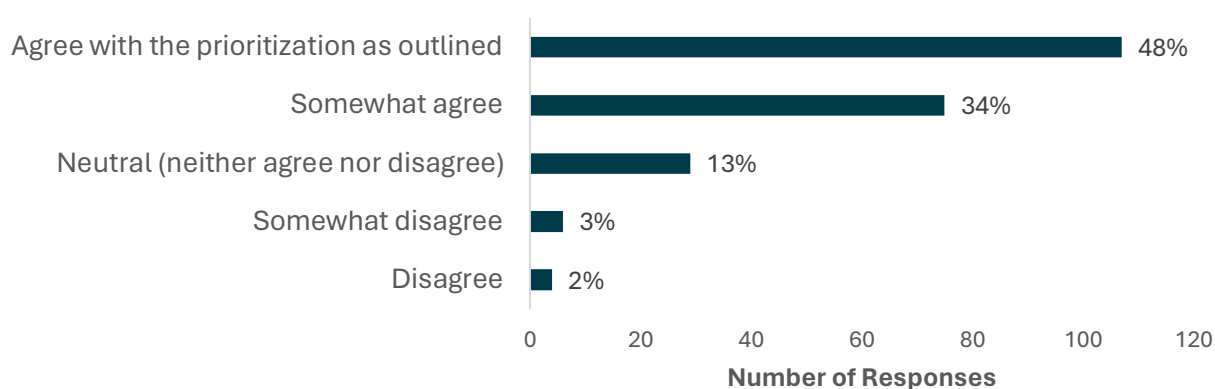


Prioritizing Infrastructure Projects

Pedestrian Network

The proposed pedestrian network aims to fill gaps in the network, provide more multi-use pathways, pedestrian walkways, and new connections implemented as part of future developments. Survey participants were asked to consider the pedestrian network prioritization (Figure 8) and share their level of agreement with the prioritization of the pedestrian network.

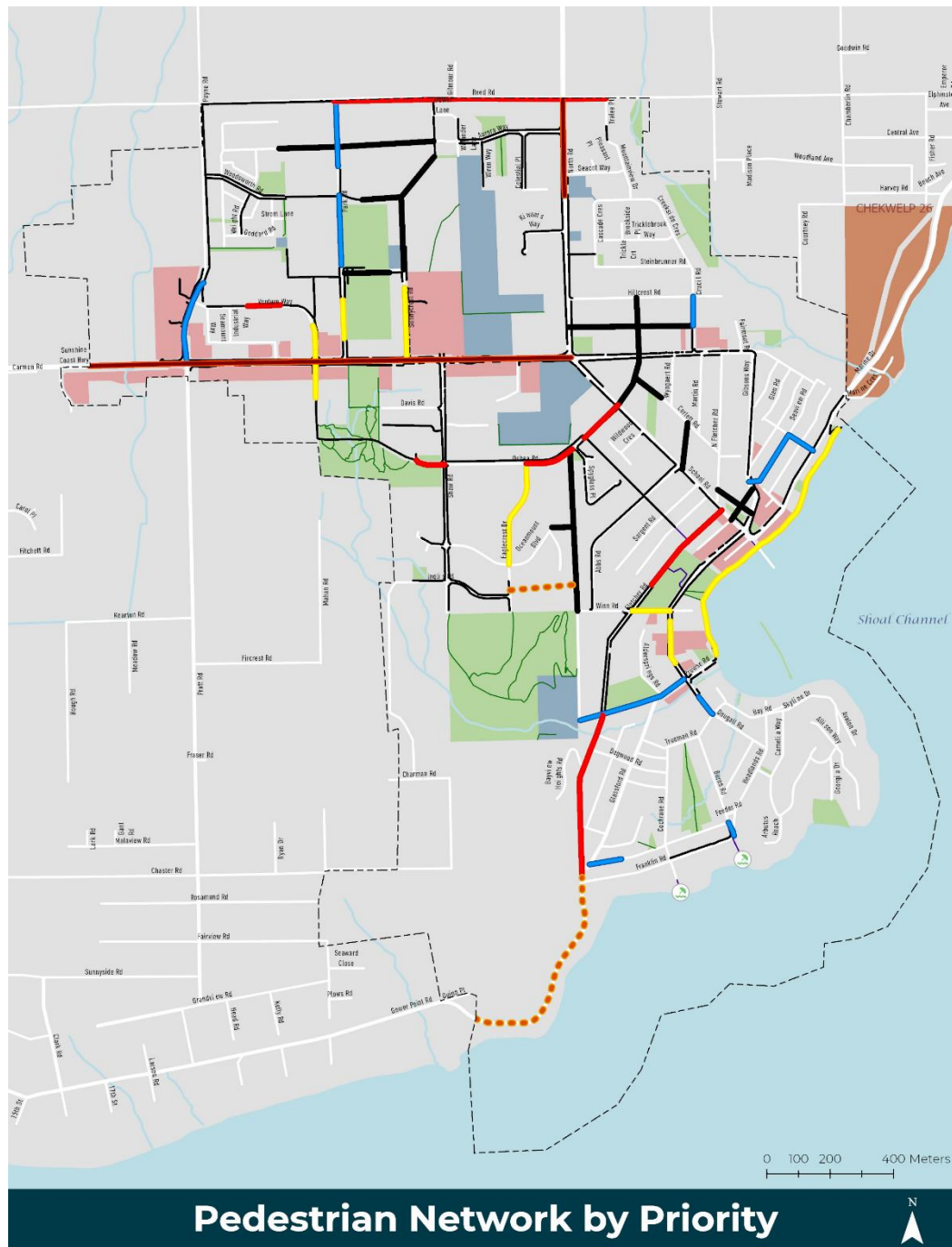
Figure 7 What is your level of agreement or disagreement with the prioritization of the pedestrian network? (221 responses)



The majority of survey participants (82%) indicated that they agree or somewhat agree with the prioritization as outlined, and 5% of participations disagree or somewhat disagree (Figure 7).



Figure 8 Pedestrian Network by Priority



- High Priority - MOTI Jurisdiction
- High Priority - Development Driven
- High Priority
- Medium Priority
- Low Priority
- Dependent on Development
- Existing Facility
- Stairway
- Trail
- Stream

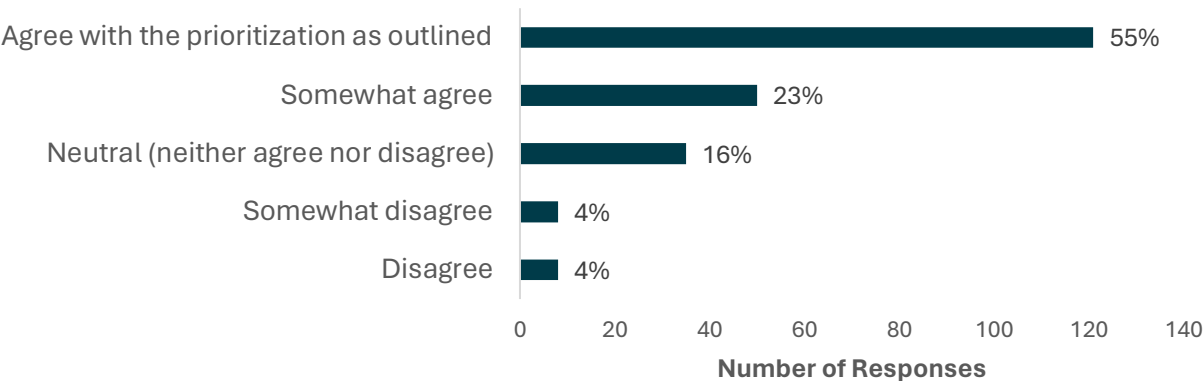
- Park
- Commercial Zone
- Institutional Zone
- Ocean
- First Nation Reserve
- Municipal Boundary
- Beach Access



Cycling Network

The proposed cycling network aims to provide a connected cycling network of high-quality cycling facilities. It recommends locations for proposed separated bicycle lanes, multi-use pathways, local street bikeways, and new connections implemented as part future developments. Survey participants were asked to consider the proposed cycling network (Figure 10) and share their level of agreement with the prioritization of the cycling network.

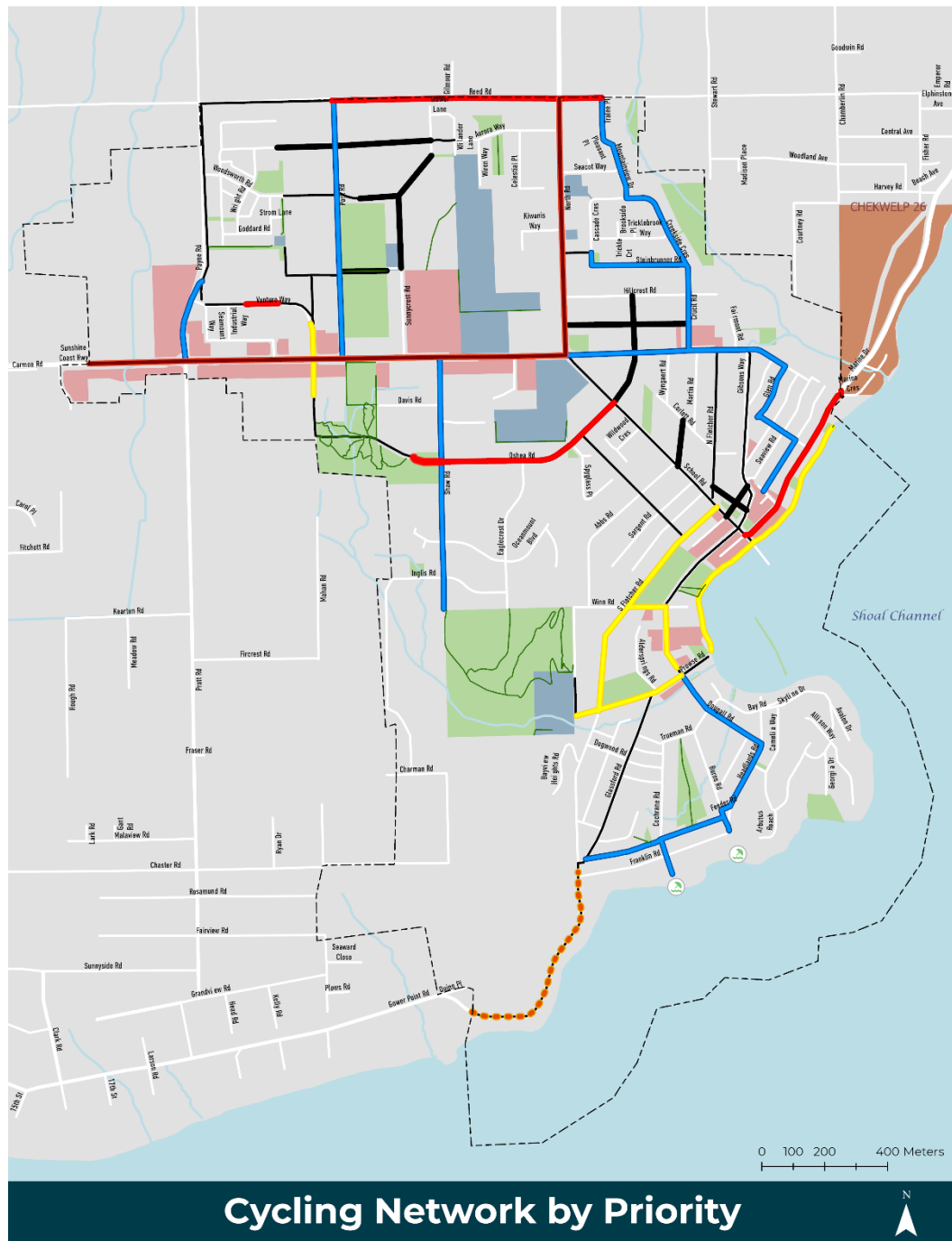
Figure 9 What is your level of agreement or disagreement with the prioritization of the cycling network? (222 responses)



Most survey participants (78%) indicated that they agree or somewhat agree with the prioritization as outlined, and 8% of participations disagree or somewhat disagree (Figure 9).



Figure 10 Cycling Network by Priority



Cycling Network by Priority



- High Priority - MOTI Jurisdiction
- High Priority - Development Driven
- High Priority
- Medium Priority
- Low Priority
- Dependent on Development / Alignment Unknown

- Existing Facility
- Trail

- Park
- Commercial Zone
- Institutional Zone
- Municipal Boundary
- First Nation Reserve
- Beach Access

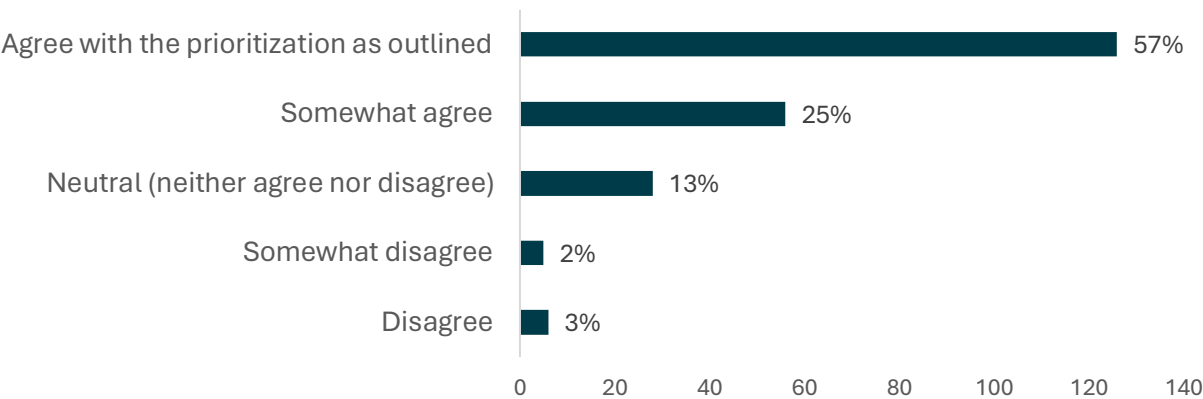


Intersection Improvements

Several locations were identified where intersection upgrades are recommended to improve conditions for people walking and cycling. These projects range from reviewing the existing intersection design to determining if a crosswalk should be implemented, and treatments to improve access to trails.

Survey participants were asked to review the priority intersection improvements (Figure 12) and share their level of agreement with the prioritization.

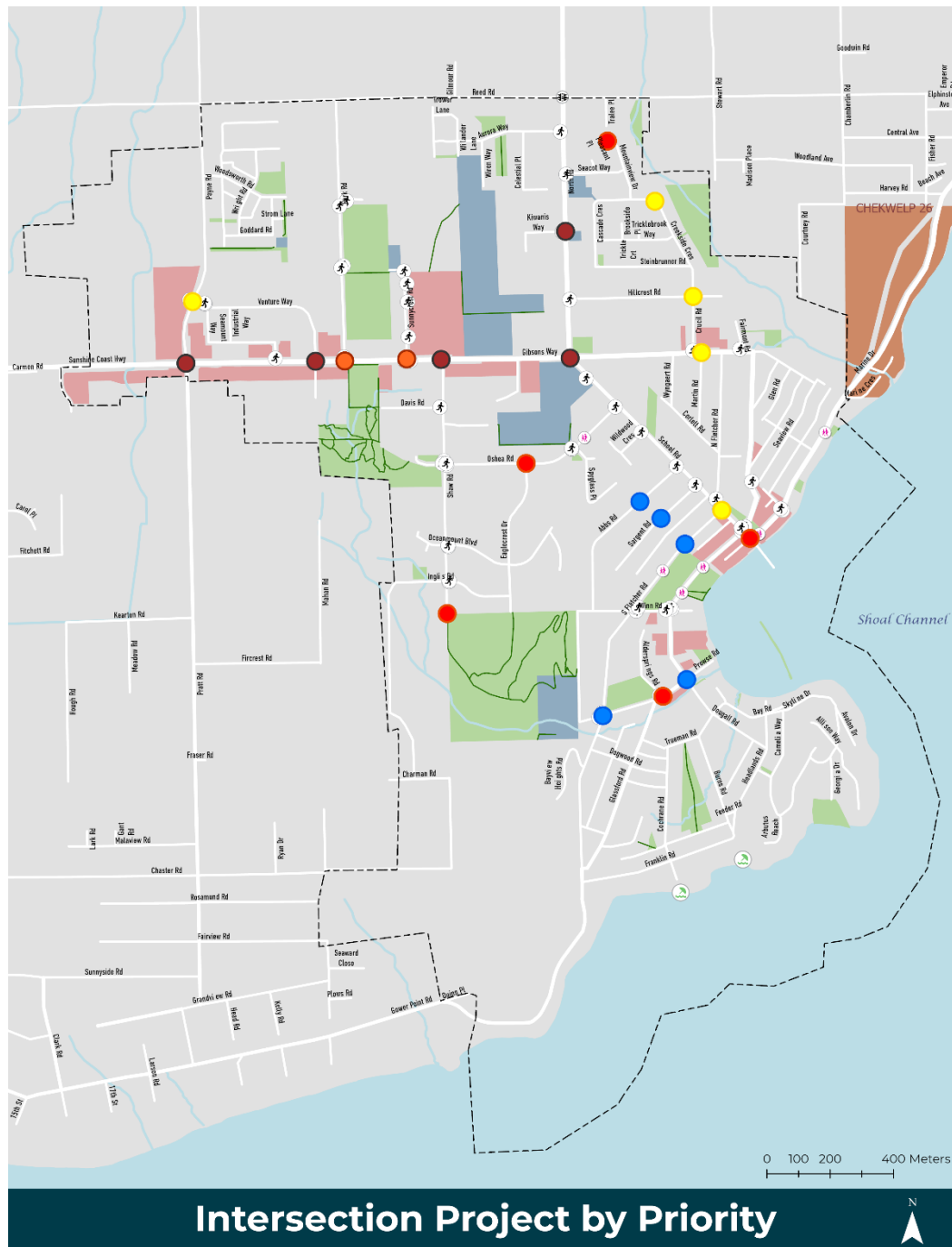
Figure 11 What is your level of agreement or disagreement with the prioritization of the intersection improvements? (221 responses)



Most survey participants (82%) said that they agree or somewhat agree with the prioritization as outlined, and 5% disagree or somewhat disagree (Figure 11).



Figure 12 Intersection Project by Priority



- MOTI Jurisdiction**
- High Priority
 - Medium Priority
- Town Jurisdiction**
- High Priority
 - Low Priority

- Medium Priority
- Crosswalk
- Crosswalk - Midblock
- Traffic Signal
- Beach Access

- Trail
- Commercial Zone
- Institutional Zone
- Park
- First Nation Reserve
- Municipal Boundary



Survey participants were able to provide additional feedback about the proposed pedestrian, cycling, and intersection projects. In total, there were 86 responses which have been grouped into themes based on similar sentiments or ideas. Themes with two or more responses are summarized below:

Walking and Cycling

- Active transportation facilities should be physically separated. (5 responses)
- Missing pedestrian improvements on Gower Point Road. (4 responses)
- Improved pedestrian and cycling facilities requested in the Bay Area / Georgia View neighbourhood. (3 responses)
- Missing the multi-use path on Glassford Road. (2 responses)
- Lower Gibsons cycling improvements should be higher priority. (2 responses)
- Sea Walk should not be a multi-use path. It is dangerous combining pedestrians and cyclists. (2 responses)
- Need pedestrian improvements on Harmony Lane to improve access to childcare and seniors' facilities. (2 responses)
- Need to widen roads to create safer bicycle lanes. (2 responses)

Intersections and Crossings

- 5 Corners (School Road / Marine Drive / Gower Point Road) is a dangerous intersection for all road users and needs to be prioritized. (5 responses)
- Need more pedestrian crossings throughout Gibsons, especially in the O'Shea/Oceanmount and Upper Gibsons Commercial Area neighbourhoods. (4 responses)
- Gower Point Road and South Fletcher Road (at Dougall Park) should be a high priority. (3 responses)
- Missing Improvements for Truman Road / Dougall Road / Bay Road intersection. (2 responses)
- Unsure / unclear what the intersection improvements will include. (2 responses)
- School Road at South Fletcher is a dangerous intersection for all road users. (2 responses)
- Shaw Road at Inglis Trail should not be a high priority. (2 responses)
- Intersection improvements needed on Reed Road at Park Road and Payne Road. (2 responses)
- School Road and Gibsons Way / North Road intersection is a top priority. (2 responses)

Speeding and Traffic Calming

- Support implementing more traffic calming throughout Gibsons. (2 responses)
- Should reduce speed limits in Gibsons. (2 responses)



Project Planning Approach

- Need to work with neighbouring communities to connect facilities. Community members use roads beyond the Town boundaries. (4 responses)
- Need to consult with community groups including the Accessibility Committee and Sunshine Coast Conservation Association. (2 responses)
- Concerned about the cost to implement all of the priorities. (2 responses)
- Maps are difficult to view. (5 responses)

Gower Point Road / Gospel Rock Connection

Providing safer conditions for walking and cycling on Gower Point Road to Gospel Rock was identified as an important project by community members during the first round of community engagement.

With a new development planned in the area, several options have been explored to provide an enhanced active transportation connection along or adjacent to Gower Point Road to Gospel Rock and the Town boundary. Each of these options have trade-offs that impact road users and the natural environment in the area.

Survey participants were asked to consider each option, including the relative cost, ease of implementation, environmental impact, and active transportation improvement, and let us know which option they would be most interested in the Town continuing to explore.

The options described on the following page are in the order they were ranked by participants. Options were ranked on a three-point scale, with 1 being the most preferred and 3 being the least preferred. The average rankings for each option are presented below, with the average closest to 1 showing the most preferred option.

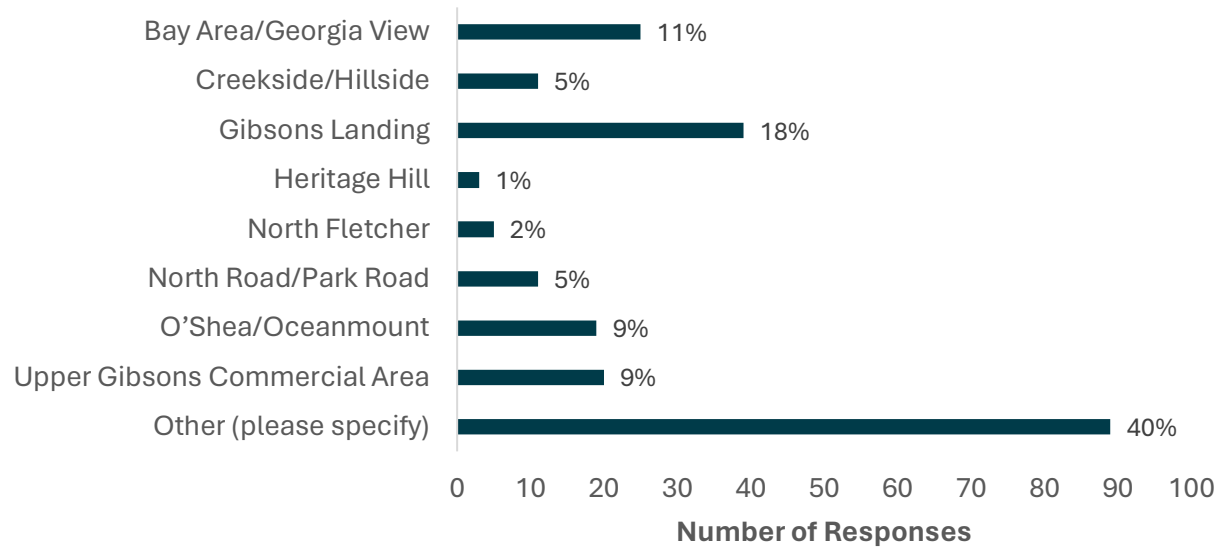


| Average Survey Ranking (Closer to 1= most preferred option) | Option | Description | Considerations |
|--|--|--|--|
| 1.5 | Option 2: Roadside Multi-use Pathway | Widen the road and provide a multi-use pathway on one side of Gower Point Road for people walking and people cycling in one direction. | Cost: Medium Ease of Implementation: Moderate Environmental impact: Moderate AT Improvement: High level of improvement for pedestrians and moderate for cyclists. |
| 1.8 | Option 3: Separated Trail | Provides a forest trail that runs parallel but separated from the Gower Point Road. | Cost: High Ease of Implementation: Low Environmental impact: High AT Improvement: Moderate to high level of separation from motor vehicles but less direct routing. |
| 2.2 | Option 1: Traffic Calming (208 responses) | Slow traffic and raise awareness of the presence of people walking and cycling. People walking and cycling share the road with motor vehicles. | Cost: Low Ease of implementation: High Environmental impact: Low AT Improvement: Minimal improvement |



Respondent Demographics

Figure 13 Where do you live? (222 responses)



Most participants who selected “other” live in communities in the SCRD, including Roberts Creek, Elphinstone, Grantham’s Landing, Hopkins Landing, and Langdale.

Figure 14 Which best describes your residence in Gibsons or the Sunshine Coast? (222 responses)

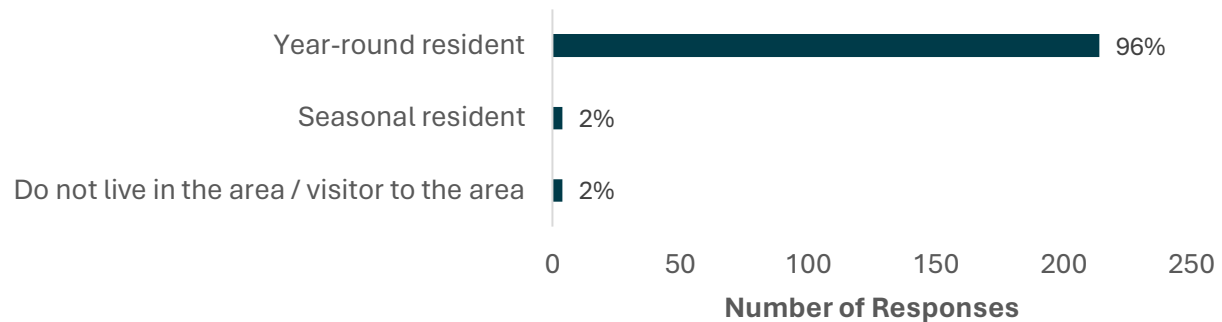


Figure 15 What is your age? (222 responses)

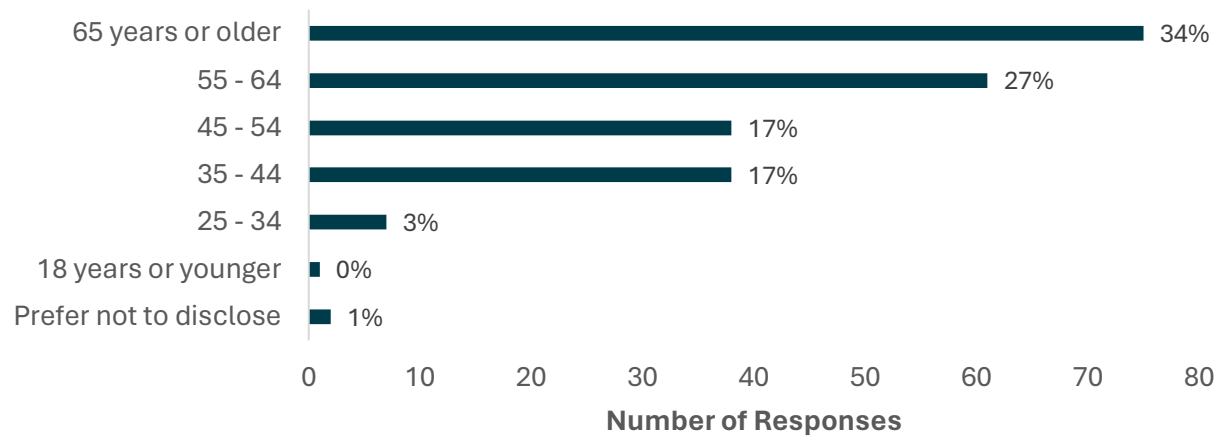


Figure 16 What is your gender? (221 responses)

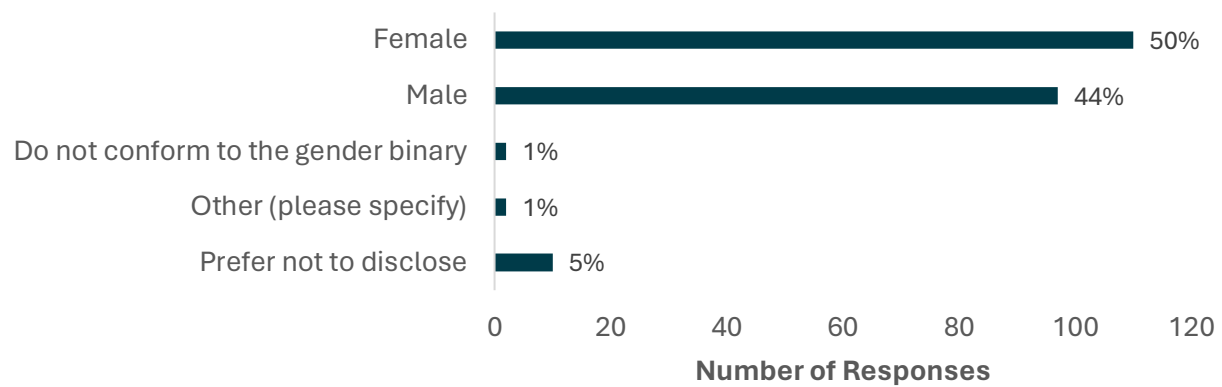


Figure 17 Do you have any limitations? Select all that apply. (209 responses)

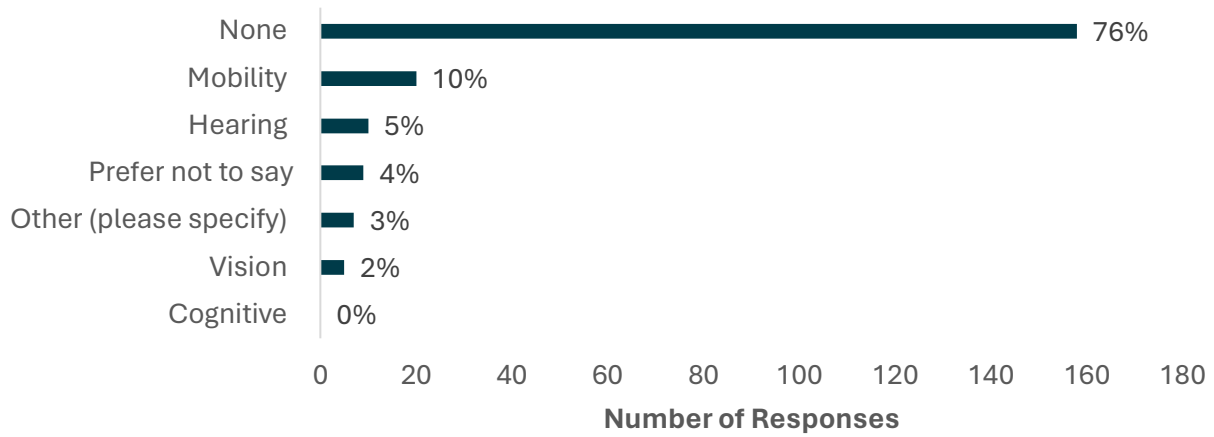
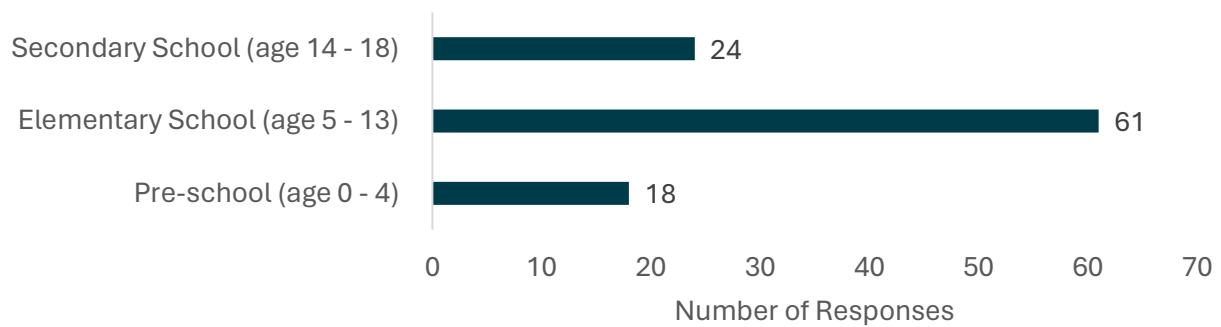


Figure 18 How many children does your household have? (117 responses)



VIRTUAL OPEN HOUSE

The Town hosted two virtual open houses to share the proposed strategies, actions, draft active transportation network, and priority projects with community members. Both sessions were held on December 12, 2023.

- Afternoon session: 2:00 - 3:30pm
- Evening session: 6:00pm – 7:30pm.

There were 20 community members that attended the virtual open houses and provided feedback on the draft elements of the ATNP. Key takeaways from the virtual sessions are summarized below:

- Would like accessibility to be a central part of the plan. The Town needs to ensure that people with different levels of mobility are being considered.
- Concerned about the Glassford Road multi-use path not being included on the proposed network maps.
- Community members were interested in exploring more options for the Gower Point Road / Gospel Rock connection and raised concerned about some of the proposed options that conflict with the Sunshine Coast Conservation Association's covenant to protect the area.
- Would like clarification on how the priorities were set and how different criteria were weighted. There was a desire for more transparency in how findings from the first round of engagement were used to inform the project prioritization.
- There were concerns about multi-use paths proposed on busy roads or areas with lots of driveways.
- Curious how the Town will be held accountable to follow the guiding principles when implementing projects in the future.

YOUTH ENGAGEMENT

A youth engagement session was held with Grade 6/7 students at Gibsons Elementary. The class was provided context on what active transportation is and asked what they liked about walking and biking in Gibsons. Students generally mentioned that they enjoyed using the trails in Gibsons for active transportation, walking around in Lower Gibsons in areas that had lots of people, and walking and biking near where they live.

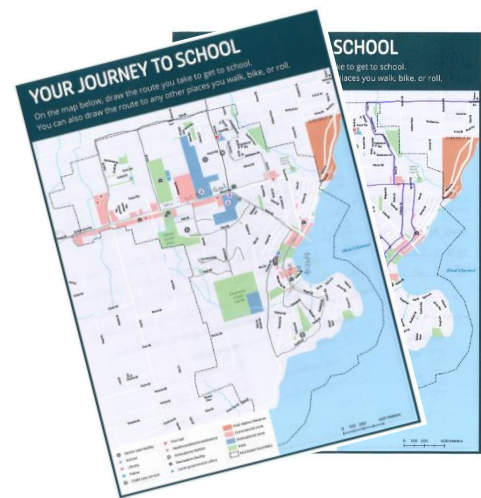
Students were also provided activity sheets to further understand their experience with active transportation and seek feedback on the plan. Below is a summary of what we heard from working with this group of students.



Journey to School

The following corridors were identified as a part of student’s routes to get to school and places they enjoy walking, biking, or rolling:

- Gibsons Way
- O’Shea Road
- North Road
- Shaw Road
- Glassford Road
- Gower Point Road
- South Fletcher Road
- Franklin Road
- Eaglecrest Drive
- Walkway between Sargeant Road and O’Shea Road



Most students said that the areas they walk, bike, and roll are included as part of the proposed network. The students were also asked if there are places in Gibsons where they feel unsafe walking, biking, or rolling. Popular responses included:

- **School Road**
 - Lots of traffic and steep hills at School Road and Marine Drive
 - Feel unsafe crossing without a crossing guard at School Road and Gibsons Way
 - Not enough lighting
- **Sunshine Coast Highway**
 - Lots of motor vehicle traffic
 - Not enough safe pedestrian crossings

The project team discussed different types of walking and biking infrastructure that could be implemented in Gibsons. Students were asked which routes they would feel most comfortable walking, biking, and rolling on. Their answers are shown below:

| Walking Facilities | | | | Biking Facilities | | |
|--------------------|-------------------|--------------------|-------|------------------------|-------------------|-----------------------|
| Sidewalks | Multi-use Pathway | Pedestrian Pathway | Trail | Separated Bicycle Lane | Multi-use Pathway | Neighbourhood Bikeway |
| 10 | 4 | 8 | 10 | 5 | 4 | 6 |

These activity sheets were also shared with Elphinstone Secondary School students on January 23, 2024. None were submitted back to the Town.



IN-PERSON STAKEHOLDER WORKSHOP

The project team hosted an in-person stakeholder meeting at the Town's Council Chambers to provide an update on the planning process, share initial community input, and discuss priorities of what should be implemented first. Ten stakeholders representing government agencies and community organizations attended the meeting.

A presentation about the preliminary strategies and actions, draft active transportation network, and the results of community input were first shared with the group. After the presentation, the group broke into rotating discussions to provide feedback on the draft strategies and actions, proposed active transportation network, and active transportation network priorities.

Strategies and Actions

Stakeholders also had the opportunity to review the proposed strategies and actions for the ATNP and share their level of support for each. Not all stakeholders participated in this activity, and response rates varied for each action. Every proposed action was unanimously fully or somewhat supported. Stakeholders were also able to highlight actions they felt should be a high priority. Additional feedback on the strategies and actions have been summarized below.

- All active transportation should be AAA.
- Reduce parking requirements in development.
- Provide a central hub for active transportation at Langdale Terminal.
- Provide bicycle parking in Lower Gibsons on Gower Point Road and bicycle maintenance locations at the library.

The full results from this activity can be viewed in **Appendix A**.

Draft Network and Prioritization

Stakeholders were encouraged to share their input on the draft active transportation network to see if there were any routes that should be added or removed and provide feedback about the proposed priorities. Maps for the proposed network and network prioritization were printed out and placed on different stations organized by pedestrian, cycling, and intersections around the room. This input was captured by marking up the printed maps and has been summarized below.

Pedestrian Network

- Formalize the pathway from North Road to Elphinstone Secondary School.
- Check if right-of-way is available from Cascade Crescent to North Road.
- Provide and off-highway pathway through new development adjacent to the Sunshine Coast Highway to better serve people living in the neighbourhood.



- Consider pathways along the routes students are using to travel to school, including Davis Road, Poplar Lane, and O'Shea Road.
- Maps are missing existing trails at Woodsworth Road to Park Road and trails on private property in Bayview Heights area.
- There is no indication that drivers are approaching a school on Trower Lanes.

Cycling Network

- Supportive of adding in Upper Gibsons cycling connections (Woodsworth Road, Aurora Way).
- There is a lack of signage and markings on Reed Road multi-use pathway to indicate shared facility between pedestrians and cyclists.
- There is a need for stronger consideration about how e-bike speed differentials will affect the network.
- The newer switch back trail on Inglis Trail is the route most cyclists from Lower Gibsons take to Upper Gibsons.
- Need to provide a cycling facility either on waterfront path or on Gower Point Road between Schol Road and Gower Point since going up School Road to get to South Fletcher is too difficult.

Intersection Projects

- Feedback on proposed intersections:
 - Gibsons Way, North Road, and School Road
 - This is a high conflict intersection for drivers and people using active transportation.
 - There is a lack of enforcement in this area.
 - Crossing guards work a very dangerous job in this area.
 - School Road and South Fletcher Road
 - Should be high priority.
 - Prioritize crosswalk RRFB at Gibsons Way and Shaw Road for students enroute to Gibsons Elementary School and Elphinstone Elementary School
- Suggestions for additional intersection improvements:
 - Gibsons Way and Farnham Road – new intersection.
 - Gibsons Way and Farnham Road – new intersection.
 - Students cross from the bus stop on Highway 101 to school without a crosswalk.
 - Provide a crossing on Gibsons Way between Crucil Road and North Road for the future connection – alignment unknown in the Hillcrest area.



Other

- Consider the impact and integration of new developments to the active transportation network.
 - High density development planned along Gibsons Way.
 - New affordable housing development on O'Shea Road and Shaw Road.
- Partner with developer to provide connections to and from the new development.
- Review new development connections and confirm there are crossings.
- Lack of facilities at and to Elphinstone Secondary School.
- Some concerns of people using School District land for connections as part of undesignated. Concerns about personal safety.
- Property acquisition is an option for the area around Elphinstone Secondary School.
- Ensure the plan has a clear vision for the future and highlight how the recommendations of the plan will help to achieve this.

NEXT STEPS

Thank you to all community members who participated and provided valued input during the second round of engagement!

The project team collected valuable feedback on the active transportation network and level of support for proposed improvements. Feedback from this round of engagement will be used to refine the recommendations and ensure the Plan reflects the community's interests and priorities. Based on the input received from community members, we will be reviewing and updating some of the projects identified, how they have been prioritized, and provide recommendations for implementation. The Plan will be finalized and presented to Council in the Spring of 2024.



APPENDIX A:
STAKEHOLDER STRATEGIES AND
ACTIONS ACTIVITY

| Action # | CONNECTED | Support | Somewhat Support | No Opinion | Somewhat Do Not Support | Do Not Support | Stakeholder Prioritization |
|---|--|---------|------------------|------------|-------------------------|----------------|----------------------------|
| | <i>Active transportation is direct and consistent</i> | | | | | | |
| Strategy 1: Expand and enhance pedestrian facilities to ensure complete and continuous access | | | | | | | |
| 1.1 | Action: Fill in the gaps in the pedestrian network | 5 | | | | | |
| 1.2 | Action: Continue to provide more active transportation pathways and connections between blocks and developments. | 4 | 1 | | | | 1 |
| Strategy 2: Ensure cycling facilities are consistent and connect to community destinations | | | | | | | |
| 2.1 | Action: Fill in the gaps in the cycling network to ensure connections to key destinations | 5 | | | | | |
| 2.2 | Action: Upgrade existing cycling facilities | 5 | | | | | 1 |
| Strategy 3: Ensure active transportation best practices are incorporated into all plans, policies, and projects | | | | | | | |
| 3.1 | Action: Ensure all future plans and policy documents support and encourage active transportation | 3 | | | | | 2 |
| 3.2 | Action: Ensure all projects and developments integrate with the active transportation network. | 5 | | | | | |
| 3.3 | Action: Incorporate best practices and active transportation design guidance to the Gibsons Subdivision and Development Servicing Bylaw | 3 | | | | | |
| 3.4 | Action: Design active transportation infrastructure to meet current best practice guidance | 4 | | | | | |
| 3.5 | Action: Work with the province (MoTI) to ensure roads in Gibsons under their jurisdiction are designed for the community context and in accordance with current best practice. | 7 | | | | | 2 |

| Action # | SAFE <i>Active transportation is safe</i> | Support | Somewhat Support | No Opinion | Somewhat Do Not Support | Do Not Support | Stakeholder Prioritization |
|--|---|---------|------------------|------------|-------------------------|----------------|----------------------------|
| Strategy 4: Improve transportation safety throughout the community | | | | | | | |
| 4.1 | Action: Address safety and accessibility concerns at crossing locations | 5 | | | | | 1 |
| 4.2 | Action: Continue to monitor pedestrian and cycling collision locations | 3 | | | | | |
| 4.3 | Action: Review, identify, and implement strategies to reduce speeding throughout the community | 6 | | | | | 2 |
| 4.4 | Action: Monitor and mitigate user conflicts on multi-use pathways | 2 | | | | | |
| Strategy 5: Maintain the active transportation network year-round | | | | | | | |
| 5.1 | Action: Ensure active transportation infrastructure is inventoried and maintained | 3 | | | | | 1 |
| 5.2 | Action: Design active transportation facilities to provide adequate drainage, snow storage and removal, and sand and gravel removal | 2 | | | | | |
| 5.3 | Action: Review and update snow and ice removal priorities to include multi-use pathways and on-street cycling facilities | 2 | | | | | |

| | CONVENIENT | Support | Somewhat Support | No Opinion | Somewhat Do Not Support | Do Not Support | Stakeholder Prioritization |
|---|---|---------|------------------|------------|-------------------------|----------------|----------------------------|
| Action # | <i>Active transportation is convenient</i> | | | | | | |
| Strategy 6: Make it easier to get around Gibsons | | | | | | | |
| 6.1 | Action: Provide more active transportation route wayfinding and signage throughout the Town | 3 | | | | | |
| 6.2 | Action: Sign and map alternative routes (with less steep grades) on maps and signage | 2 | | | | | |
| 6.3 | Action: Develop a central hub for active transportation with a network map and information kiosk in community centres | 5 | | | | | 1 |
| Strategy 7: Provide more bicycle parking and end-of-trip facilities | | | | | | | |
| 7.1 | Action: Update the Town's Zoning Bylaw requirements for bicycle parking and end-of-trip facilities | 3 | | | | | |
| 7.2 | Action: Develop a program to install more bicycle parking within the public right-of-way and at facilities operated by the Town | 3 | | | | | |
| 7.3 | Action: Work with School District 46 to enhance and provide more bicycle parking on school grounds | 3 | | | | | |
| Strategy 8: Improve integration with other modes | | | | | | | |
| 8.1 | Action: Monitor new mobility technologies and consider the impact on the active transportation network | 3 | | | | | |
| 8.2 | Action: Improve transit connections and experience | 2 | | | | | 1 |

| Action # | EQUITABLE | Support | Somewhat Support | No Opinion | Somewhat Do Not Support | Do Not Support | Stakeholder Prioritization |
|---|--|---------|------------------|------------|-------------------------|----------------|----------------------------|
| | <i>Active transportation is for everyone</i> | | | | | | |
| Strategy 9: Apply an intersectional, equity-focused lens to the planning, design, and implementation to support equity-seeking groups | | | | | | | |
| 9.1 | Action: Use outreach and engagement strategies that are inclusive and collaborative | 2 | | | | | |
| 9.2 | Action: Leverage existing relationships and build new relationships with community-based organizations to distribute information and encourage engagement and feedback on active transportation projects | 1 | | | | | |
| 9.3 | Action: Consider the development of Active Transportation Committee that includes diverse representation | 5 | | | | | 1 |
| Strategy 10: Create a more accessible, comfortable, enjoyable, and interesting active transportation network | | | | | | | |
| 10.1 | Action: Enhance lighting along active transportation facilities where appropriate | 2 | | | | | 1 |
| 10.2 | Action: Support the installation of public amenities such as seating, garbage, and recycling bins | 3 | | | | | |
| 10.3 | Action: Continue to support the Public Art Path and expand the installation of public art | 2 | | | | | |
| 10.4 | Action: Incorporate street trees, landscaping, stormwater treatments and other "green street" infrastructure as part of active transportation project | 6 | 1 | | | | |
| Strategy 11: Develop and support initiatives to encourage active transportation | | | | | | | |
| 11.1 | Action: Consider exploring ways to expand access to e-bikes | 5 | | | | | 1 |
| 11.2 | Action: Support education and awareness programs that are tailored to different community groups | 2 | 1 | | | | |
| 11.3 | Action: Continue to support programs and events that promote road safety and active transportation | 5 | | | | | |