



STAFF REPORT

TO: Advisory Design Panel

MEETING DATE: October 12, 2023

FROM: Planner I

FILE NOs: ZA-2023-03 & DP-2023-13

SUBJECT: Zoning Amendment and Form and Character Development Permit for mixed use Commercial/ Residential Development at 1057 Gibsons Way

REPORT FOR ADP RECOMMENDATION TO COUNCIL

PURPOSE

The Town is looking for a review of development plans for a mixed-use commercial and residential development at 1057 Gibsons Way and recommendations from the Advisory Development Panel in relation to:

1. The conformance of the proposal with the form and character guidelines of Development Permit Area No. 3 (DPA 3).
2. Other considerations of the proposal within the terms of reference of the ADP that Council might consider in relation to the rezoning request for the property.

BACKGROUND

Council reviewing the proposal on January 23, 2023, and passed the following resolution:

R2024-265 Zoning Amendment to allow mixed use Commercial/ Residential Development at 1057 Gibsons Way

MOVED by Councillor Croal
SECONDED by Councillor Thompson

THAT staff prepare a Zoning Amendment to accommodate the requested building height for the proposed Development at 1057 Gibsons Way as a height bonus conditional on secured rental tenure for the residential units located above the second storey;

AND THAT the amendment be applied to all properties of the C1 Zone to incentivize and streamline development of rental apartment housing in the Gibsons Way Commercial Corridor.

CARRIED

Additional details of staff's analysis of the proposal in relation to the Town's policies can be found in the January 23rd report to Council: [2024-01-23 Staff Report to Council re Zoning Amendment for 1057 Gibsons Way](#)

PROPOSAL SUMMARY

The following offers a summary of the proposal and planning context.

- The Town has received applications for a Zoning Amendment and form and character Development Permit to allow a proposed development at 1057 Gibsons Way having 2 commercial/ residential mixed-use buildings with Commercial uses at grade facing the Gibsons Way street frontage and 141 apartment residential units located above and behind.
- The proposal is consistent with the Mixed Use Commercial Land Use Designation, which supports an active, pedestrian-oriented commercial street frontage with residential apartment use above and behind.
- Currently proposed building heights are approximately 5-8 m higher than typical existing development in the C1 zone as measured from natural grade and 0.5 m higher than the Soames Place building as measured from the street. In response to feedback received in the January Council meeting, the applicant is reviewing the option to increase the proposed height by an additional storey (3.2 m).
- Parking is under both buildings and at grade, with a single strip of commercial parking stalls fronting the building on Gibsons Way, buffered by a planting strip. Parking spaces are proposed to be reduced to 1.1 per unit versus 1.5 spaces per apartment unit required in the Bylaw. The reduction is supported with a traffic study and recommended transportation demand-management strategies ('TDM') including transit-orientation, walkability and bicycle parking and storage amenities.
- The proposed building height, setbacks, and parking requirements are reviewed in relation to OCP land use policies and the existing Upper Gibsons Commercial C1 and C1-A zones.
- Tenure of the residential units are proposed to be fully rental and may include supported seniors care.
- Council received the zoning amendment application on January 23rd and has given its support in principal for the proposal, with building height conditional on securing rental tenure or affordable units through a 'height bonus'. The intent is to encourage development of housing types identified as most needed in the Town's Housing Needs Assessment and to implement elements of the Town's Housing Strategy.
- A member of Council suggested that the applicant consider additional height. As this of interest to the applicant, staff would appreciate the ADP's comment on a potential increase in building height.



Figure 1 - Proposed development in summary and as seen from Gibsons Way

SITE AND SURROUNDING USES

The property at 1057 Gibsons Way has an 'L' shape and occupies just over one hectare (10,150 m²/ 112,600 ft²) at the southwest corner of the Highway 101 and Pratt Road intersection in Gibsons, across from the Wendy's and Tim Hortons restaurants and kitty-corner to the IGA Plaza.

The property is largely undeveloped, with large areas previously cleared and used for machinery storage. Stands of large coniferous trees are retained at the south and west edges of the site, some of which are proposed to be protected and retained in the development.

The north-east corner of the property has an existing commercial building fronting Highway 101, currently occupied by More Bakery and David Coyle furniture shops.

The property slopes to the south, dropping about 5m from the highway along the Pratt Road frontage and a further 5 m to the southern-most boundary of the site at the western municipal boundary.

The land uses of neighbouring properties are summarized in Table 1, below.

Table 1: Surrounding Uses.

	Existing Land Use	OCP Designation
North	Commercial	Mixed Use Commercial
South	Commercial & Rural Residential (SCRD)	SCRD Commercial (SW) and Multifamily Residential (SE)
East	Commercial	Mixed Use Commercial
West	Commercial	Mixed-Use Commercial



Figure 2 - The subject property, as viewed from the intersection of Pratt Rd and Highway 101, has a large undeveloped area and includes the site of More Bakery and David Coyle furniture shop at its northeast corner.

Figure 2, is a photo of the site taken from the intersection of Highway 101 and Pratt Road, looking to the southeast. Figure 3, below, shows the proposed site in the context of surrounding properties.

PROPOSAL

The following provides a summary by staff of key elements of the proposal.

The proposal package is enclosed with this report providing details for the ADP's consideration:

- Proposal Overview by applicant (Attachment A)
- Architectural Plan Package (Attachment B)
- Landscape Plan (attachment C)

Proposed Land Uses

The proposal for development of the large property includes two mixed-use buildings, shown in Figure 1 as seen from Gibsons Way at Pratt Road. The buildings each have commercial and live/work uses fronting Gibsons Way on the ground floor and 64 and 77 apartment residential units on the upper stories and behind the commercial uses.

Building A, as shown in the site context map in Figure 3, is oriented east-west fronting Gibsons Way at the corner of Pratt Road and the highway. Building B is oriented north-south along the east side of the property, also fronting Gibsons Way.

The development is designed to meet the intent of the mixed-use commercial land use designation in the OCP to activate street frontages in the Upper Gibsons Commercial area, and to provide apartment residential uses above and surrounding the commercial uses. The building frontages on Gibsons Way are therefore occupied with three commercial retail units and five

street-front live-work units intended to have commercial use at grade level and residential accommodation located above the ground floor.

Vehicle access to the site is provided from both Gibsons Way and Pratt Road, with parking areas for the two buildings connected internally.



Figure 3 - Proposed development in the context of the site and surrounding uses

Height & Massing

The proposed height of the buildings is higher than existing development along the Highway 101 commercial corridor, and is in line with the recently approved mixed-use development at 835 Gibsons Way, and with the land uses envisioned for the upper Gibsons Commercial Area in the Official Community Plan (OCP), which do not specify a target height.

As experienced from Gibsons Way, the height of the proposed buildings is 15.5 m and four to five stories. This is half a meter (0.5 m) higher than the “Soames” building at 875 Gibsons Way per C1-A zoning regulations.

As experienced from the lower south side of the sloped property, the buildings each have 5 stories above underground parking that rises above grade for access on the downslope side.

Since receiving feedback from Council, the applicants are considering adding an additional storey (3.2 m additional height) to their proposal along with an agreement to provide secured rental and/or affordable housing units. The applicants intend to share draft plan revisions with the ADP at the February 9th meeting for comments on this proposed change.

Figure 4 - Rendering of proposed buildings A and B, as seen looking southeast from the Pratt



Road/ Highway 101 intersection.

Landscape



Figure 5 - Downslope view of Building A as seen from Pratt Road, looking northeast

The landscape plan is provided with the development application, enclosed with this report as Attachment C. The site plan distributes surface parking and access largely at the rear of the buildings. Planted greenspaces include four stands of large conifer trees to be retained and protected – two on the Pratt Road frontage and two at the south end of the property.

Additional plantings serve to buffer the development from neighbouring properties, to frame the site- and building entrances, and to screen and buffer surface parking areas.

The landscape plan supports comfortable pedestrian circulation connected by marked crossings within driveways and well-connected sidewalks.

Plantings include a mix of trees, shrubs and perennials, with an emphasis on native tree, shrub and ground-cover species.

The infrastructure department has flagged that the property provides the only feasible opportunity to connect a multi-use path from WhiteTower Park at Mahan Road to Pratt Road at Gibsons Way, subject to upcoming development of neighbouring properties to the east. Provision of a ROW for the trail connection within the site would align with objectives under development for the west end of Gibsons in the Town's Active Travel Plan, due to be completed this spring. The trail link is not included in the current site plan.

Amenities

The development proposal outlines onsite amenities in detail. These include:

- An indoor amenity room with a sheltered balcony for resident gatherings and a fitness room in each building.
- Outdoor table seating for private use of the commercial unit fronting Pratt Road.
- Outdoor bench seating within the landscaping at the northeast and northwest frontages to Gibsons Way and at the main entrance for Building B, addressing form and character guidelines calling for furniture in the street frontage.
- Irrigated pet relief areas for the use of residents of each building – an amenity the applicants note is often overlooked in residential developments.
- Bicycle storage and charging facilities.

Staff notes that no outdoor amenity space is provided for residents within the site plan. Additionally, there are no parks, children's play areas or community gardens available nearby for residents' use, and therefore staff recommended to Council that an on-site outdoor amenity space would be a desirable addition to the development proposal.

Parking

The proposal provides 191 vehicle parking spaces, a portion of which are in underground parking structures beneath each of the buildings and accessed at grade from the downslope side of the buildings. Surface-level parking is also provided and distributed throughout the site for convenient access to commercial and residential units. While commercial frontages are largely intended to be pedestrian-accessed and oriented to the street, a single row of vehicle parking fronts Building A on Gibsons Way, helping to buffer a grade differential at the street edge and to provide visible and convenient parking for customers of the commercial tenants.

The number of parking spaces includes a reduction to the number of residential units required in the zoning bylaw. This was initially proposed as a reduction to 1.1 spaces per unit from the 1.5 spaces required in Bylaw 1065, and justification for this reduction is provided in a traffic demand

report by Bundt and Associates. The parking study report recommends measures to reduce parking demand for parking and support active travel for residents. The Traffic Demand Management (TDM) measures include:

- Easily accessed vehicle storage rooms (as proposed)
- E-Bike Charging Infrastructure (provided in bicycle storage rooms)
- A Bicycle Repair Station: (provided in the bicycle storage room in Building A)
- Pedestrian Infrastructure

In consideration of increasing height to an additional storey, the applicants note they would require a further reduction to the parking requirement to about 0.85 spaces per unit and are looking into providing one or more car share spaces and vehicles in further compensation for the parking space reduction.

Staff notes that the parking rates of 0.85 and 1.1 spaces per unit are in line with typical recommendations the Town has seen for similar apartment residential proposals, particularly rental housing units, though further review may be needed to understand the appropriateness of a further reduction in parking for the site and proposed use.

Form and Character Development Permit Area No. 3 (DPA 3) Guidelines

The property at 1057 Gibsons Way is within Form and Character Development Permit Area No. 3. DPA 3 guidelines address the form and character of development in the Upper Gibsons Commercial Area. The objective of the guidelines is

“to improve the commercial area and enhance the appearance of private developments for the benefit of visitors, residents and businesses.”

DPA3 guidelines address interest and variety in the building massing and design, pedestrian orientation, landscape buffering of the site, buildings and parking, low glare lighting, location of parking behind or at the side of buildings, and site signage.

The ADP is tasked with reviewing the proposal in relation to the guidelines and to provide recommendations to Council with regard to whether the guidelines are met in the proposal or if changes should be required to better address the guidelines.

The guidelines are enclosed with this report as Attachment D, and a Summary form to assist in the review is enclosed as Attachment E.

RECOMMENDATIONS / ALTERNATIVES

Staff have provided a guide for recommendation language below:

THAT the Advisory Design Panel finds that DPA3 guidelines are well addressed and the proposal for mixed use development at 1057 Gibsons Way (ZA-2023-03/ DP-2023-13) well considered, and recommends that Council approve the development form and character of as proposed;

OR

THAT the Advisory Design Panel recommends that Council approve the form and character of the proposed mixed use development at 1057 Gibsons Way (ZA-2023-03/ DP-2023-13) with the following changes/ conditions:

[list additional changes/conditions]

*AND THAT the Advisory Design Panel [**supports/ does not support**] an additional storey to the original proposal;*

Attachments

- A – Proposal Overview (by applicant)
- B – Architectural Plan Package
- C – Landscape Plan
- D – DPA 3 Guidelines
- E – DPA 3 Review Form

Respectfully Submitted,

Kirsten Rawkins
Planner I

Report reviewed by:

	Chief Administrative Officer
	Director of Corporate Services
	Director of Finance
	Director of Infrastructure Services
X	Director of Planning and Development



Render from Gibsons Way

Aerial Context Photo

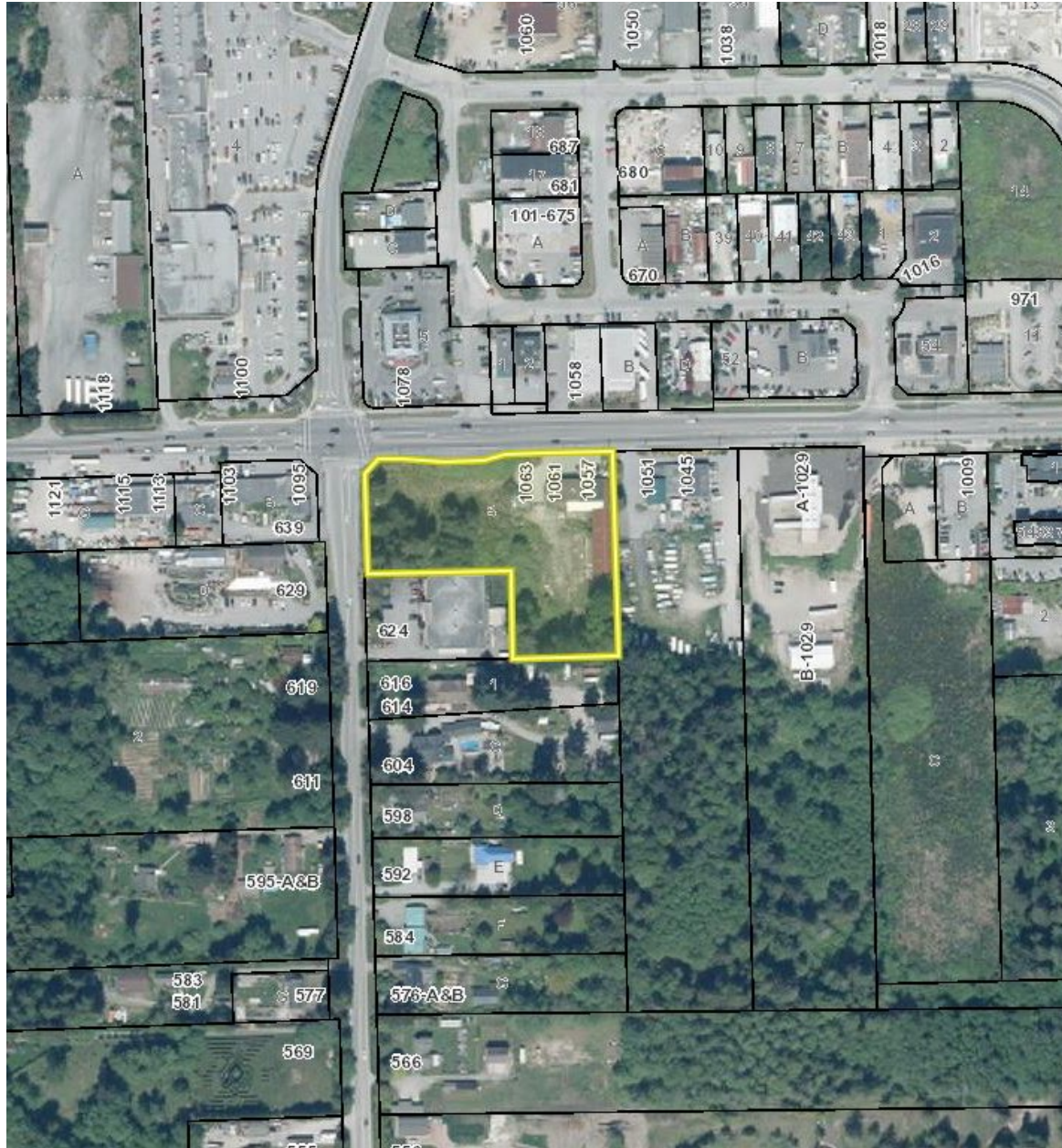


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1 Background

Project Overview

The proposed development is located at 1057 Gibsons Way at the South-East corner of Gibsons Way (Hwy #101) and Pratt Road in Upper Gibsons. This general corridor is comprised of a mix of established commercial and light industrial businesses as well as more recent mixed-use residential and commercial developments. Notably, the site is cross-corner to the Gibsons Park Plaza shopping centre and is immediately accessible via Highway #101, the #1 Langdale Ferry/Sechelt Transit Route and #90 Langdale Ferry/Sechelt Express Transit Route which provide convenient connectivity throughout Gibsons and beyond.

The proposed development provides much needed housing units, commercial/retail business, and Live-Work opportunities in a key growing area of the Town of Gibsons where a wide variety of services is accessible within walking distance and via transit. Elphinstone Secondary School and Gibsons Elementary, the Gibsons & Area Community Centre, White Tower Park, and Brothers Park are all conveniently located within approximately 1.0 km of the subject site.

Site Description

The project site is comprised of:

Building A

- Five-storey mixed-use building
- 3 Commercial Retail Units (total gross area 3,061 sq. ft.)
- 3 Live-Work units
 - total commercial area 3,086 sq. ft.
 - total residential area 2,864 sq. ft.
- 64 residential apartment units.
 - 26 two-bedroom units
 - 23 one-bedroom units
 - 15 studio/bachelor units
- 36 residential underground parking stalls in P1.
- 38 residential surface parking stalls
- 6 surface visitor parking stalls
- 14 surface commercial/live-work parking stalls
- 1 Loading stall
- 2 Accessible parking stalls (included in above calculations)
- 83 Class 1 Bicycle spaces and 14 + 4 Class 2 Bicycle spaces
- 9 EV Chargers as per bylaw in addition to rough-in for additional EV chargers in parkade

Building B

- Five-storey mixed-use building
- 2 Live-Work units
 - total commercial area of 2,934 sq. ft.
 - total residential area of 1,962 sq. ft
- 77 residential apartment units
 - 24 two-bedroom units
 - 43 one-bedroom units
 - 10 studio/bachelor units
- 36 residential underground parking stalls in P1
- 51 residential underground parking stalls in P2
- 5 visitor surface parking stalls
- 5 surface live-work parking stalls
- 1 Loading stall
- 4 Accessible parking stalls (included in above calculations)
- 101 Class 1 Bicycle spaces and 16 Class 2 Bicycle spaces
- 10 EV Charges as per bylaw in addition to rough in for additional EV chargers in parkade

The proposed development will provide much needed housing to the community, as well as commercial and Live-Work spaces at the ground floor suitable for local businesses, retail, professional services or medical practices, and restaurants. Residents will benefit from the diversity of unit mix, including studios, one bedroom and family-sized two-bedroom units. There will also be features such as a shared fitness center and two amenity rooms, one in each building.

2 Design Rationale

Site Planning

Various scenarios were examined in determining the site layout, in balance of several goals: ease of access from main roads, visibility of commercial uses, mature tree retention, using the slope to advantage, and providing residents with privacy and views. Commercial units were focused along the northern axis of the site, parallel to Gibsons Way.

The main points of entry to the site are off of Pratt Road and Gibsons Way, at the appropriate distance from the main intersection to avoid congestion. The access and parking directly off of Gibsons Way facilitates parking for commercial uses, while the other access provides a direct route to the parkade entries and surface parking for residents at the lower levels of both buildings. The significant grade change on the lot also facilitates the entry for the second level of Building B's parkade to be at the south end of the lot. The shape of the lot lends itself to the proposed configuration, with the length parallel to Gibsons Way for Building A and perpendicular for Building B.

Pedestrian connections to Gibsons Way are provided and are set back from being directly by traffic through means of a landscape buffer, with the exception of the existing bus stop lay-by connection. This allows for both the public to walk to the commercial uses and residents to have easy walking access to other services located nearby. Bicycle lockups are conveniently located to allow residents easy access to and from the site. Waste and Recycling collection will take place entering off Gibsons Way and stopping and proceeding south.

Mature trees on the site to be retained are located at the corner feature of Pratt Road and Gibsons Way, as well as at the entry off Pratt Road, and also occupy the south end of the lot.



Architecture

The buildings are designed with a two-storey commercial and live/work element fronting Gibsons Way and the residential suites set back from this, providing both separation for the residents and a break in the massing. Stonework and cement board paneling is used as cladding to distinguish the commercial and live/work areas from the residential suites, which are clad with lap siding. Additionally, the façades are stepped at different heights over the commercial portions and at the residential entry to distinguish the different suites and break up the massing. The corner façade which faces both Gibsons Way and Pratt Road was raised the most to highlight this important intersection and create a sense of place, grounded by the retained trees and the proposed plaza area (see Landscape Plan) The arrangement of residential suites dictates the shape of the upper floorplates, allowing for jogs in the building which break up the massing and add private nooks for residents to enjoy their outdoor spaces.



The two buildings are articulated with subtle differences in colour. Building A commercial areas have white paneling while Building B have a cream-coloured paneling and more stonework. The residential entries are signalled by the navy-blue paneling on each entrance façade, and the building masses are broken up with cladding colour changes. Horizontally, the commercial areas have a banding of dark grey just above their awnings, to which their unit signage is affixed.



Rendered View of Building A from Gibsons Way & Pratt Road

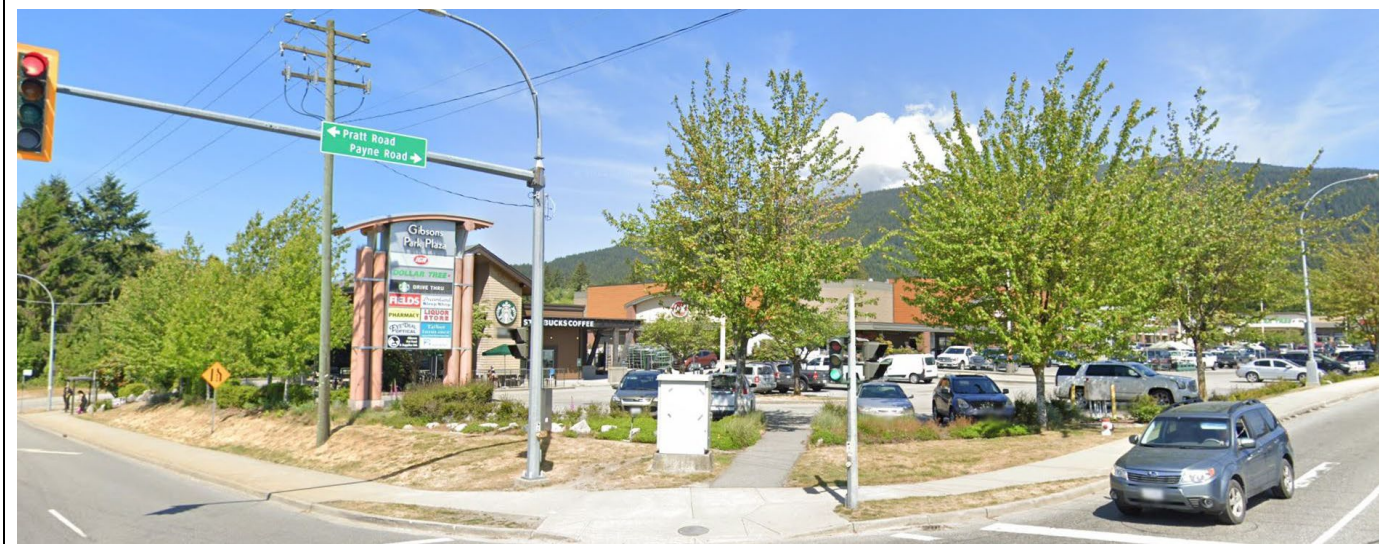


Rendered View of Building B from Gibsons Way



Views of Existing Neighbourhood along Gibsons Way:

North Side of Gibsons Way at Payne Road: IGA/Starbucks/Mall



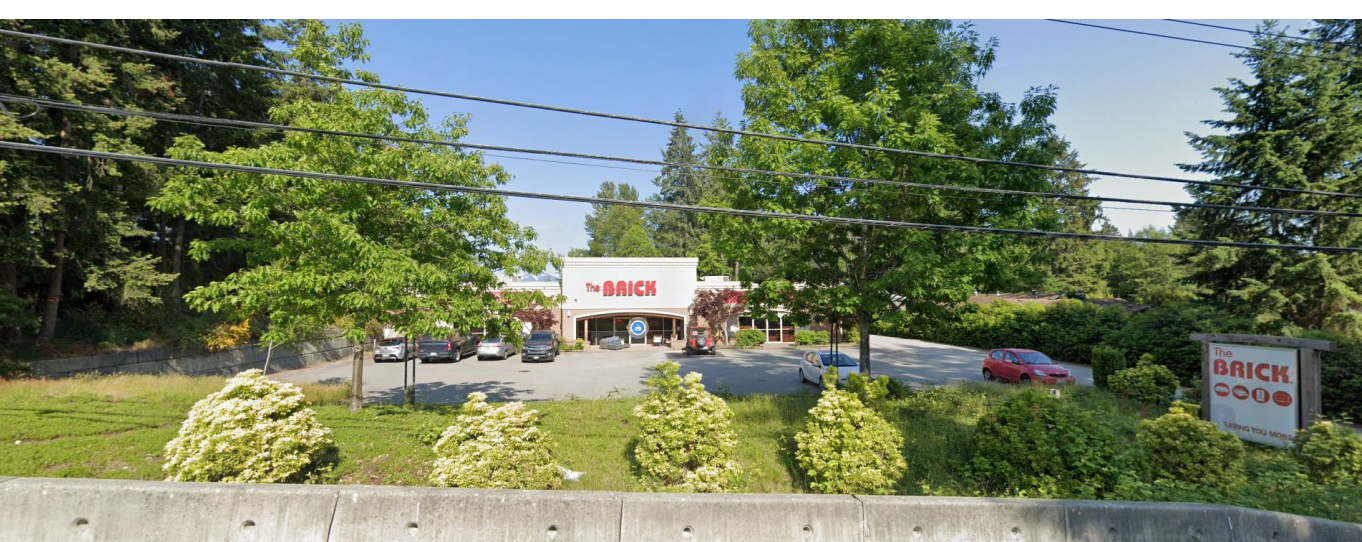
North Side of Gibsons Way, East of Payne Road: Wendy's/Tim Hortons/Veterinary Clinic



South Side of Gibsons Way, West of Pratt: Industrial



Pratt Road, South of Site: The Brick



South Side of Gibsons Way, East of Site: Auto parts & Dance



North Side of Gibsons Way, East end of Site: Industrial



Existing Site Viewed from Gibsons Way



Existing Site Viewed from Pratt Road



Project Colours & Finishes:

Natural tones were used in the following colours:

White, Brown, Dark Blue, Beige, Dark Gray



FINISHES

	<p><u>BUILDING A - Commercial entry</u></p> <p>Lap siding - White</p> <p>Lap siding - Dark Blue</p> <p>Lap siding - Brown</p> <p>Panel siding - Dark Blue</p> <p>Stone veneer cladding - Beige</p> <p>Panel siding - Dark Gray</p> <p>Panel siding - White</p> <p>Stone veneer cladding - Beige</p>
	<p><u>BUILDING A - East side</u></p> <p>Lap siding - White</p> <p>Lap siding - Brown</p> <p>Panel siding - Dark Gray</p> <p>Panel siding - White</p> <p>Stone veneer cladding - Beige</p>

	<p><u>BUILDING B - Residential entry</u></p> <p>Panel siding - Dark Blue</p> <p>Lap siding - White</p> <p>Lap siding - Brown</p> <p>Stone veneer cladding - Beige</p>
	<p><u>BUILDING B - Live/Work</u></p> <p>Soffit - Brown</p> <p>Panel siding - Dark Blue</p> <p>Panel siding - Dark Gray</p> <p>Panel siding - Beige</p> <p>Stone veneer cladding - Beige</p>



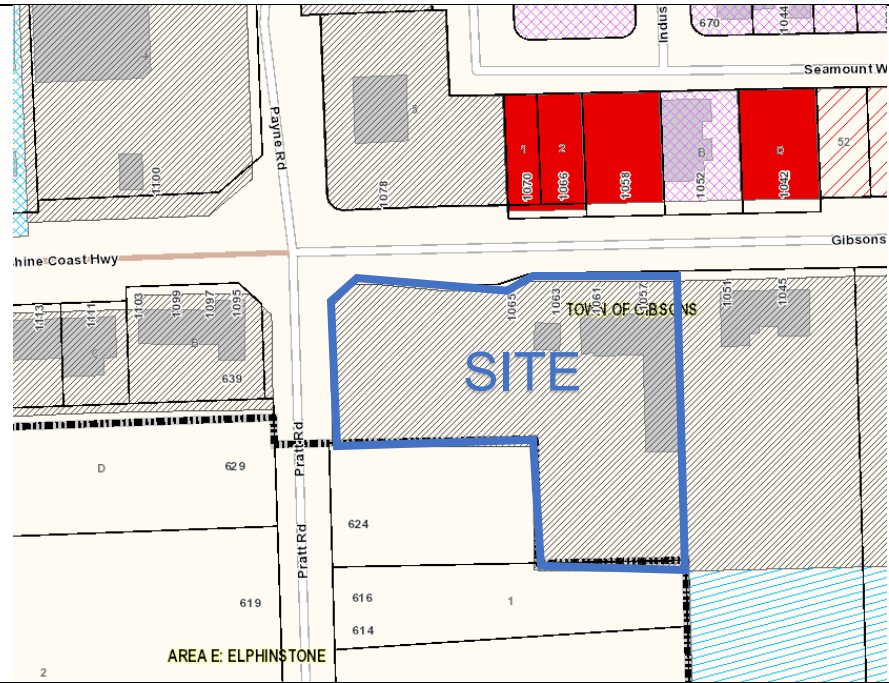
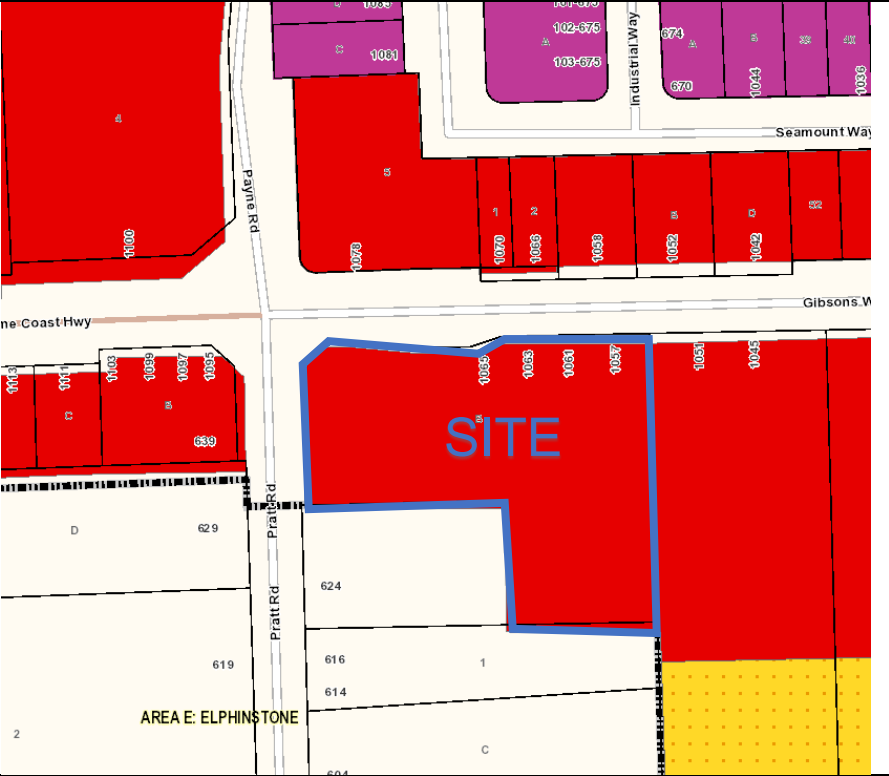
<p>Infrastructure servicing</p> <p>Servicing for this development project is proposed to extend from offsite municipal infrastructure located within both Pratt Road and Gibson Way. A gravity sanitary service connection is proposed to extend across the street to the property line from the existing sanitary main located on the north side of Gibsons Way. Grade of the site necessitates that an onsite low pressure sanitary system will be required to pump flows up to discharge to the service connection at the property line. A mechanical pump is anticipated to be required to pump flow from both buildings.</p> <p>Water servicing to the site is proposed to extend south from Seamount Way across Gibsons Way to the site. A water meter chamber is anticipated at the property line after which water servicing is proposed to split into dedicated fire and domestic service mains extending to each of the two buildings. An upgrade to the existing offsite water mains connecting between the site and the intersection of Seamount Way and Industrial Way is anticipated to be required to provide adequate fire flow for the development.</p> <p>Runoff from the site is proposed to be managed in two different ways. Grade of the site does not permit collection of runoff from the entire site to be directed to the property via gravity. The portion of runoff that can be discharged via gravity is anticipated to be collected and directed towards a detention tank located under the drive aisle in the southwest corner of the site. Flow from the site will be controlled using a flow control system and limited to pre-development levels. All flow leaving the site will pass through an oil separator water quality device prior to discharge. Flow discharged offsite is anticipated to be directed to the existing ditch on the east side of Pratt Road. Runoff from the area of the site that cannot be discharged from the site via gravity is proposed to be directed to a rock pit infiltration area located in the southeast corner of the site.</p>	<p>Landscape</p> <p>PMG Landscape Architects has developed the overall landscape design at 1057 Gibsons Way with respect to the natural context and conditions of the site, while also recognizing the Sunshine Coast’s changing climate by planning for landscape resiliency going into the future.</p> <p>A key priority for the landscape design was to save as many of the existing trees on site as possible. Trees such as these will be invaluable as land along Gibsons Way continues to develop, providing shade and contributing to the space’s sense of place, preserving the natural history of the Sunshine Coast. The planting design supports the goal to retain 23 on-site trees, and to prioritize native plants where appropriate. Our planting design also recognizes the quickly changing climate of the Sunshine Coast. We have limited the use of lawn. In areas where the planting plan deviates from native selections, we have specified plants that are drought tolerant, and will thrive in future climate models for the area. The planting throughout the site also seeks to support pollinators, particularly through the seeding of a native grass and wildflower mix that will cover the South and East landscape buffers.</p> <p>For visitors and residents of the site, the design includes ample bike parking facilities nearby all major entrances and gathering areas. There is an informal plaza at the Northwestern corner of the site, giving the public an opportunity to sit and rest along Gibsons Way. For residents, we have provided two pet relief areas, which will support those residents with active pet-oriented lifestyles, as well as provide informal opportunities for interaction between residents.</p>
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3 Sustainability

The project incorporates various strategies for achieving a sustainable footprint:

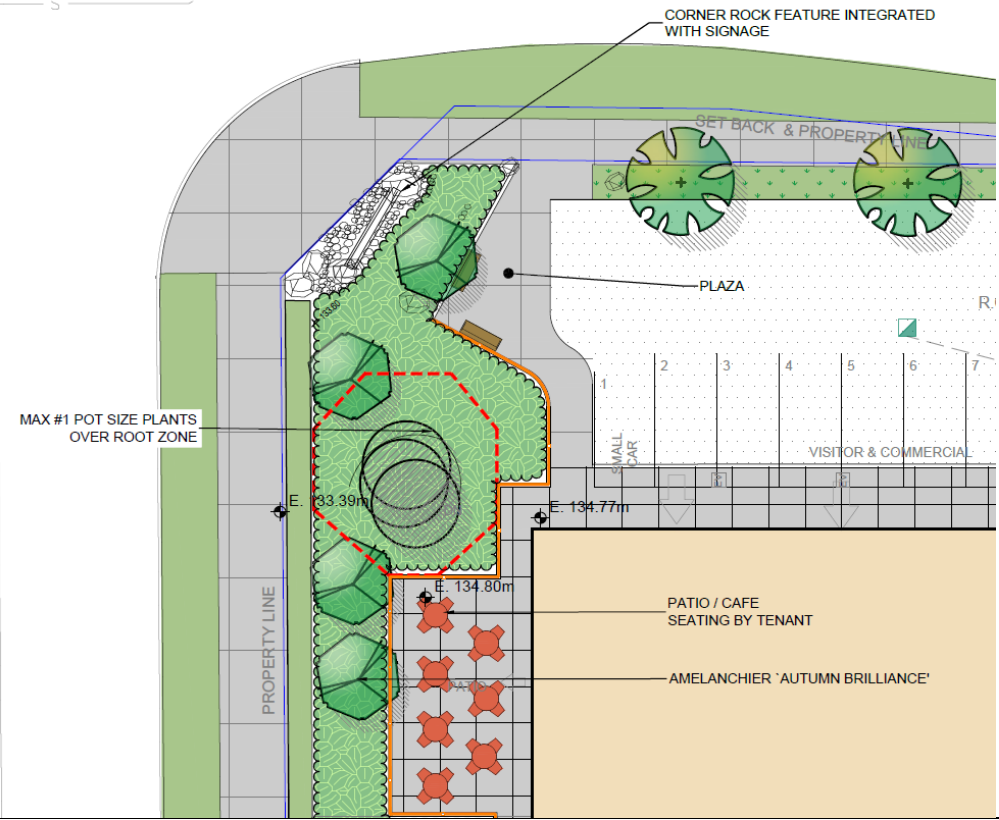
- Vehicle usage - The project proposes lower parking ratios (based on the mixed-use transit-oriented nature of the development site) and offers a variety of secure, indoor-bicycle parking stalls, including some oversized spaces and outlets for e-bikes.
- A multifamily project, through densification, allows for lower land, energy, and water usage per occupant.
- The location and mixed-use nature of this development (including Live-Work), as well as proximity to amenities, groceries, schools, employment, and transit routes, along with the lower parking utilization, are inherently sustainable as they reduce reliance on personal vehicles. There are numerous examples of communities who are relaxing, if not removing entirely, minimum parking requirements in order to support housing and help address climate change.
- Landscape Planting – Preference for indigenous and drought tolerant plantings; create an environment that is an attractive habitat.
- Storm water management will be designed by a professional engineer and will include on-site stormwater detention as well as oil separators.
- Reduce urban heat island effect - through landscaping and mature tree retention.
- Water conservation and efficiency – use low maintenance native plantings, low flush toilets, low-flow plumbing fixtures.
- Energy – All units will be constructed with use of high-performance window glazing and envelope assembly, and optimized heating and cooling strategies through energy modeling under the advice of consulting Engineers. The goal is to achieve Step 3 of BC's Energy Step Code.
- Materials – use of durable materials, avoid use of disposable finishes, use of formaldehyde free and low VOC products.
- Avoid Light Pollution – minimize glare towards sky and neighbours by using downcast lighting fixtures.
- Site Waste Management – implement construction waste recycling program.
- Air Quality – Natural cross ventilation and filtered conditioned air, carbon monoxide monitoring.
- Indoor Environment – Low-emitting materials, Controllable ventilation and lighting systems, natural day lighting, access to views.

4 Zoning & OCP Review

<p>General Form and Character of Development</p>	
<p>Land Use:</p> <p>The site is located at the border of the Town of Gibsons and Electoral Area E: Elphinstone of the Sunshine Coast Regional District (SCRD), on the traditional territories of the Sk̓wx̓wú7mesh Úxwumixw (Squamish) Nation. The approximate property area is 112,600 square feet or 2.58 acres (1.04 hectares). When this site was amalgamated into the Town of Gibsons, it retained the C-5 zoning designation per SCRД Bylaw 310 and a Mixed-Use Commercial Land Use Plan designation per the Town of Gibsons Official Community Plan. Furthermore, the site is part of the Upper Gibsons Commercial Development Permit Area No. 3.</p> <p>Our proposal, which includes a zoning change to bring the lot into conformance with current Town of Gibsons zoning, and a proposed development of two mixed use commercial and multifamily residential buildings is in keeping with the intent of the goals and policies of the Official Community Plan and DPA-3 Area. The Upper Gibsons Commercial Area is a focal point of commercial activity and is envisioned to support a strong mix of residential development that will support local retail. Specifically, some of the major policies for the area that are relevant and aligned with our proposal are as follows:</p>	
<p>OCP:</p> <p>10.1.3 Encourage the development of buildings and developments that mix commercial uses on the ground floor facing the street with residential or office uses above or behind.</p> <p>10.2.1 Create an Upper Gibsons “town centre” focused on the area between School/North Roads and Payne/ Pratt Roads with the following guidelines:</p> <ul style="list-style-type: none">• Emphasis on commercial uses, including retail stores, banks, restaurants, office uses, visitor accommodation, entertainment facilities and personal services.• A greater pedestrian orientation than currently exists, with improvements to sidewalks and landscaping emphasized with future developments.• Incorporation of multi-unit housing adjacent to the commercial areas as well as in mixed use buildings with commercial uses on the lower floors and residential above.	

DP A-3

- Significant landscaping adjacent to public roadways and integrated within the site, use of both architectural and landscape features to provide a “gateway” or distinct entrance.
- On sloped sites, building forms should step down gradually to follow the slope of the site.
- Where commercial development incorporates a residential or multi-unit use on upper floors, a graded transition in the building height is desired to ensure adjacent properties are not faced with a massive wall.
- Buildings should be sited with the entrance to the buildings facing the street (Gibsons Way) to encourage creation of an interest and access for pedestrians. Buildings on corner sites should have façade interest facing both streets.
- The buildings or structures should be used to reinforce the definition of street corners.
- Natural landscape which includes significant tree stands should be retained and incorporated into site development plans when feasible.
- Providing pedestrian amenities within the 3.0 metre transition zone between the sidewalk and the building or parking edge such as benches, shelters, alcoves, seating, walls, trellises and arbors and pathways to the adjacent businesses.
- Parking should not visually dominate a development. Parking areas should be integrated into developments by such means as incorporating significant landscaping, coordination of outdoor elements, and linking of buildings with parking by distinctively paved walkways.
- Parking lots should be paved and shall include landscaped areas within the lot. No more than 10 parking stalls in a row should be allowed without a landscaping break.
- Low dense screening of street fronting stalls is required.

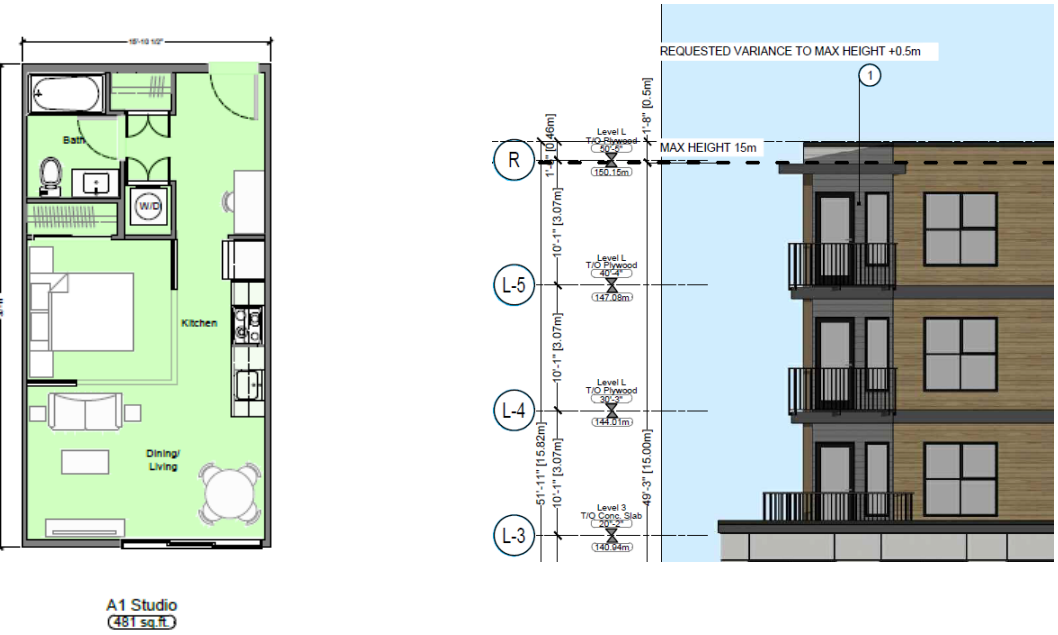


Portion of Landscape Plan at Corner of Gibsons Way & Pratt Road

Zoning

While the proposed development is generally comparable and consistent with the existing C1 - Upper Gibsons Commercial District 1 and C1A - General Commercial zoning designations, due to the unique characteristics of our proposal as we will outline below, we are requesting a Zoning Amendment for the site to a **Comprehensive Development (CD) Zone** to accommodate the proposed design elements, uses, and unique characteristics of the project. Further details are described in the Development section below, however, the main factors for this request include the following:

- Height: The proposed height is 15.5m vs the 15m allowed in C1A
- Uses: C1/C1A do not support Live Work uses.
- Apartment units: We are proposing to include bachelor suites to accommodate the growing renter demographic. Unit sizes start at 45m² (481 sq. ft.) which is below the 55.0 m² (592.0 sq. ft.) minimum outlined in the C-1 and C-1A zones.
- Parking: As demonstrated in the Transportation Report enclosed with our application and conducted by Bunt & Associates Transportation Engineers, we are seeking parking requirements within the proposed CD zone that are in keeping with the Institute of Transportation Engineers (ITE) parking utilization rates and similar past projects in Gibsons and other parts of BC. The proposed parking ratio of 1.1 stalls/unit is actually higher than the observed parking demand and vehicle ownership ratios from 3 comparable residential developments in Gibsons (see TIA by Bunt & Associates). Commercial parking is in keeping with the Bylaw Rate of 1 space per 45 m², and consistent with the Mixed-Use Gateway Zone 1 and Live-Work Zone 1 when it comes to commercial spaces that are intended to be shared with residential visitors.



Example of smallest suite size.

Height variance for Building A (Height calculated per average natural grade at front Property Line per C-1A method) (Building B meets the 15.0m calculation)

5.0 The Need for Housing

At the July 18, 2023, Committee of the Whole meeting, Town of Gibsons Council voted unanimously to commit to a new housing growth target of 90 new housing units per year (current average of 58 units/year over the past 10 years). Several relevant considerations are mentioned in the associated Staff Report:

- The purpose of the associated Staff report was “to obtain Council endorsement on an application to CMHC for the Housing Accelerator Fund, for approximately \$3 million in funding to increase the housing supply at an accelerated pace. There are 5 proposed actions as part of the Town’s Housing Accelerator Fund application:
 - Develop a Gentle Density Program
 - Review Parking Program
 - Review and Expand Density Bonusing
 - Develop a Comprehensive Land Strategy
 - OCP Update and Implementation
- Staff have been working with Urban Matters and Urban Systems to prepare a Housing Action Plan and housing supply growth target projections for the application to CMHC.

On January 19, 2021, Council endorsed the recommendations of the Housing Needs Report Implementation Framework (December 2020). This was based on the Sunshine Coast Housing Needs Assessment Report (November 2020) prepared by Urban Matters for the Town of Gibsons, District of Sechelt, and Sunshine Coast Regional District. Key determinations of the reports including the following:

- The median age in Gibsons was 54.7 in 2016 and is projected to increase to 59.6 by 2025. Between 2016 and 2025, the average household size is projected to decrease from 2.09 to 1.97. As the population continues to age, there will be increased need for smaller units in accessible housing forms and supports to meet the needs of seniors living alone, or older couples whose children have left home who are looking for diverse housing options.
- Gibsons is growing faster than the Coast as a whole. Between 2006 and 2016, the population of Gibsons grew by 10% compared to 8% population growth across the Coast as a whole. Projections suggest this rate of growth will be sustained in the coming years.
- The population in Gibsons grew from 4,605 in 2016 to 4,758 in 2021 or by 3.3%.

The table below is an excerpt from the November 2020 Housing Needs Assessment Report, identifying the need for 178 units between 2016-2020 and 197 units between 2020-2025, in a variety of bedroom types (from studio to 3+ bedrooms). The figures below are prior to the new housing targets endorsed by Council in 2023, as part of its application to the Housing Accelerator Fund.

Table 39: Anticipated Units for Gibsons, 2016-2025

Household Type	2016-2020					2020-2025				
	Number of Bedrooms					Number of Bedrooms				
	0	1	2	3+	Total	0	1	2	3+	Total
Couples without Children	30	15	15	0	59	25	12	12	0	49
Families with Children and Other Families	0	0	2	4	6	0	0	1	3	4
Non-Census Families	34	34	34	11	113	43	43	43	14	144
Total	63	49	51	15	178	68	55	57	17	197
% by bedrooms	36%	27%	28%	9%	100%	34%	28%	29%	9%	100%

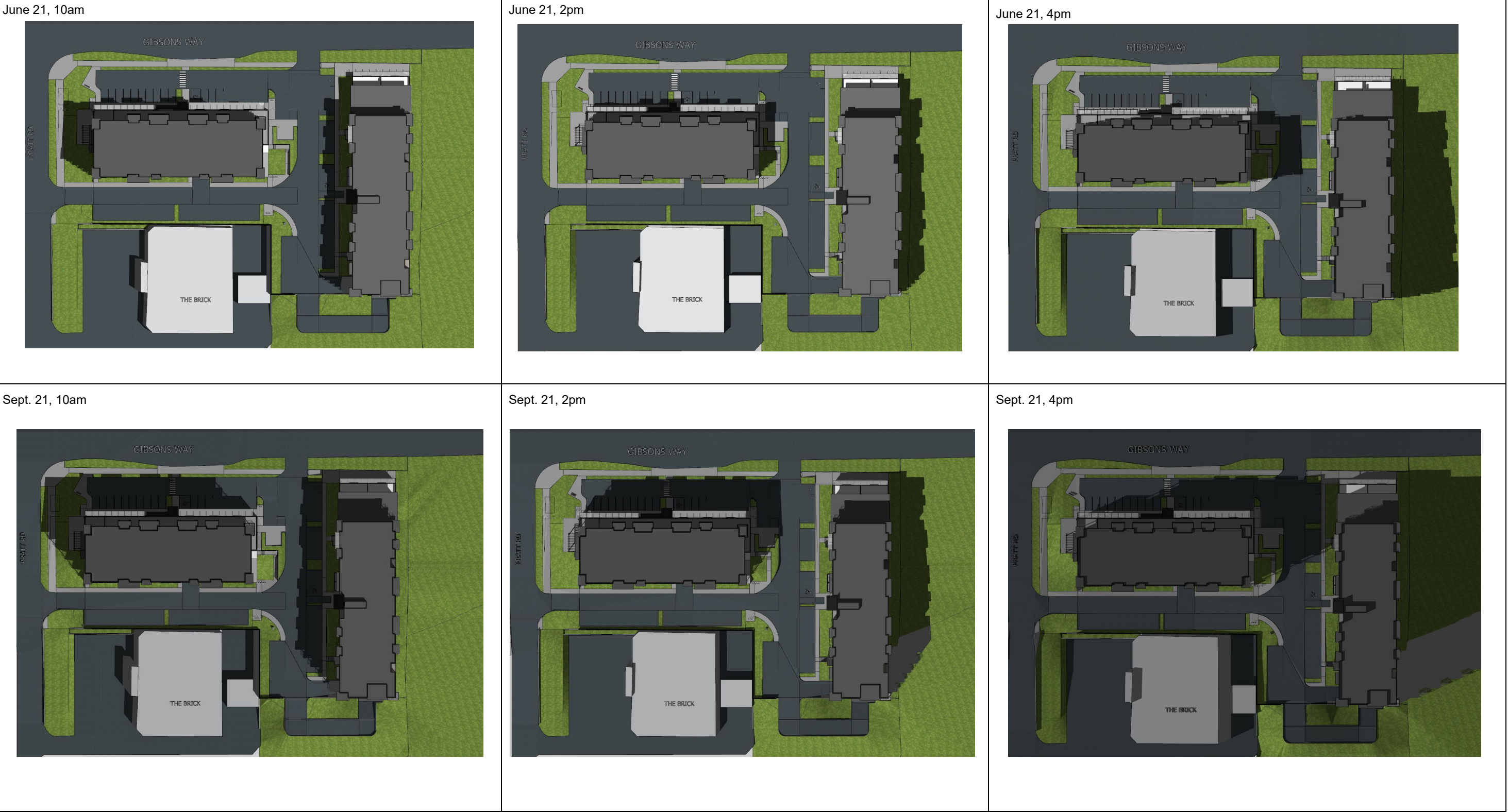
Source: Derived from Statistics Canada Census Program and BC Stats Custom Projections

To conclude, this project will add to the much-needed diversity of housing stock in the Town of Gibsons and will provide additional housing choice to residents at varying stages of their lives. Due to the current volatility of interest rate and inflationary pressures, it is not possible to commit to a rental structure at this time, however we will be monitoring this situation closely throughout the application and design process.

Image of typical floor (Building B)



Shadow Studies
Proposed development.



Dec. 21, 10am



Dec. 21, 2pm



Dec. 21, 4pm



5 Attachments

- Architectural Drawings
- Civil Drawings
- Landscape Drawings
- Transportation Impact Assessment
- Tree Evaluation Report
- Site Disclosure Statement
- Archaeological Review Letter



1 RENDER
NTS

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A-11.1	BUILDING A LEVEL 1
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GIBSONS
WAY

PARKING DATA			
BUILDING A	VEHICLE PARKING		
	AREA (M2) OR # UNITS	PARKING REQUIRED (1.5/UNIT RES.)	PARKING PROPOSED (1.1/UNIT RES.)
COMMERCIAL	571	12.7	20
RESIDENTIAL - APARTMENTS	67	100.5	72
RESIDENTIAL VISITOR (15%)	11		
SUM		113.2	92
BUILDING B			
COMMERCIAL	273	6.1	9
RESIDENTIAL - APARTMENTS	79	118.5	90
RESIDENTIAL VISITOR (15%)	12		
SUM		124.6	99
TOTAL PARKING	COMMERCIAL	18.8	29.0
	RESIDENTIAL	219.0	162
		237.8	191

BICYCLE PARKING					
BUILDING A	AREA (M2) OR # UNITS	CLASS 1 REQUIRED	C1 PROVIDED	CLASS 2 REQUIRED	C2 PROVIDED
*RESIDENTIAL - APARTMENTS	67	83.8	83	13.4	14
TOTAL A		83.8	83	13.4	14
BUILDING B					
*RESIDENTIAL - APARTMENTS	79	98.8	101	15.8	16
TOTAL B		98.8	101	15.8	16
PROJECT COMMERCIAL*	938.00	2.3	2	3.4	4
PROJECT TOTALS		182.5	184	32.6	34
*Note that Commercial units may accommodate bicycle storage within their own rental unit and are not shown in the residential storage areas					

RESIDENTIAL DATA					
UNIT MIX					
BUILDING A	LIVE/WORK	STUDIO	I-BEDROOM	2-BEDROOM	TOTALS
LEVEL 1		3	1	4	8
LEVEL 2	3	3	1	4	11
LEVEL 3		3	7	6	16
LEVEL 4		3	7	6	16
LEVEL 5		3	7	6	16
TOTALS	3	15	23	26	67
BUILDING B	LIVE/WORK	STUDIO	I-BEDROOM	2-BEDROOM	TOTALS
LEVEL 1		2	7	5	14
LEVEL 2	2	2	9	5	18
LEVEL 3		2	9	5	16
LEVEL 4		2	9	5	16
LEVEL 5		2	9	4	15
TOTALS	2	10	43	24	79

PARKING COUNT	
BUILDING A	VEHICLE PARKING
UNDERGROUND PARKADE	PARKING PROVIDED
	36
BUILDING B	
PI	36
P2	51
TOTAL UNDERGR. BLDG B	87
PARKING AT GRADE	68
LOADING	2
TOTAL	193

2 STATISTICS
Scale: NTS

CRU AREA CALCULATION		
	Square Ft.	Square Meters
BUILDING A		
RETAIL 1	997	92.6
RETAIL 2	1027	95.4
RETAIL 1	1037	96.3
LIVE/WORK 1	841	78.1
LIVE/WORK 2	844	78.4
LIVE/WORK 3	1401	130.2
TOTALS:	6147	571.1
BUILDING B		
LIVE/WORK 1	1466	136.2
LIVE/WORK 2	1468	136.4
TOTALS:	2934	272.6

RESIDENTIAL DATA																							
SUITE TYPE	A1	A2	B1	B2	B4	B5	B6	B7	B8	B9	B10	C1	C2	C2.2	C2.3	C4	C4.1	C5	C6	C7	C8	D2	TOTALS
*AREA (FT²)	481	485	613	613	537	646	646	556	623	681	681	861	861	854	856	807	806	815	835	928	755	854	
COUNT BLDG A	10	5	3	3	5	6	6	0	0	0	0	6	6	1	1	1	1	5	5	0	0	0	64
COUNT BLDG B	10	0	0	0	0	15	10	5	5	4	4	0	0	0	0	0	0	0	0	4	5	15	77
TOTALS	20	5	3	3	5	21	16	5	5	4	4	6	6	1	1	1	1	5	5	4	5	15	141

*NOTE: AREAS CALCULATED ARE PER TOWN OF GIBSONS BYLAW DEFINITION

Cover Sheet

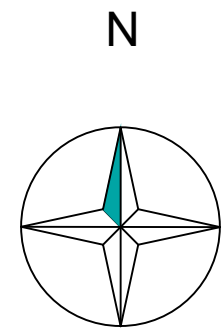
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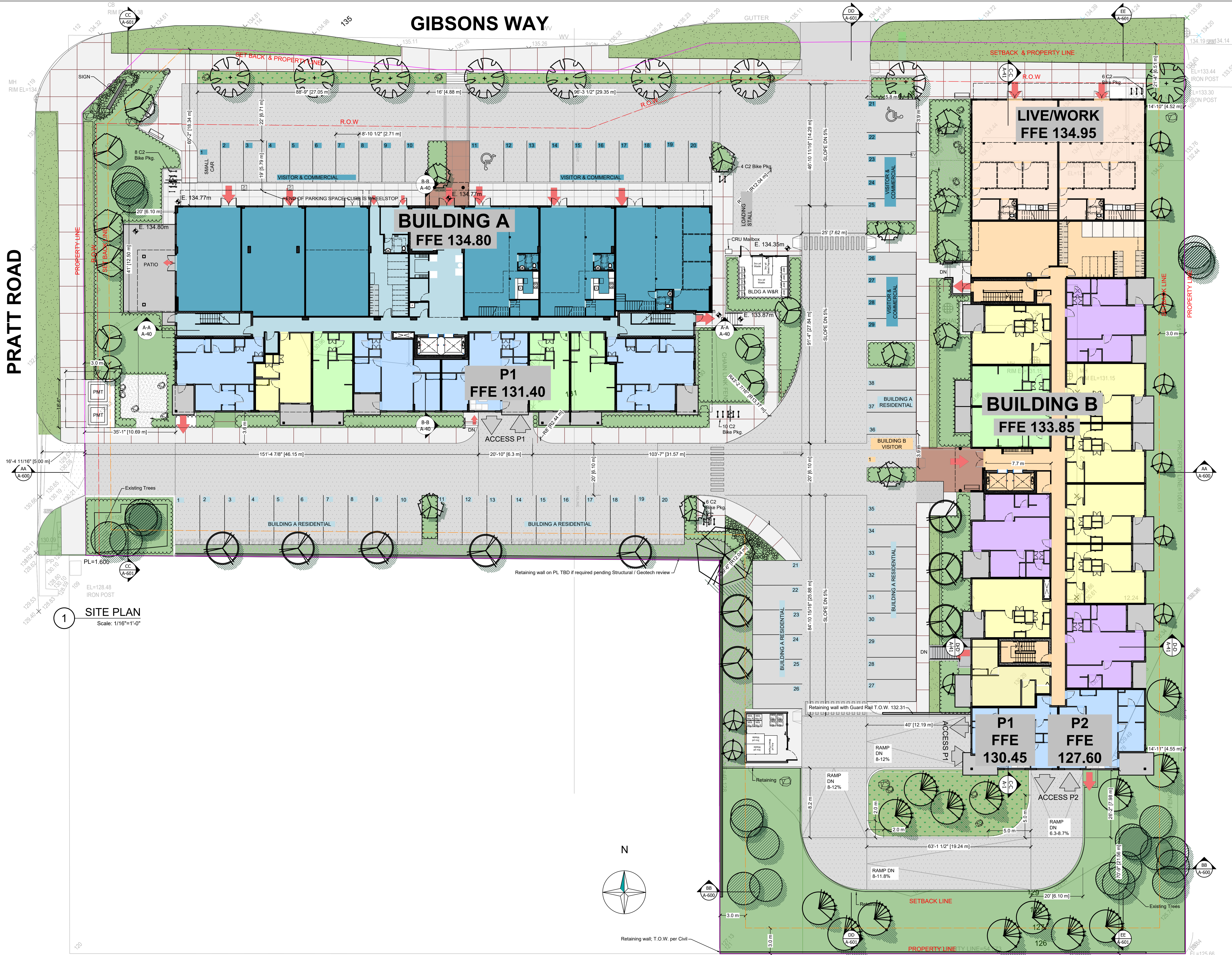
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A-C



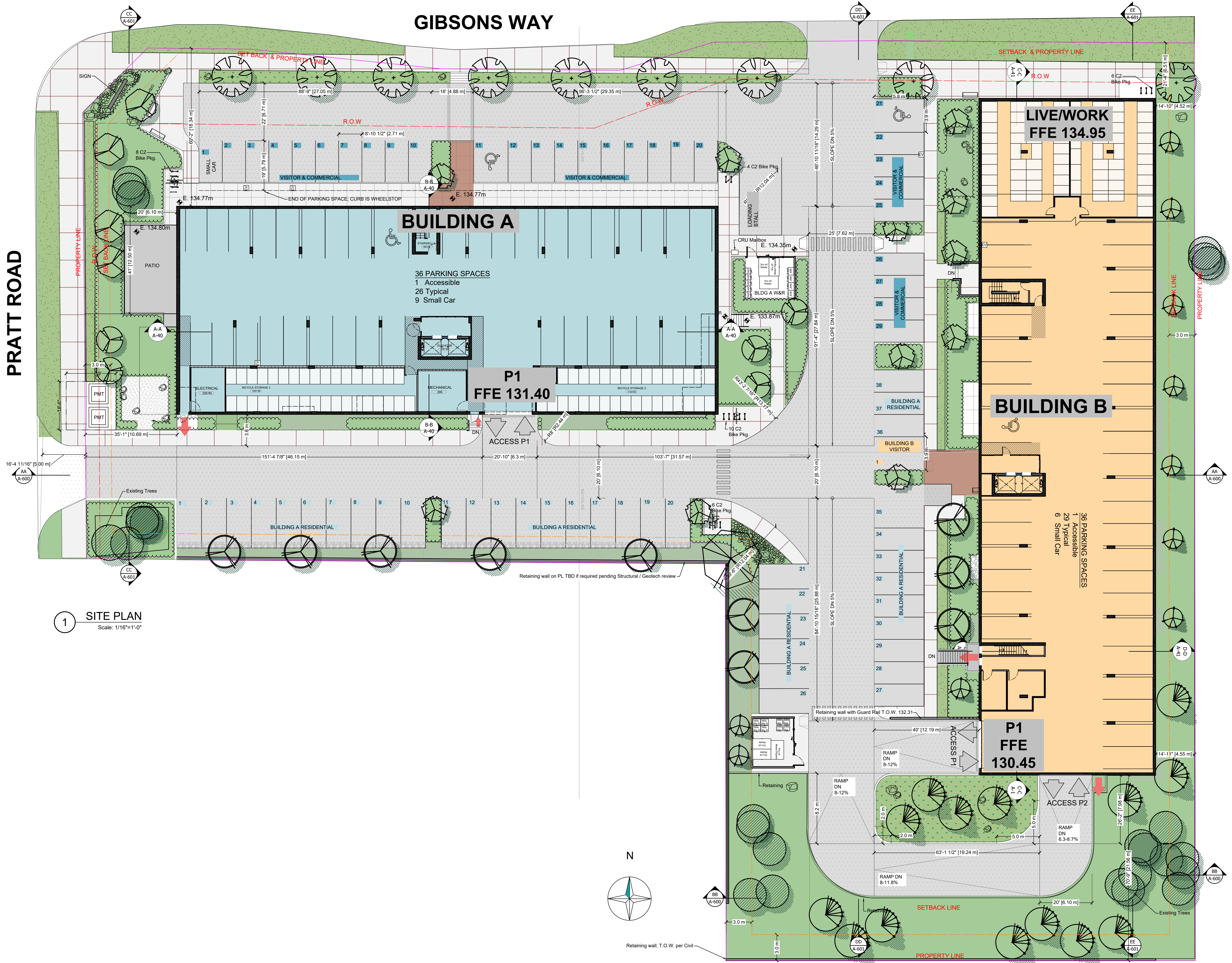
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GIBSONS WAY

PRATT ROAD



1 SITE PLAN
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Site Plan - Lower access

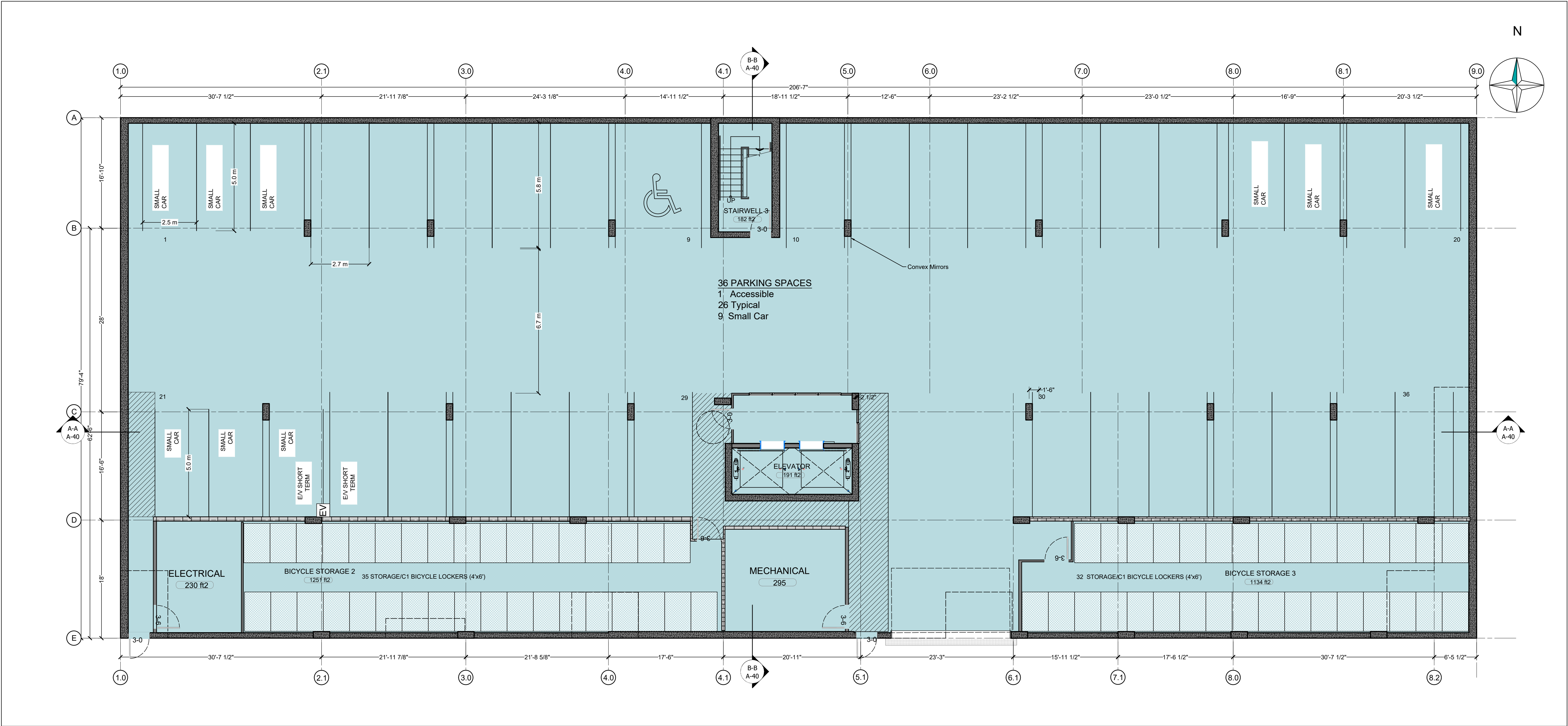
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A-02



1 BUILDING A - PARKADE LEVEL 1
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Building A Parkade 1 Plan

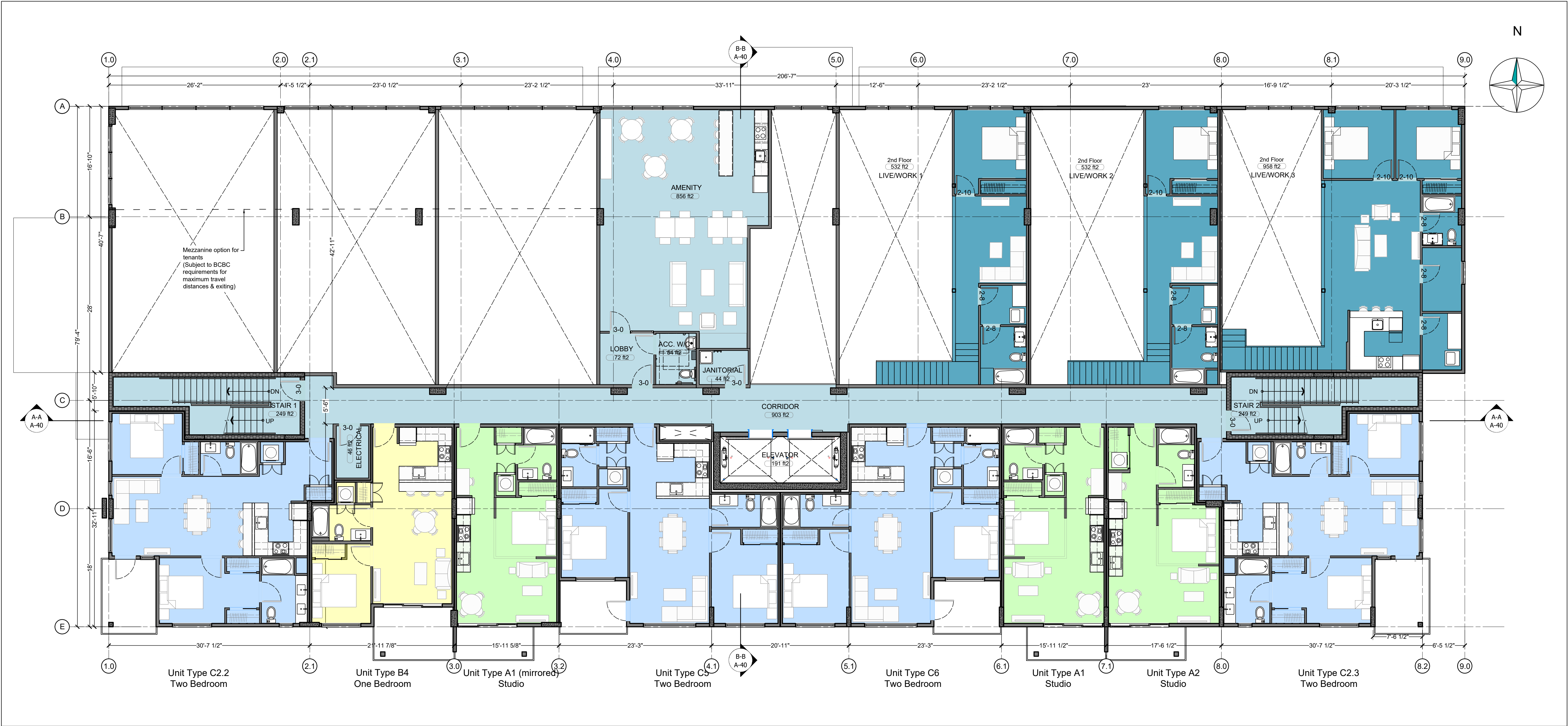
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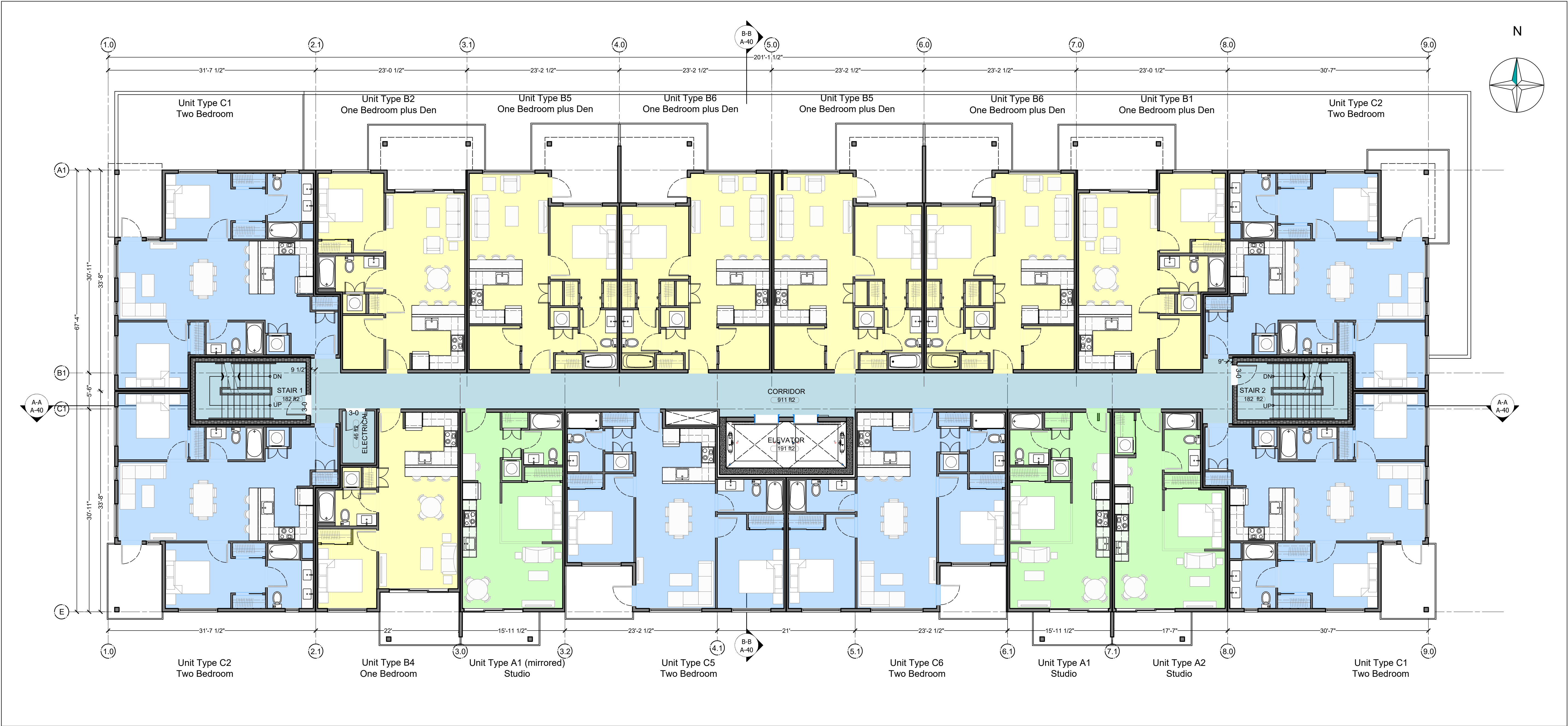
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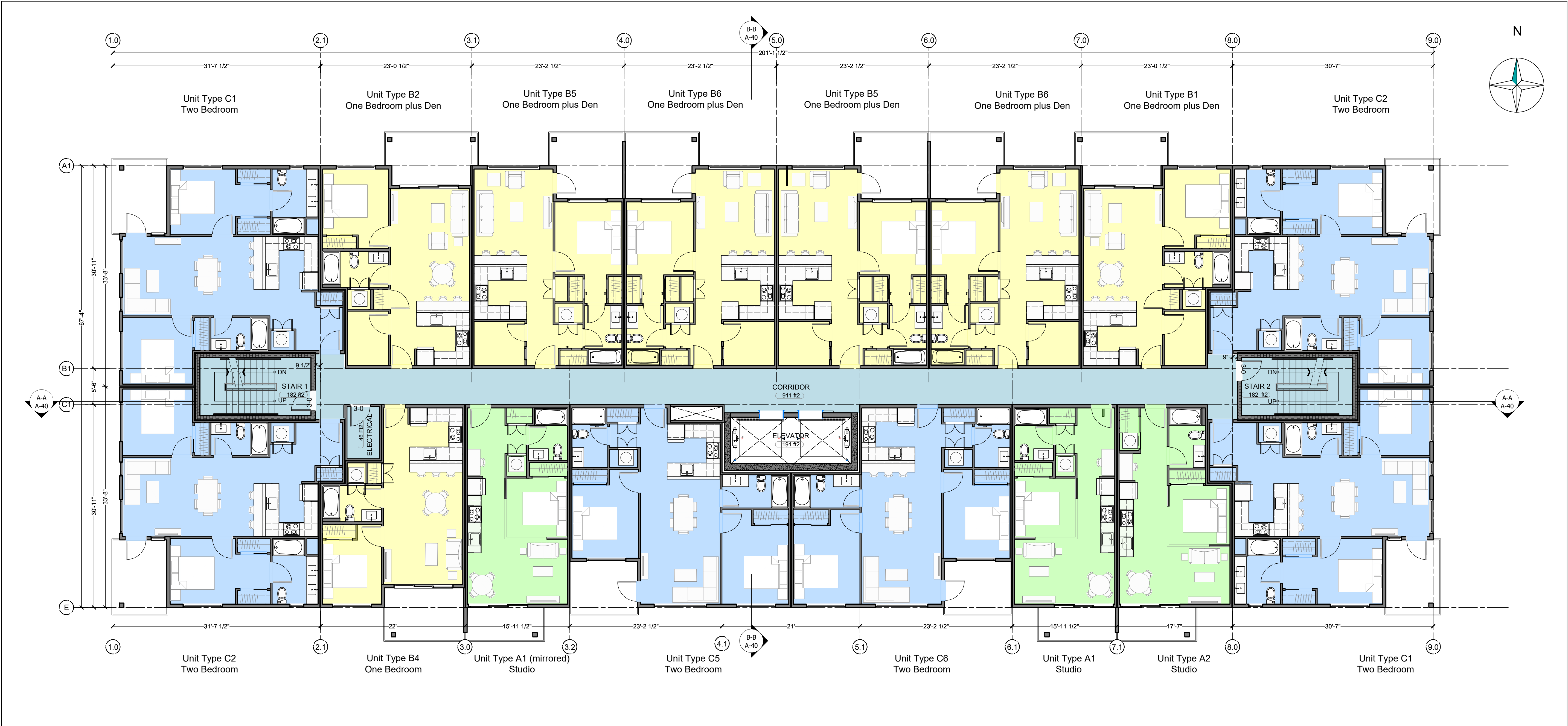
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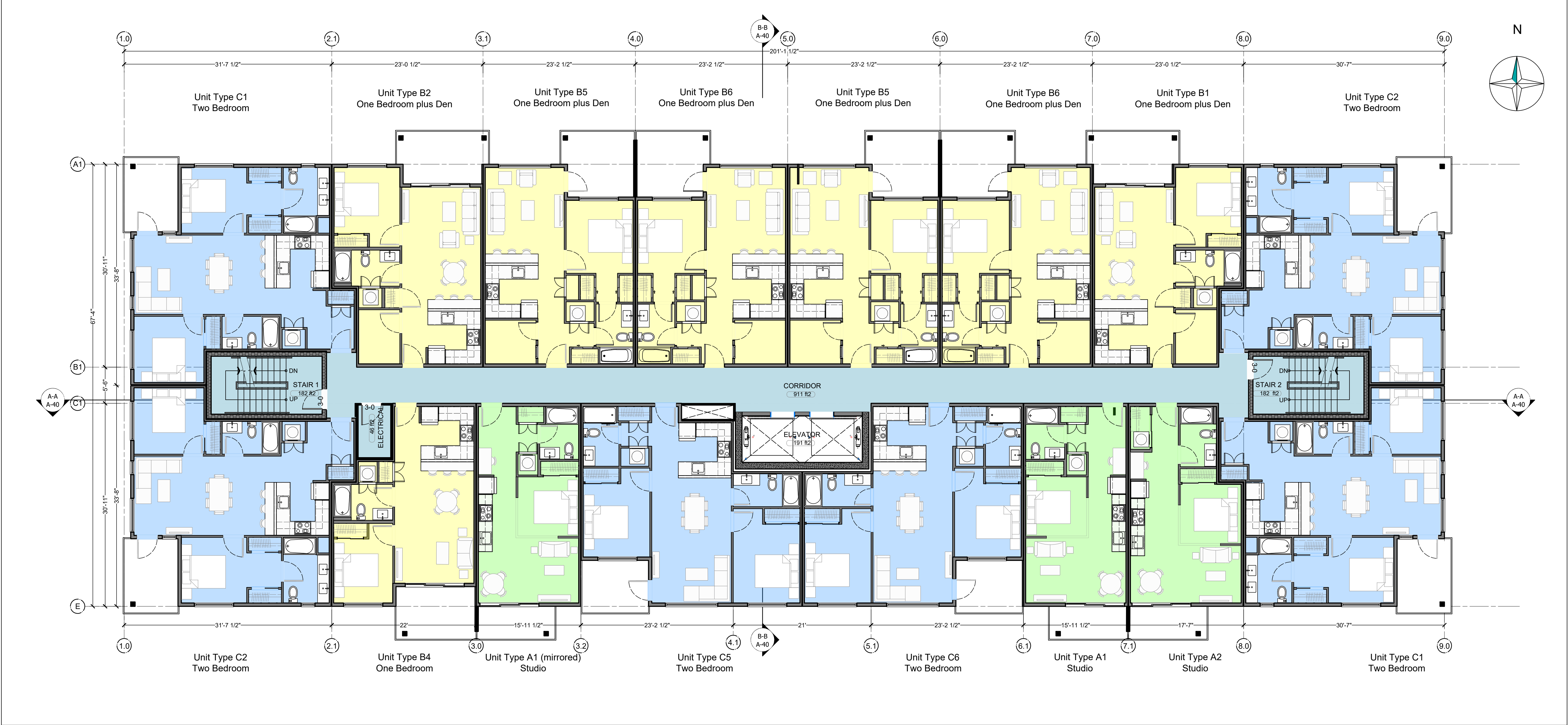
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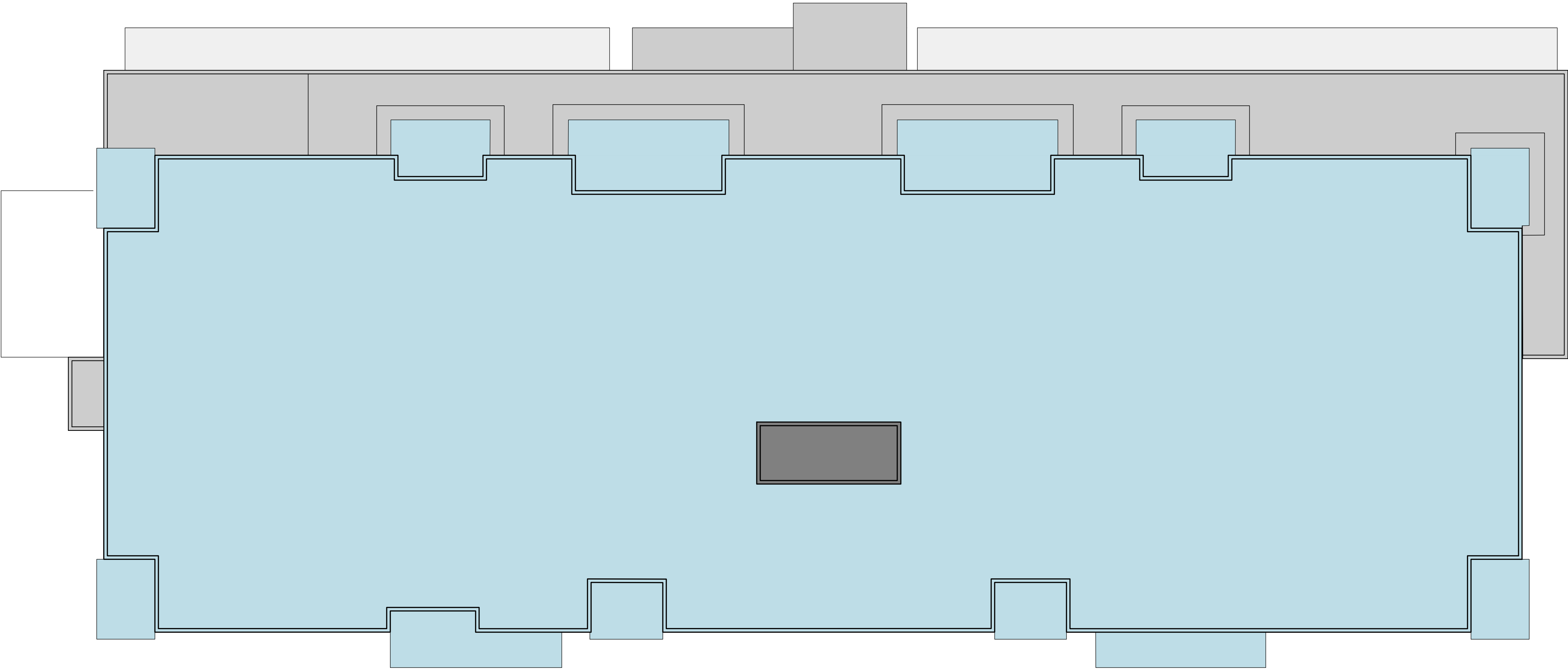
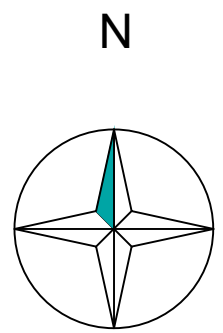
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1 BUILDING A - LEVEL 5
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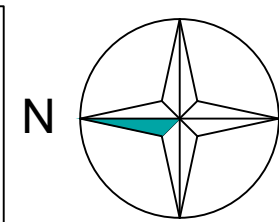
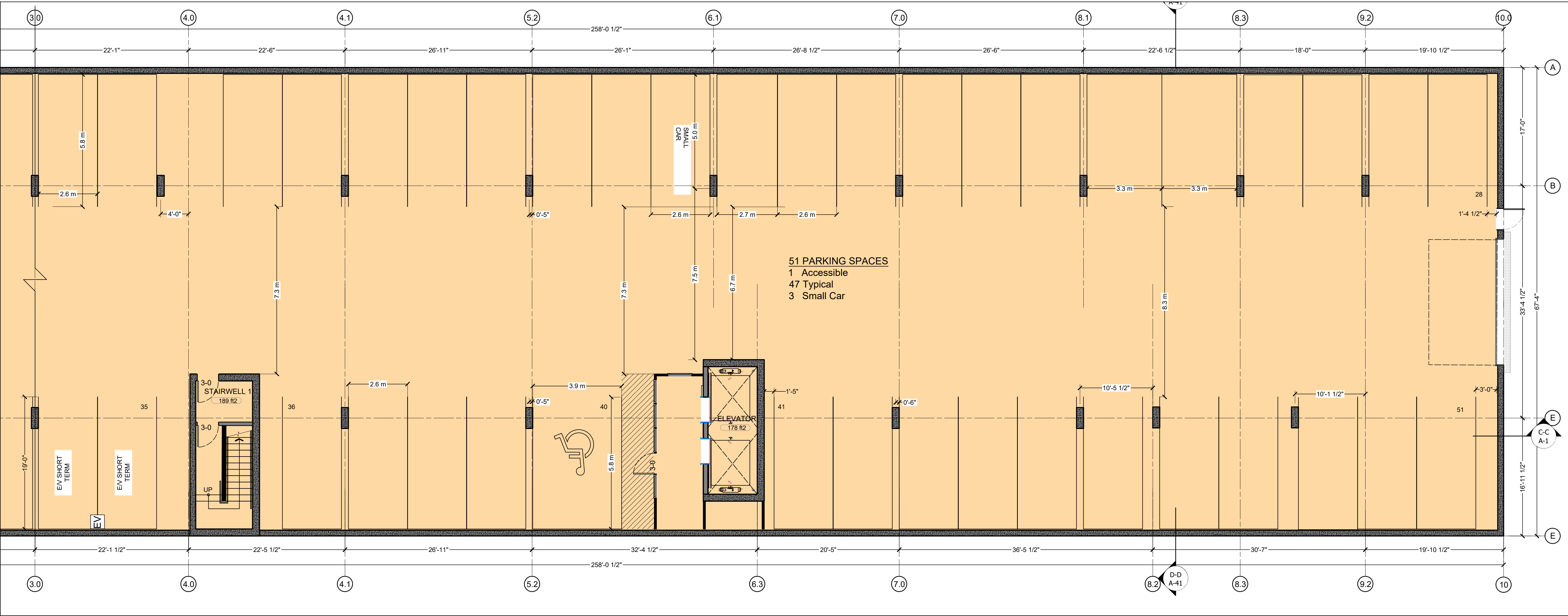
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1

BUILDING A - Roof Plan
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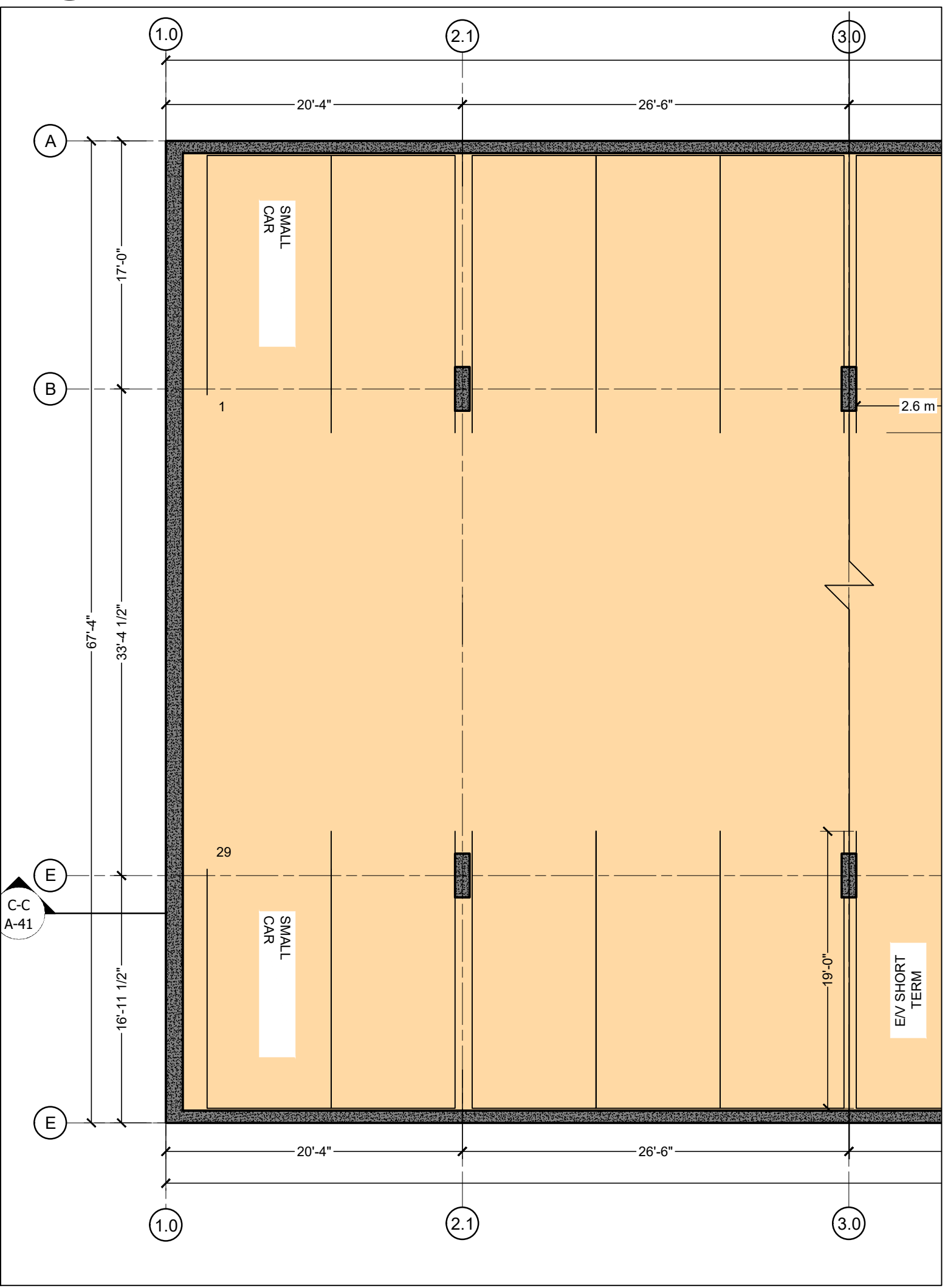
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3-4720 HIGHWAY 101
SECHLT, BC, V0N3A2
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FAX: 604 885 4312



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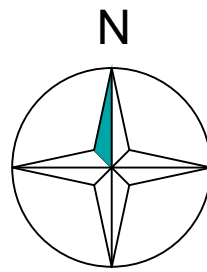
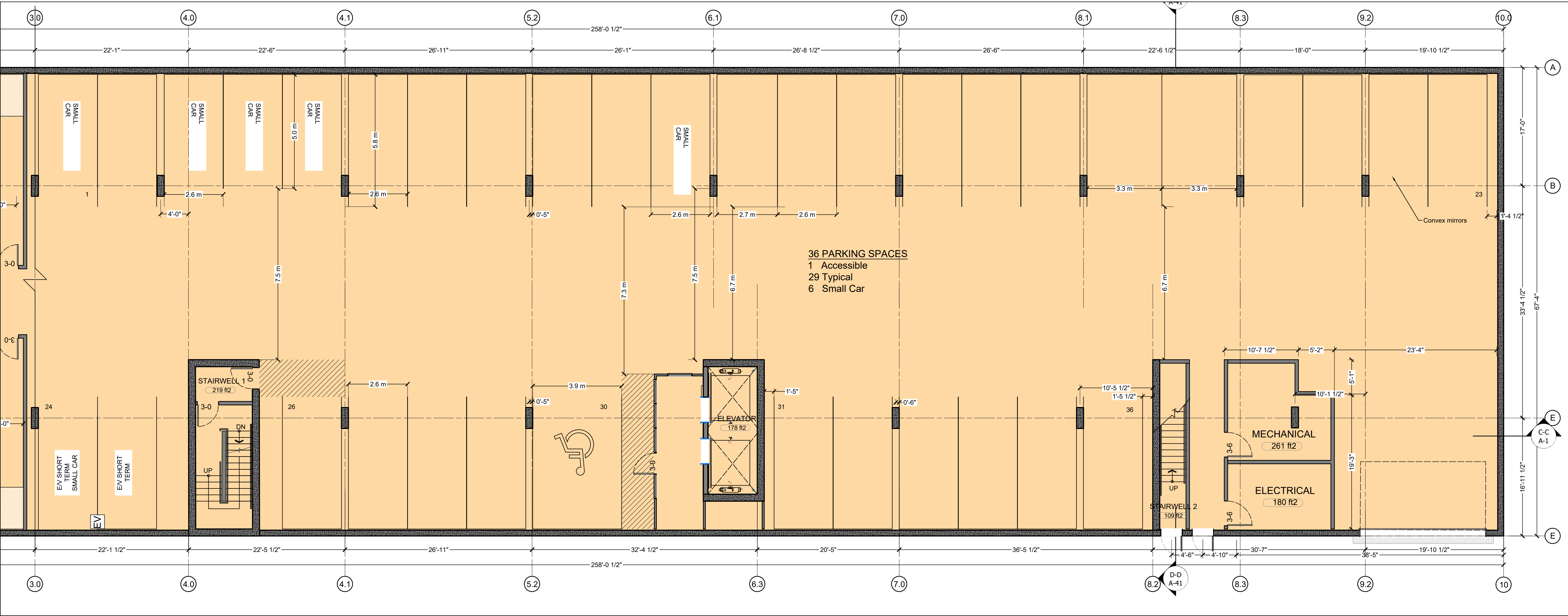
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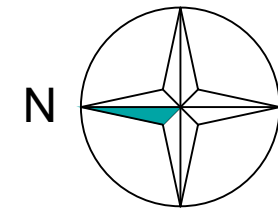


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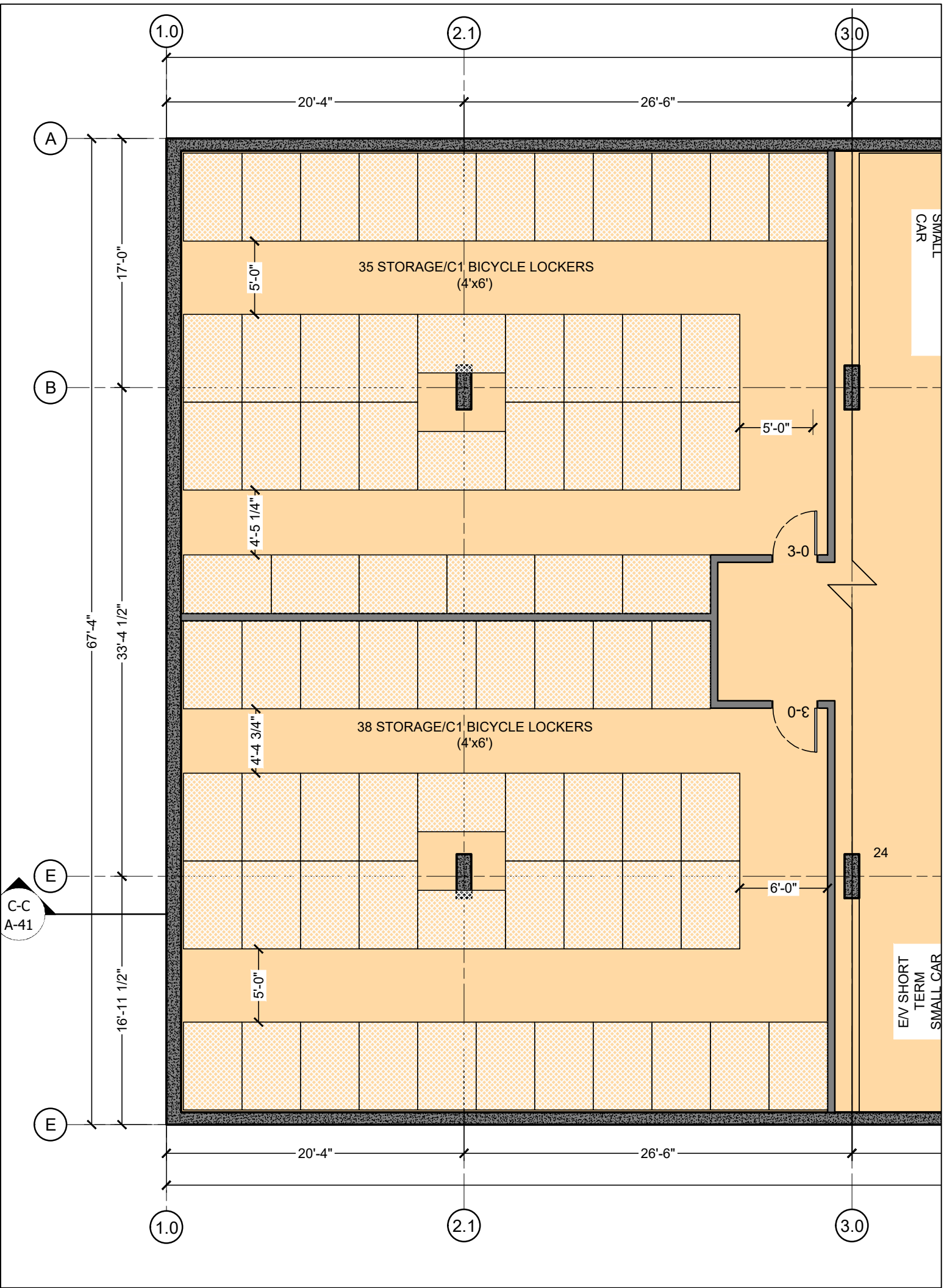
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1 BUILDING B - PARKADE LEVEL 1

Scale: 1/8"=1'-0"



2 BUILDING B - PARKADE LEVEL 1 - Continued

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Building B P1 Plan

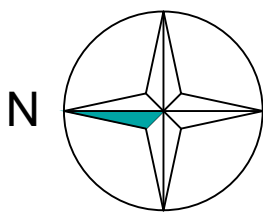
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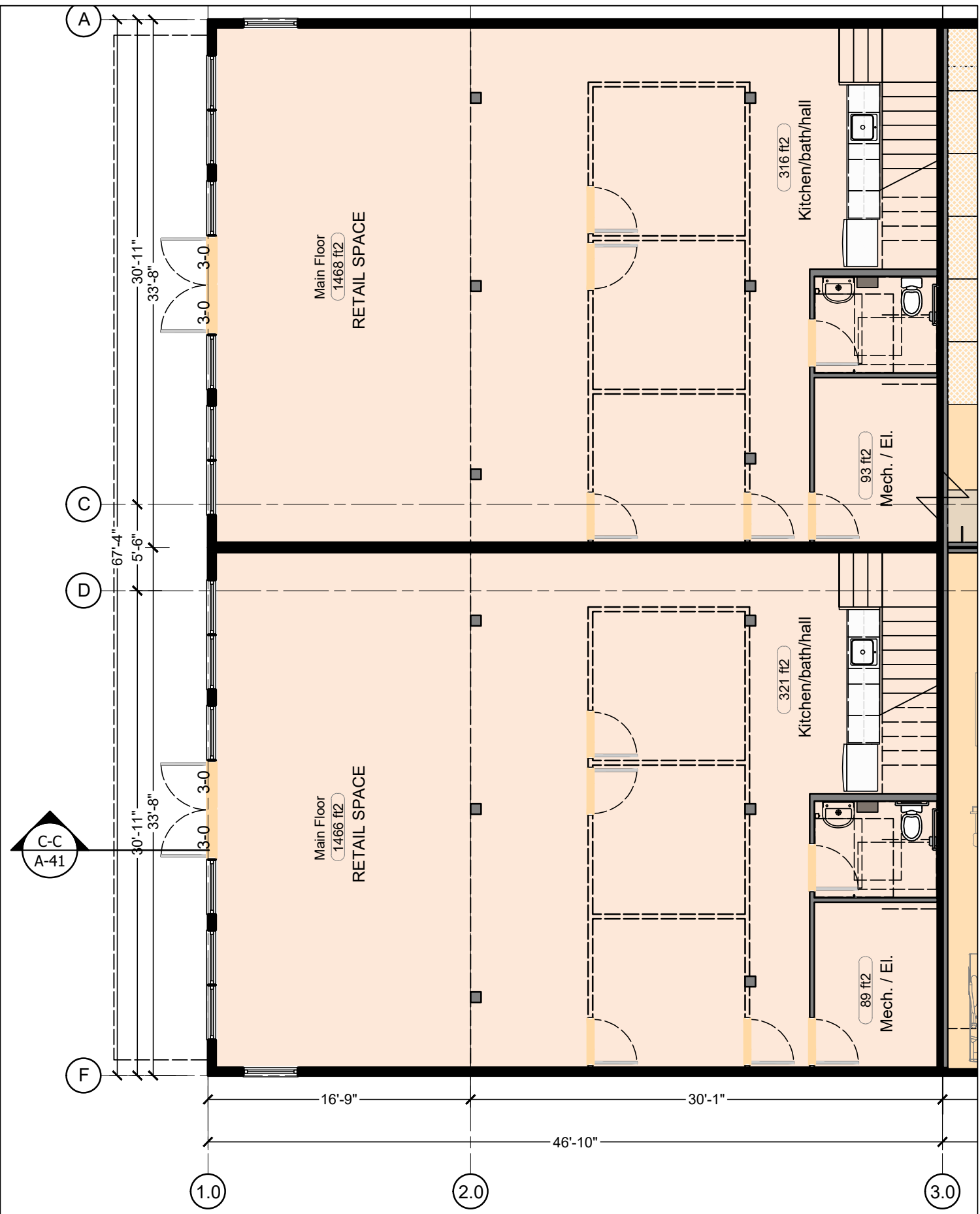
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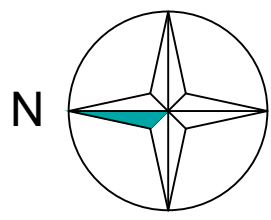
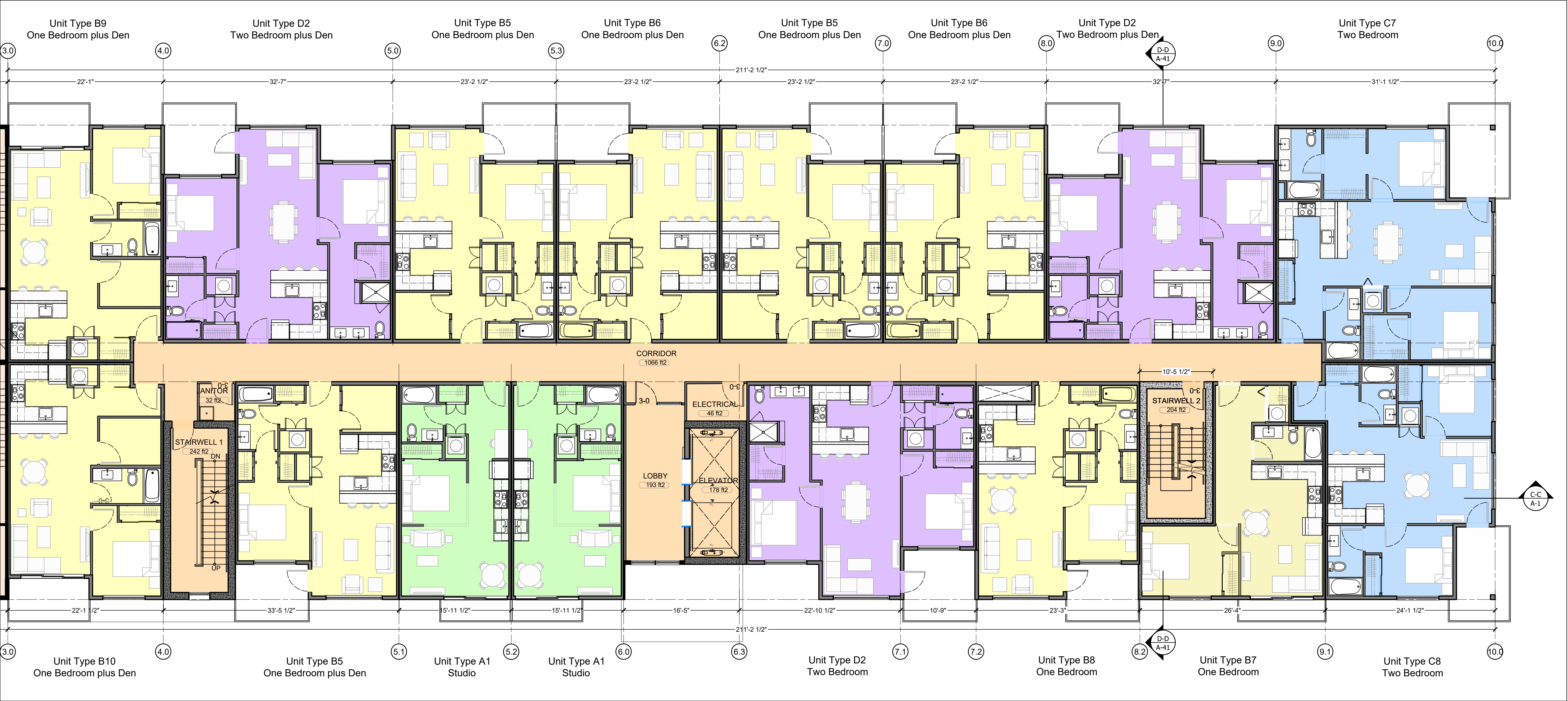
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2 BUILDING B - LIVE/WORK UNITS LEVEL 1
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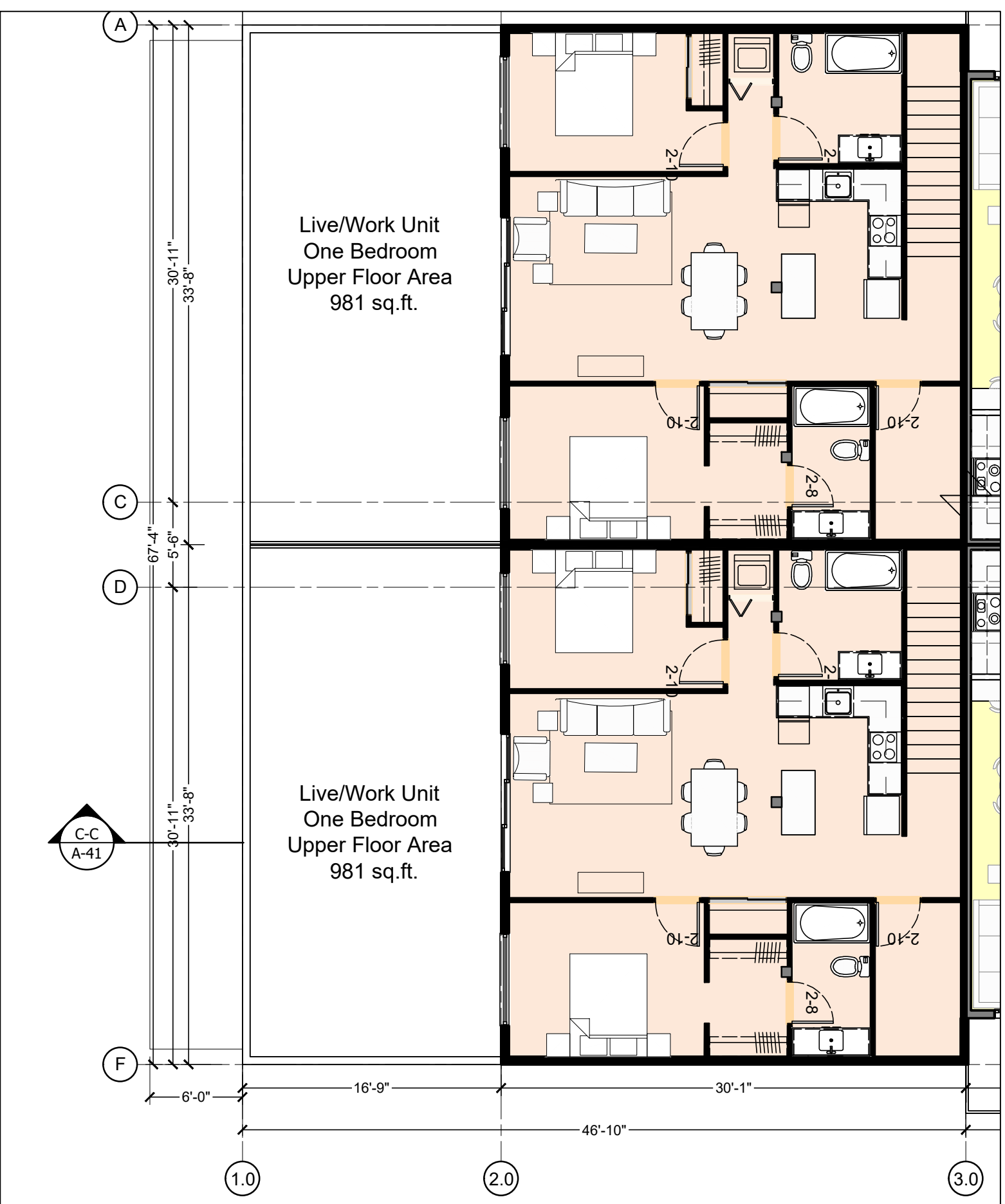
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1 BUILDING B - APARTMENTS LEVEL 2
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2 BUILDING B - LIVE/WORK UNITS - SECOND FLOOR
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Building B Level 2 Plan

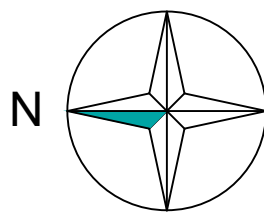
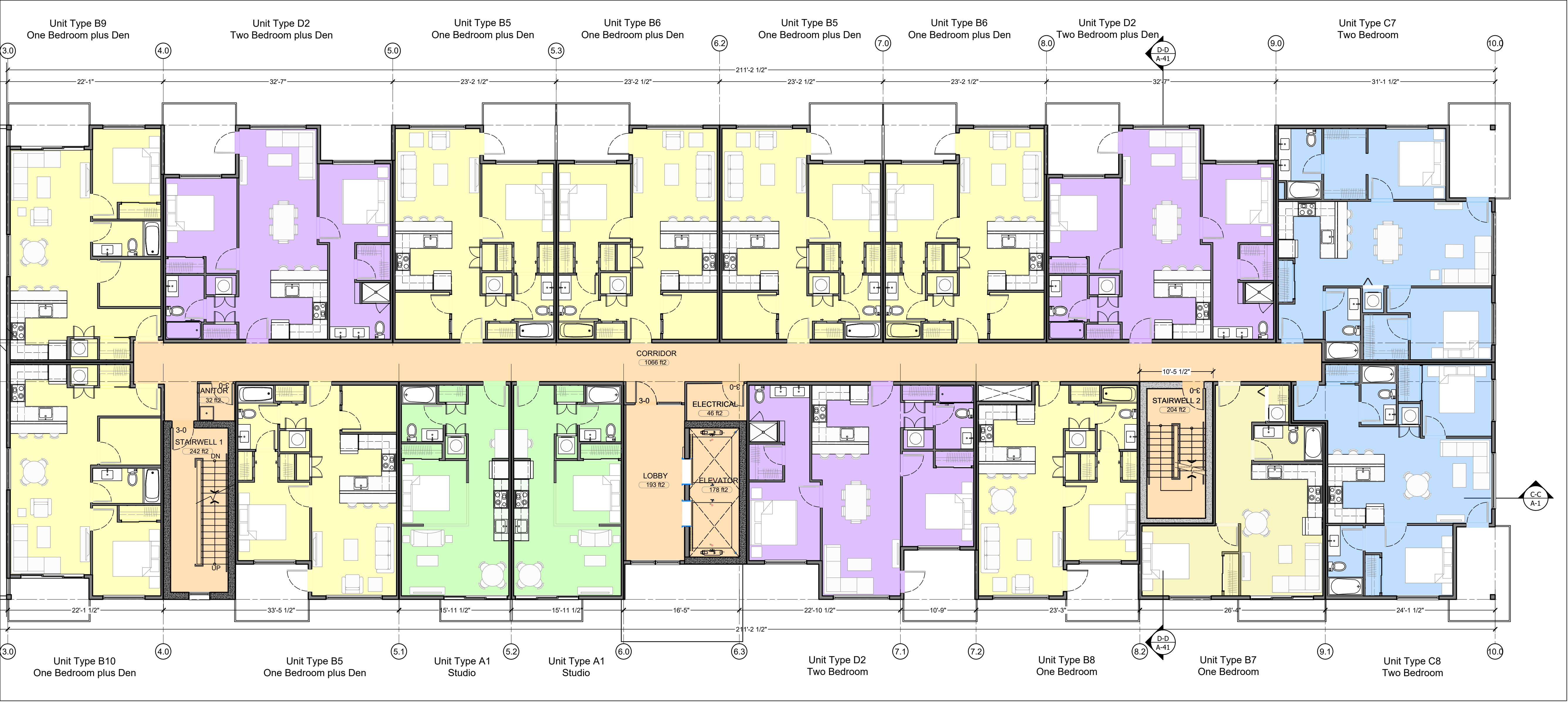
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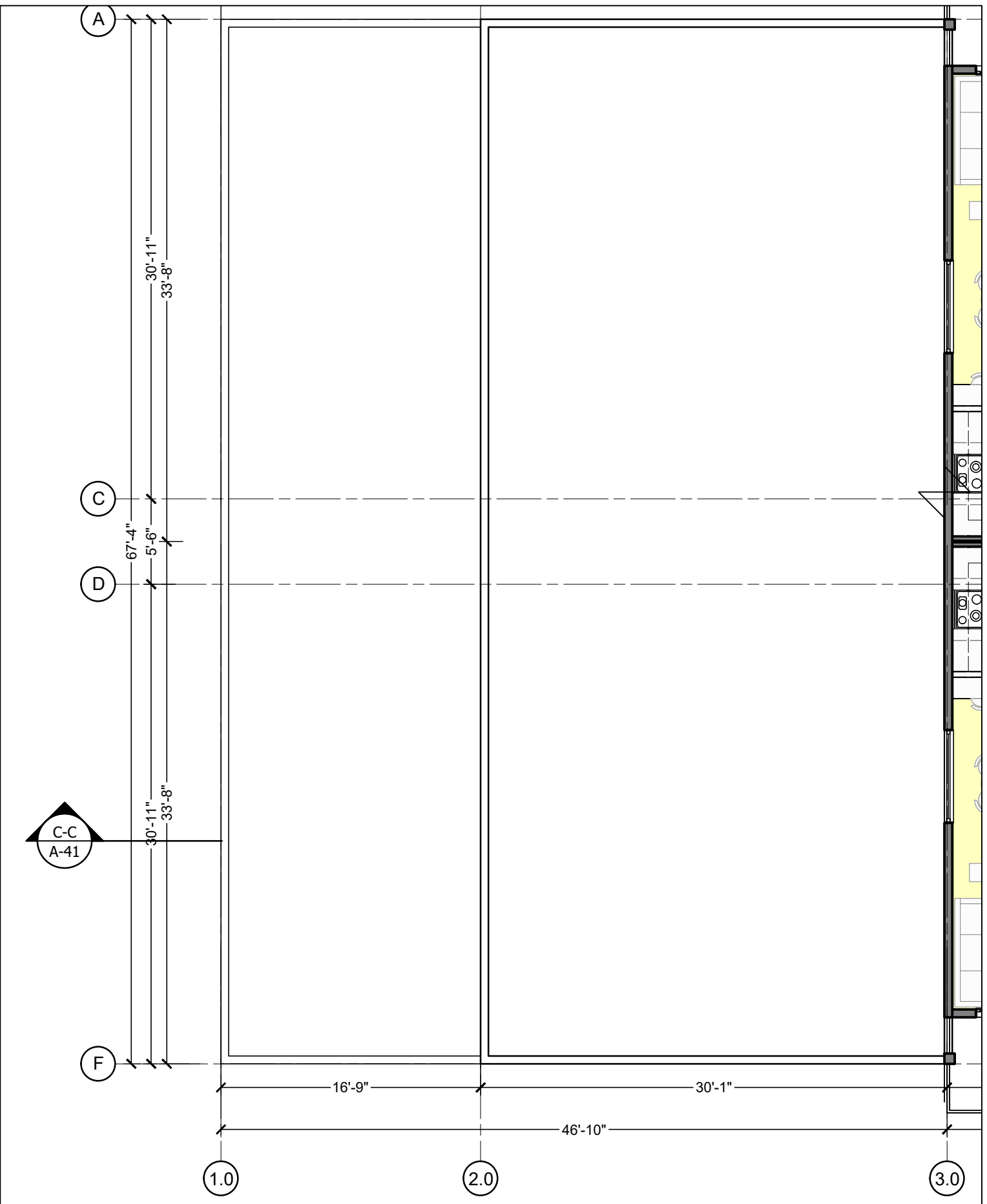
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1 BUILDING B - APARTMENTS LEVEL 3
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2 BUILDING B - LIVE/WORK UNITS ROOF
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Building B Level 3 Plan

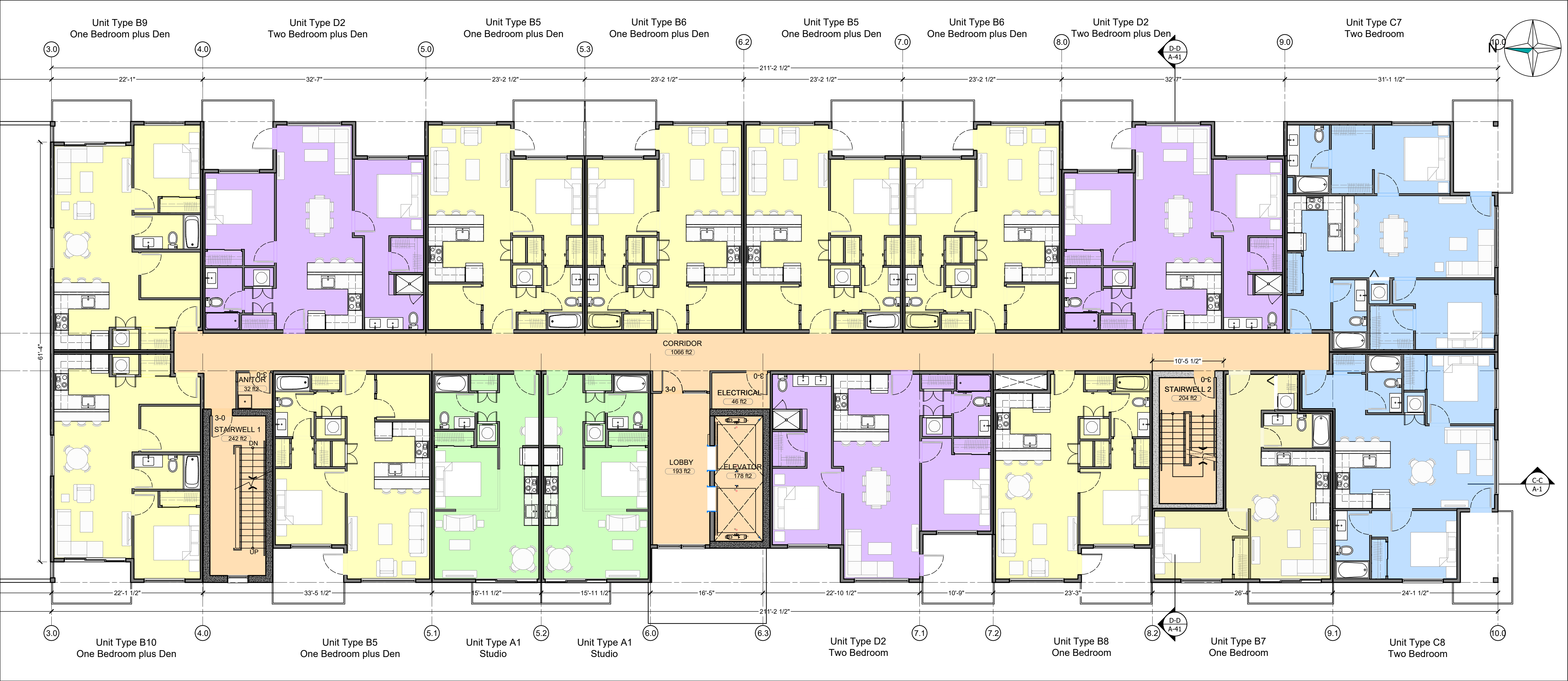
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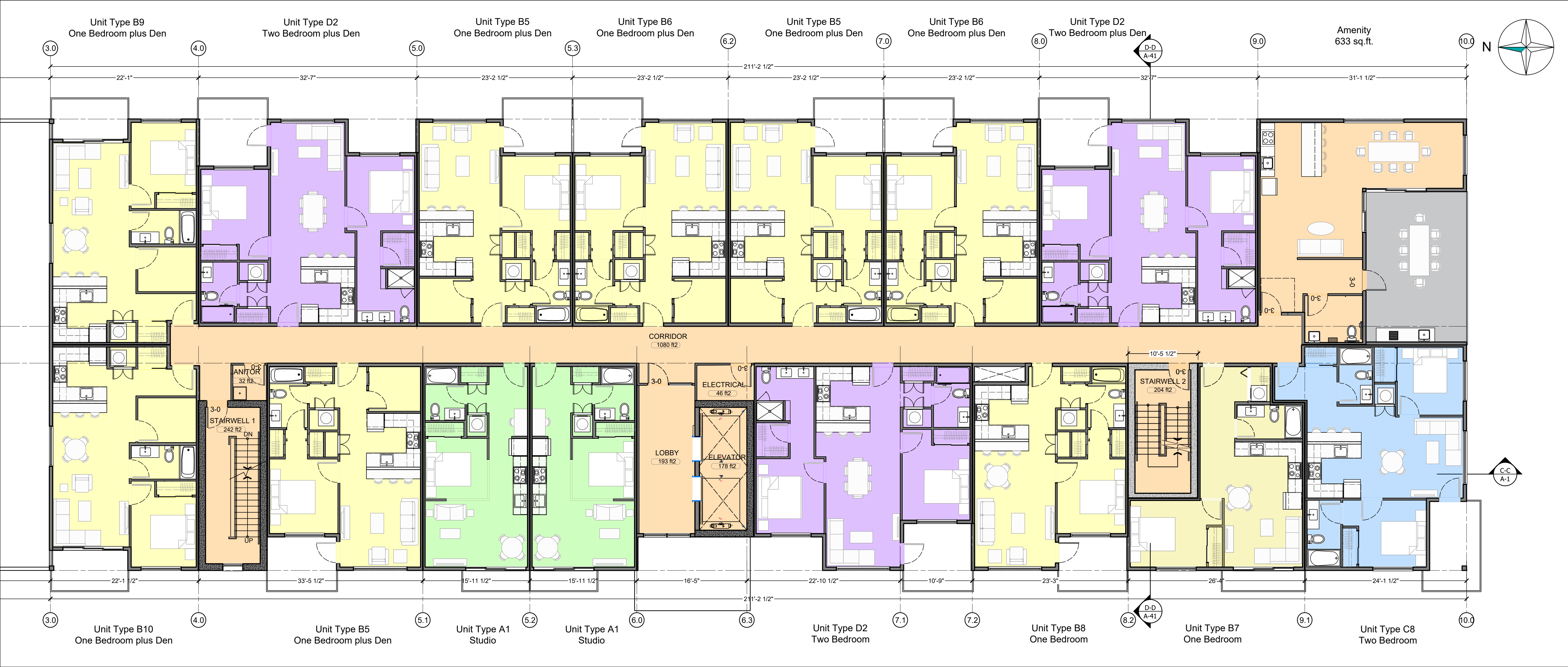
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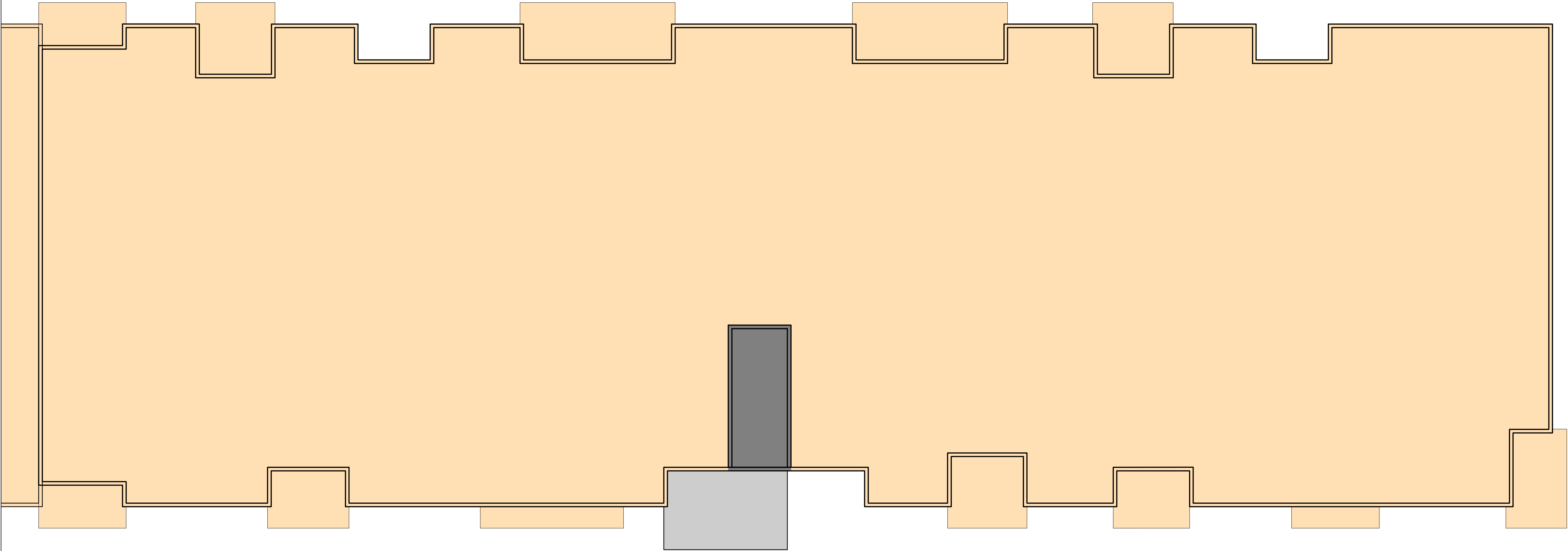
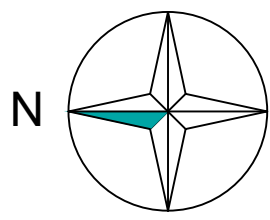
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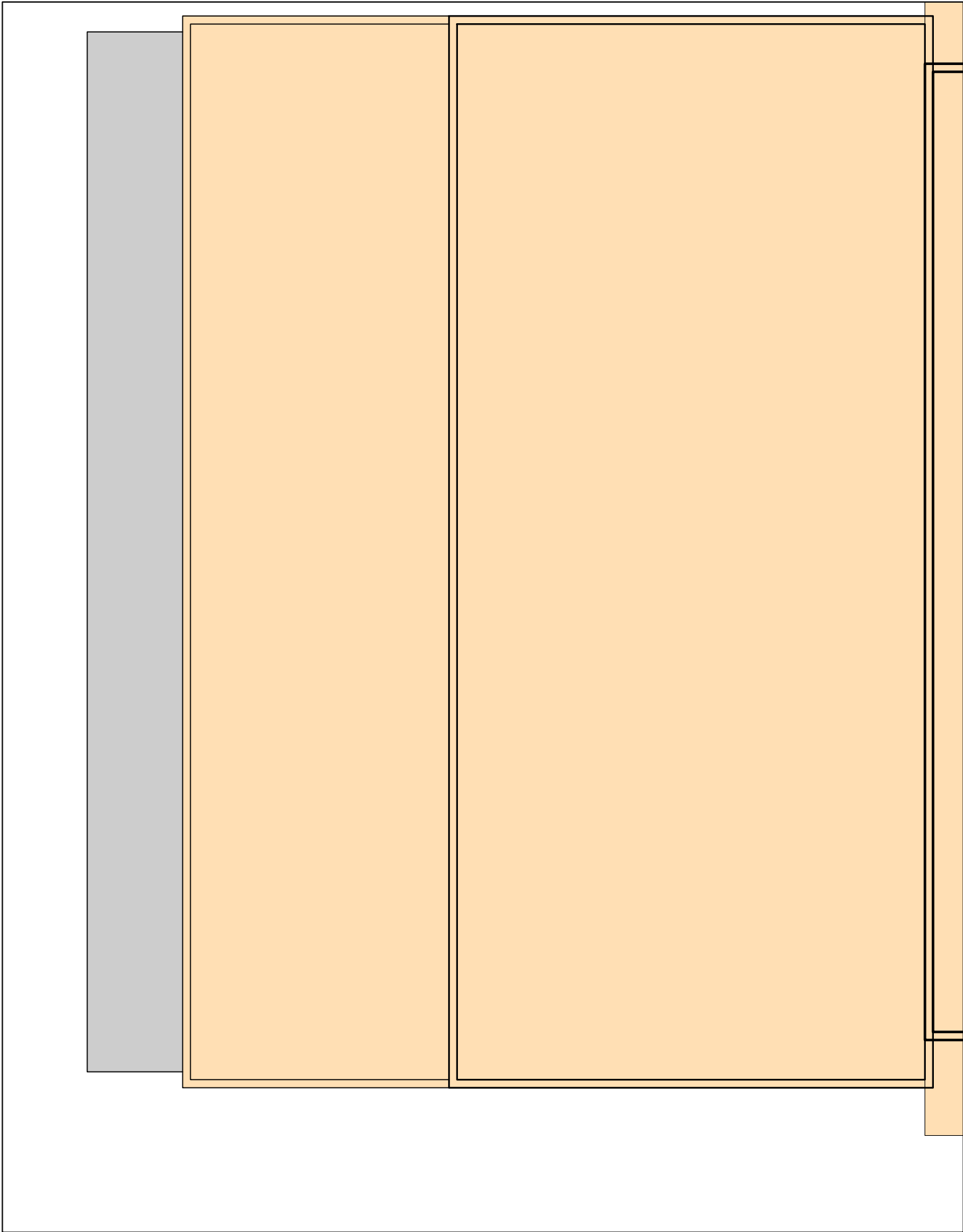


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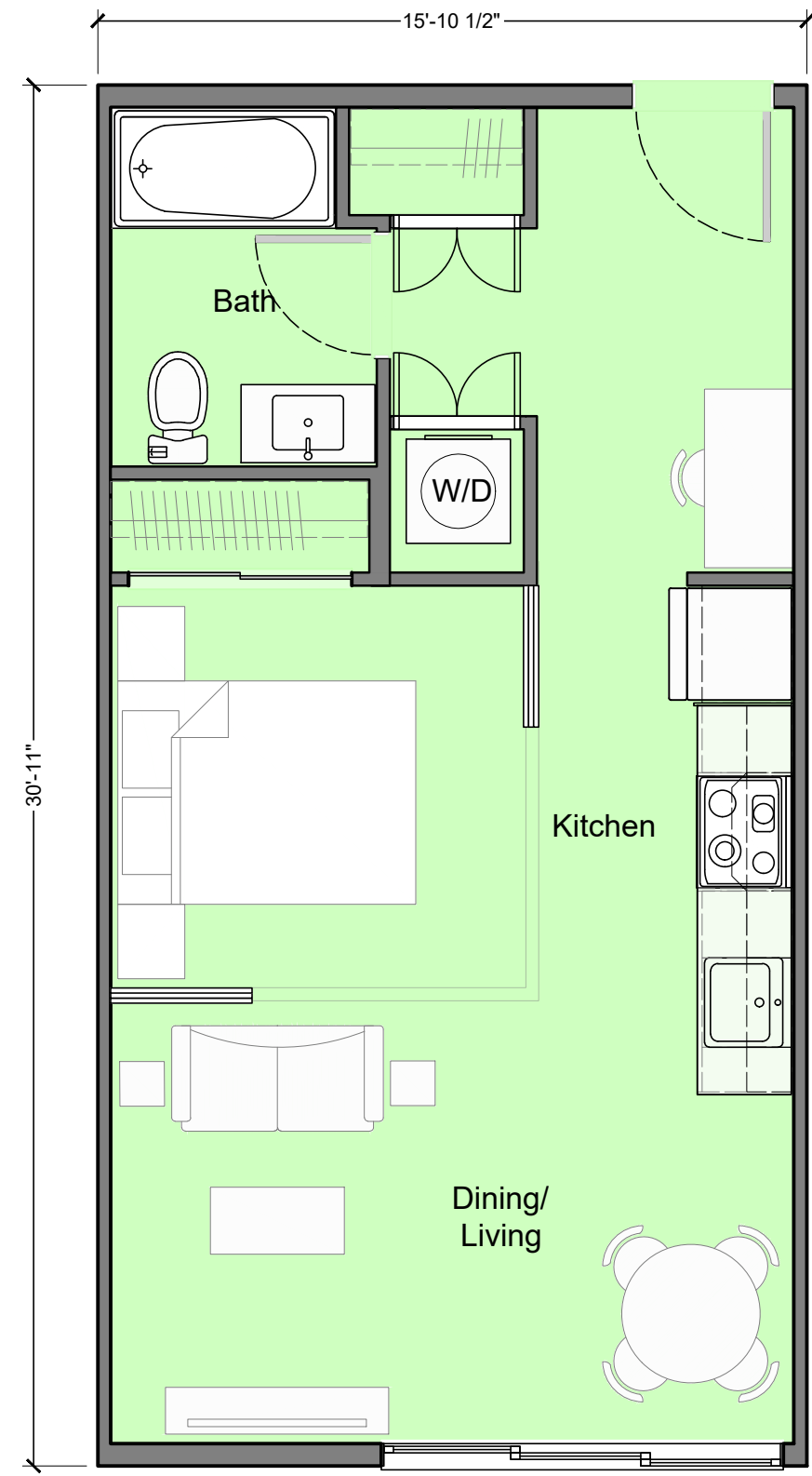


1 BUILDING B - APARTMENT ROOF PLAN
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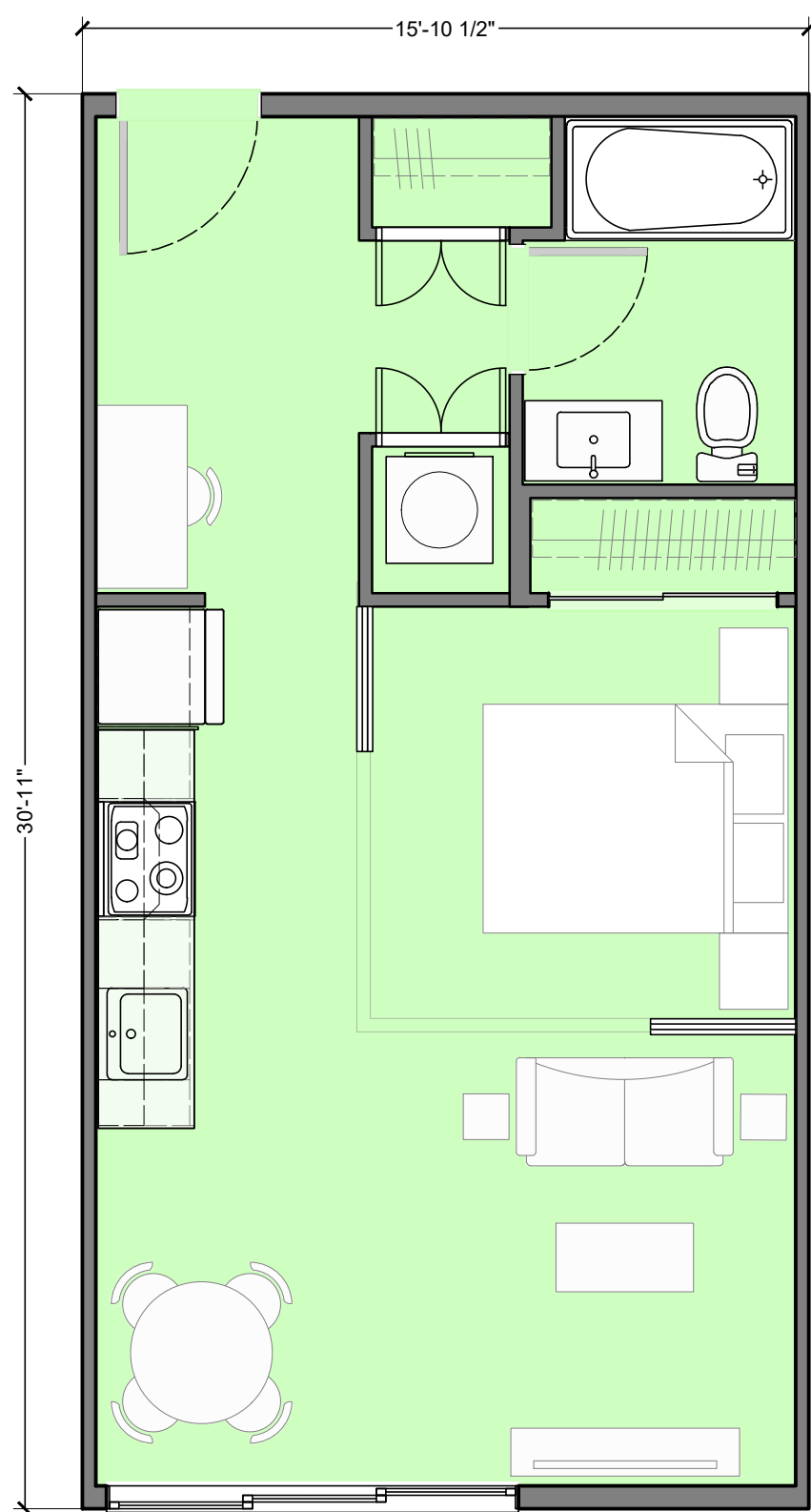


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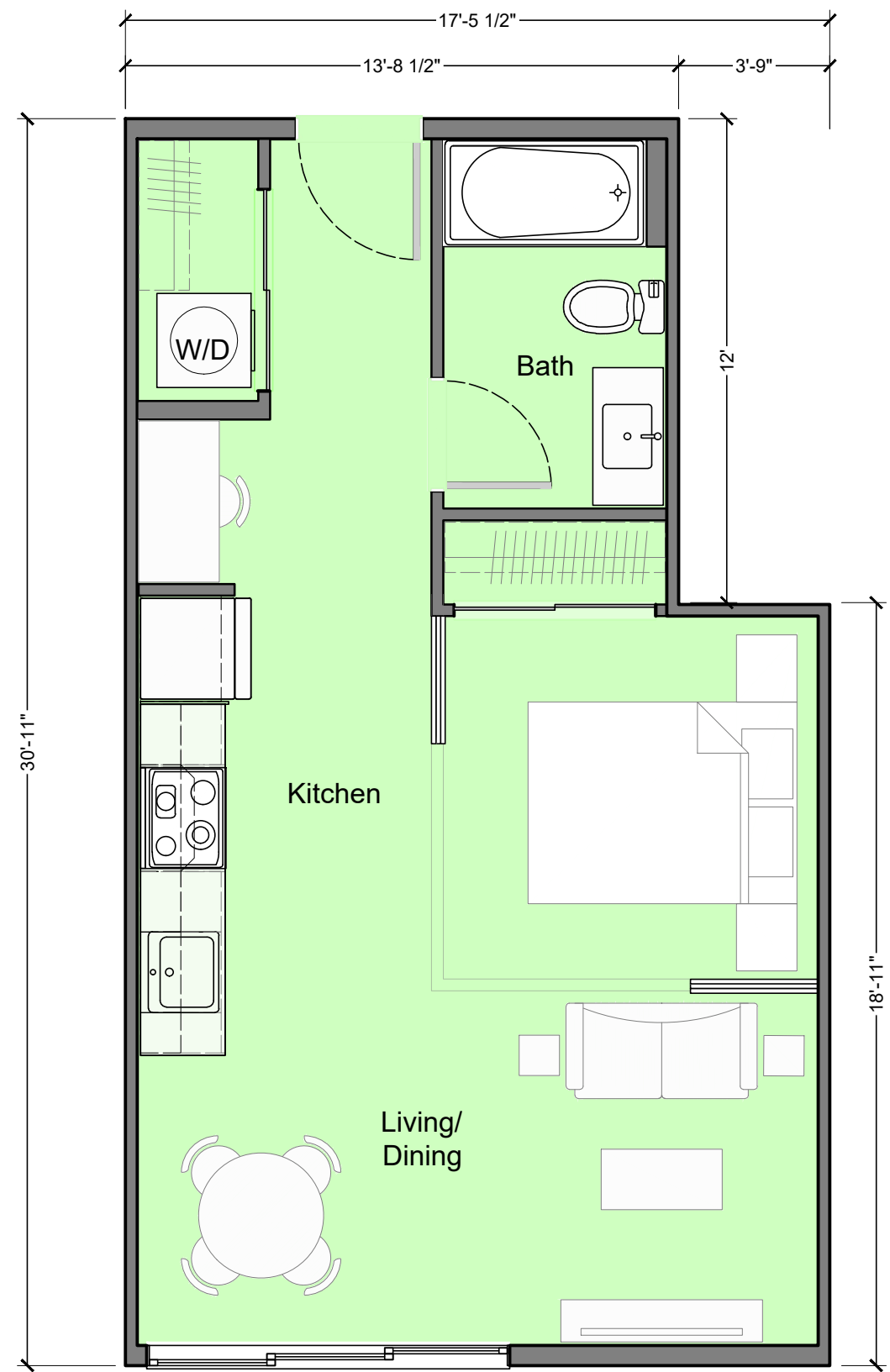
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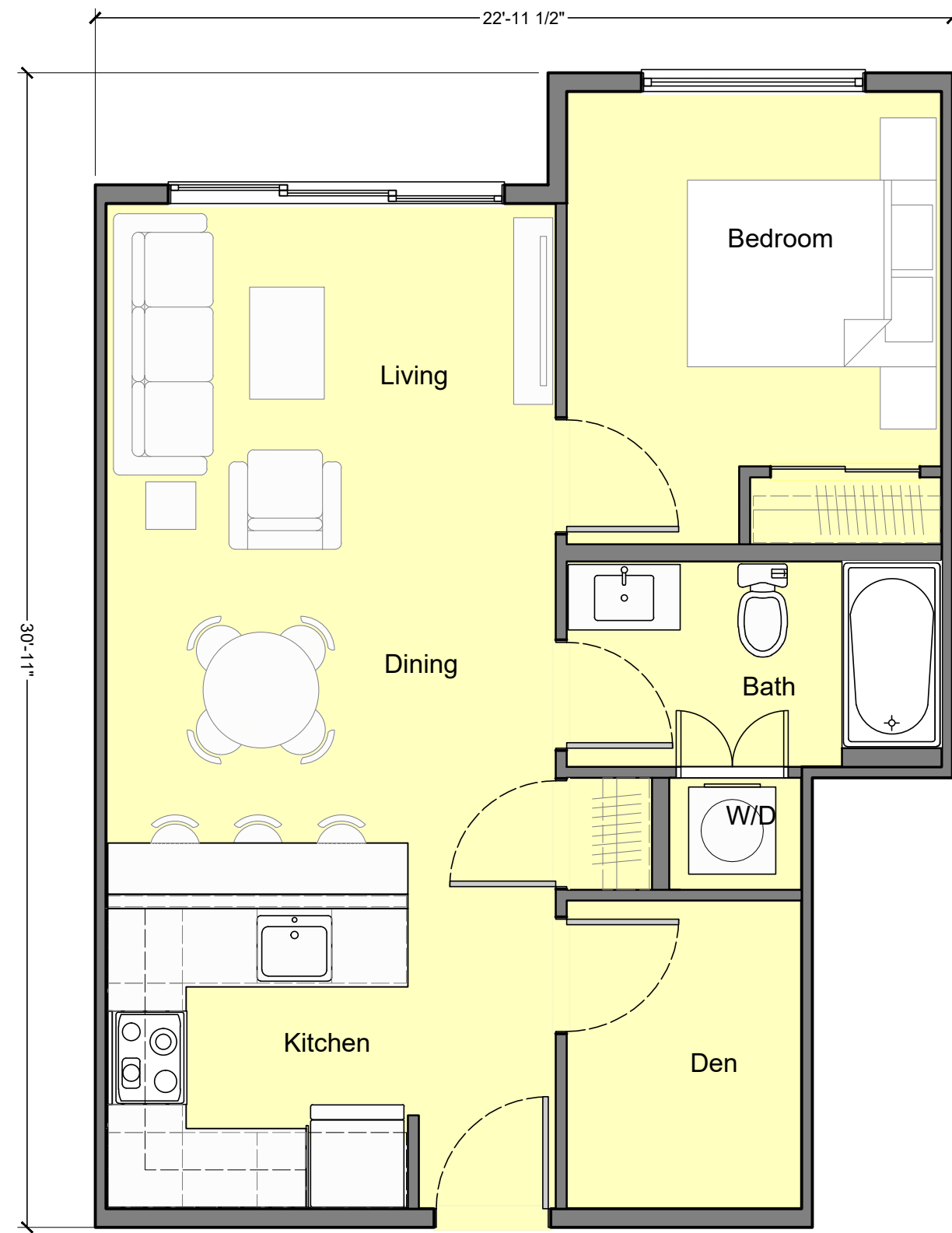
A1 Studio
481 sq.ft.



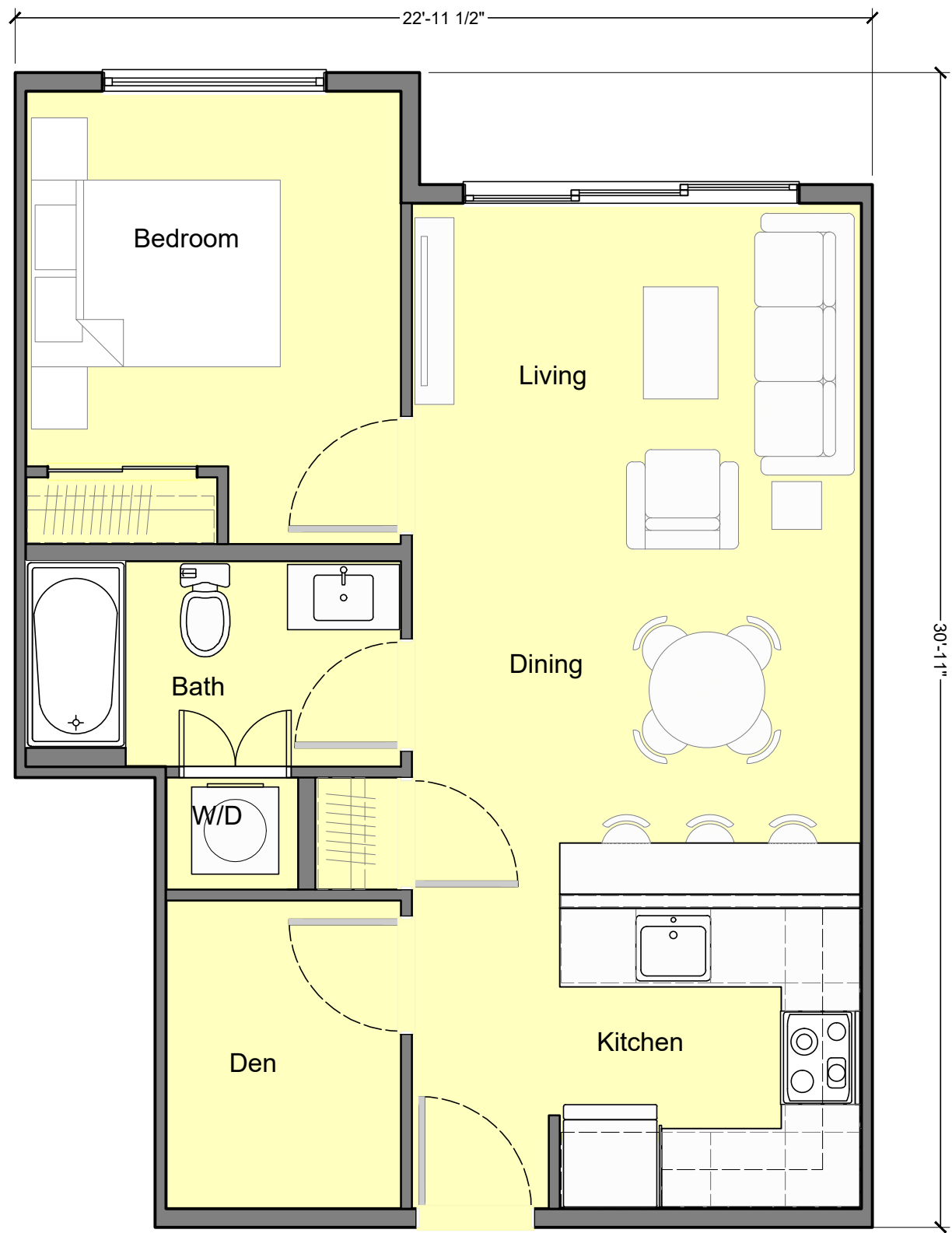
A1 Studio (mirrored)
481 sq.ft.



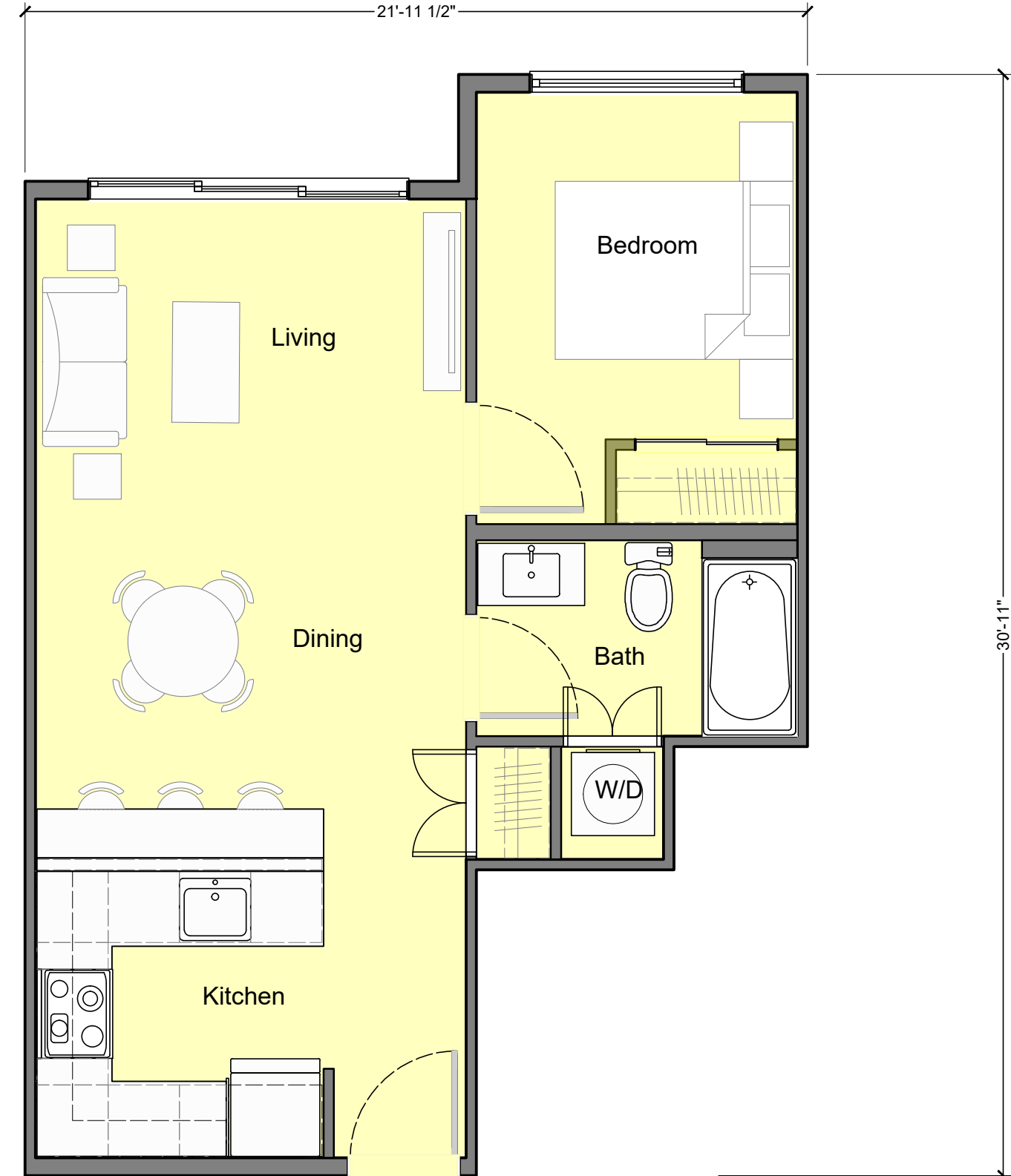
A2 Studio
485 sq.ft.



B1 One Bedroom plus Den
613 sq.ft.



B2 One Bedroom plus Den
613 sq.ft.



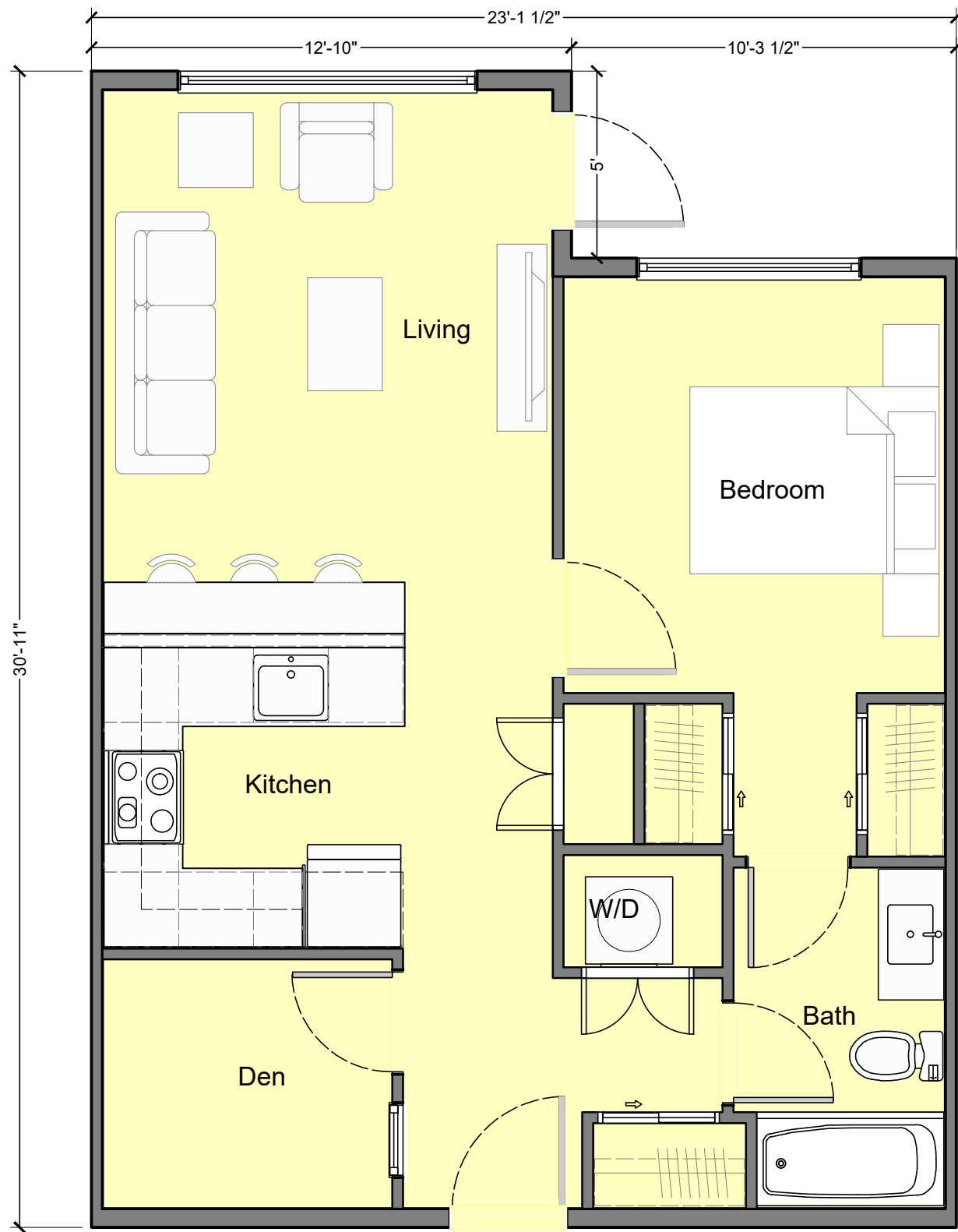
B4 One Bedroom
537 sq.ft.

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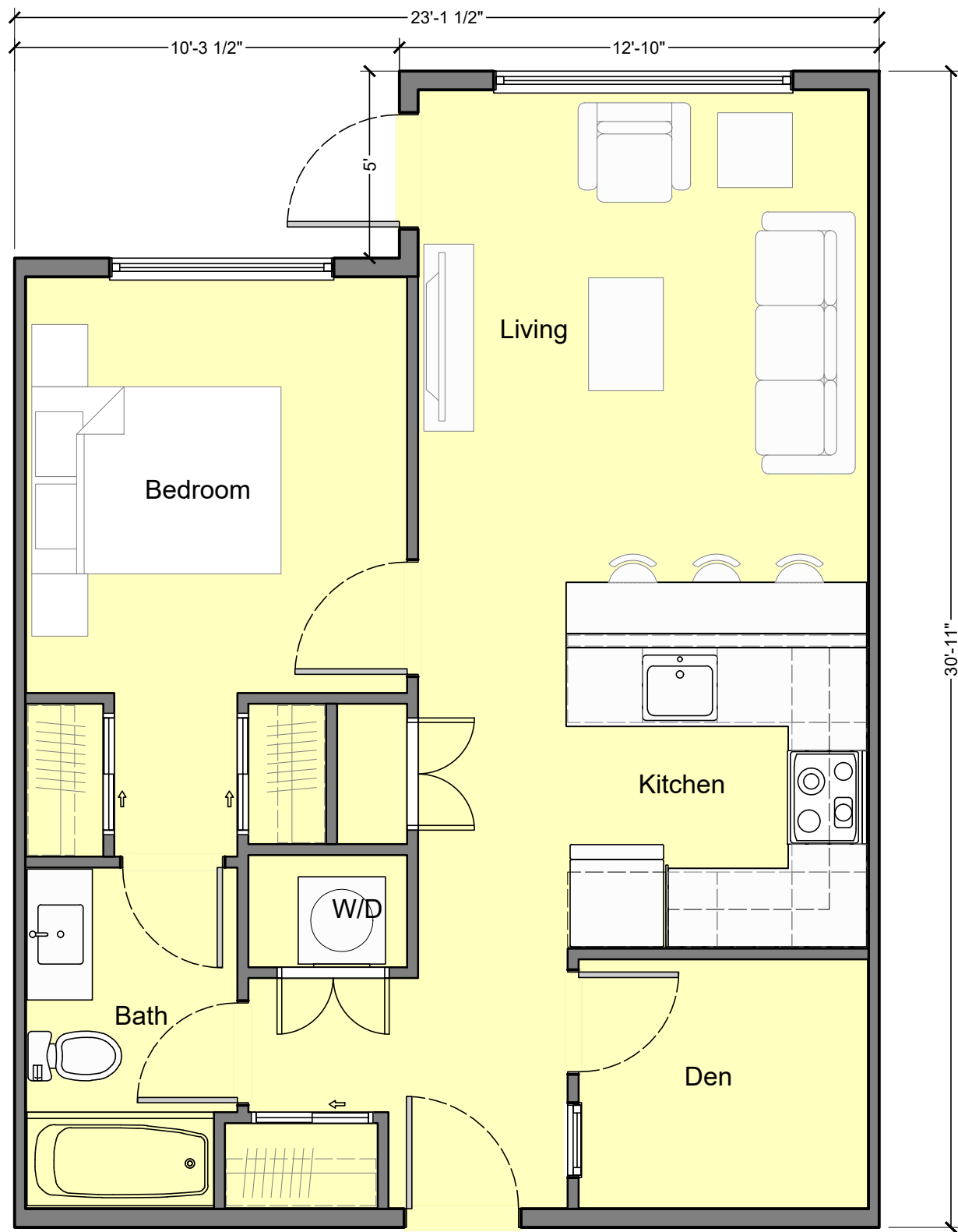
Unit Plans A and B

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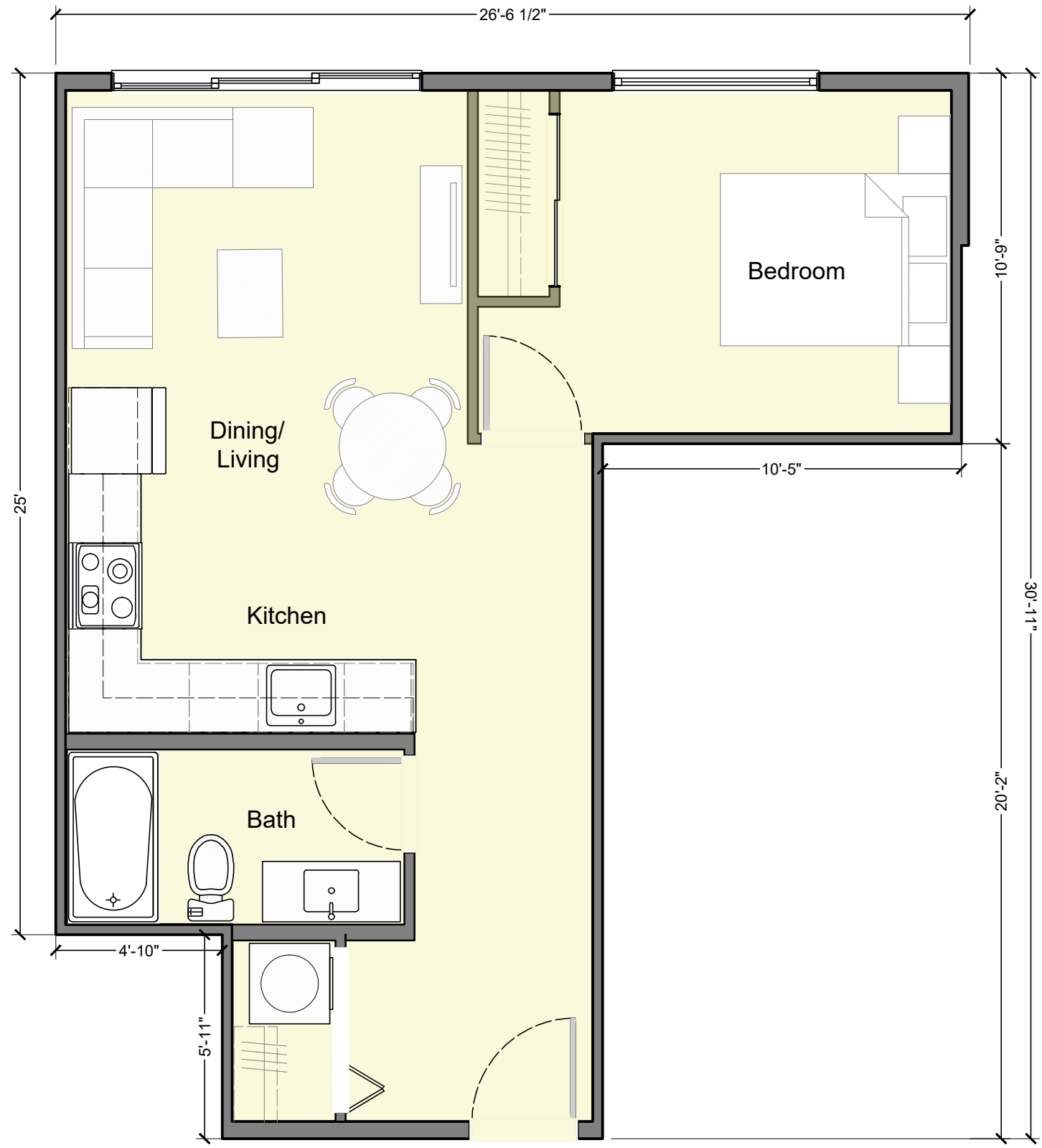
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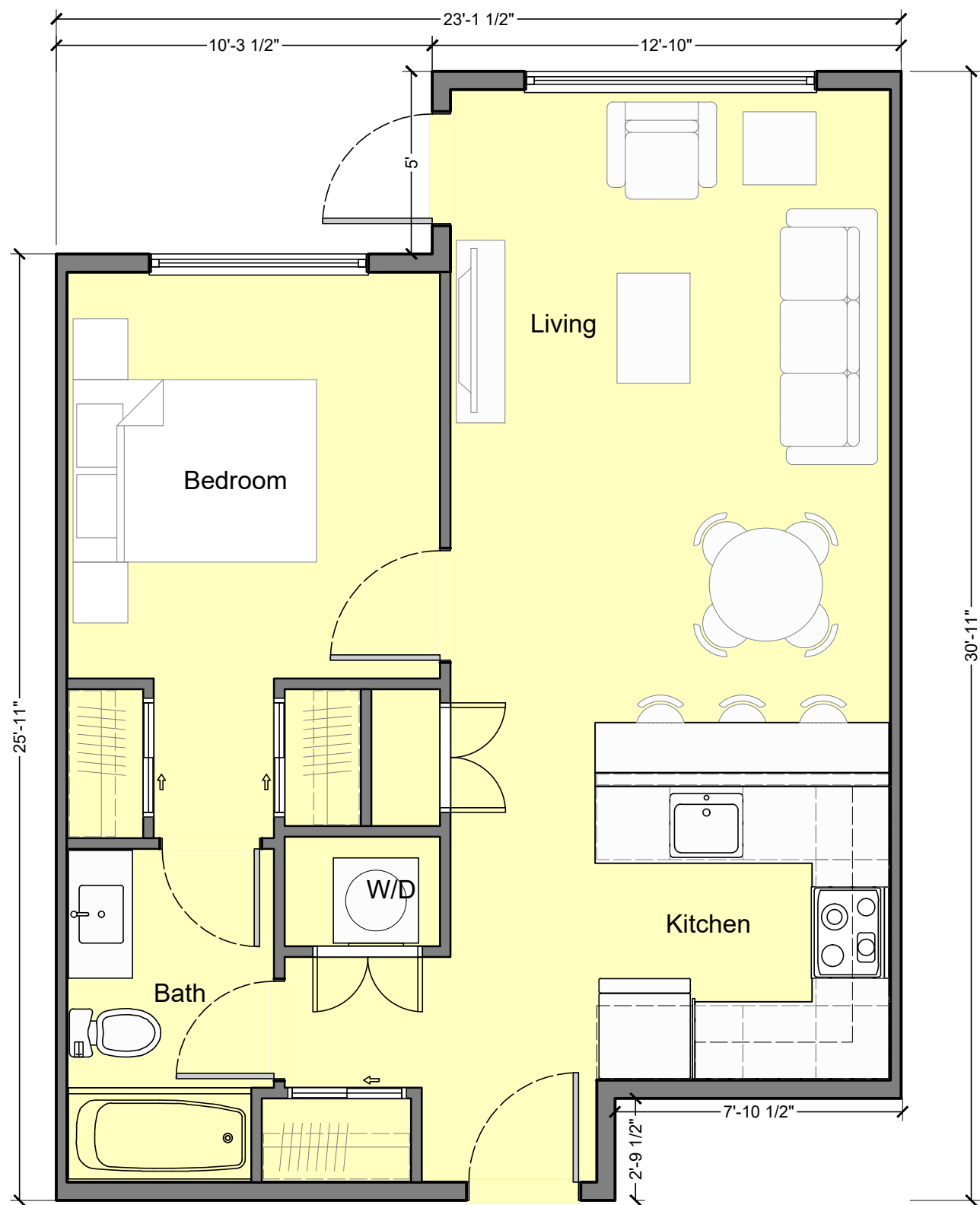
B5 One Bedroom plus Den
(646 sq.ft.)



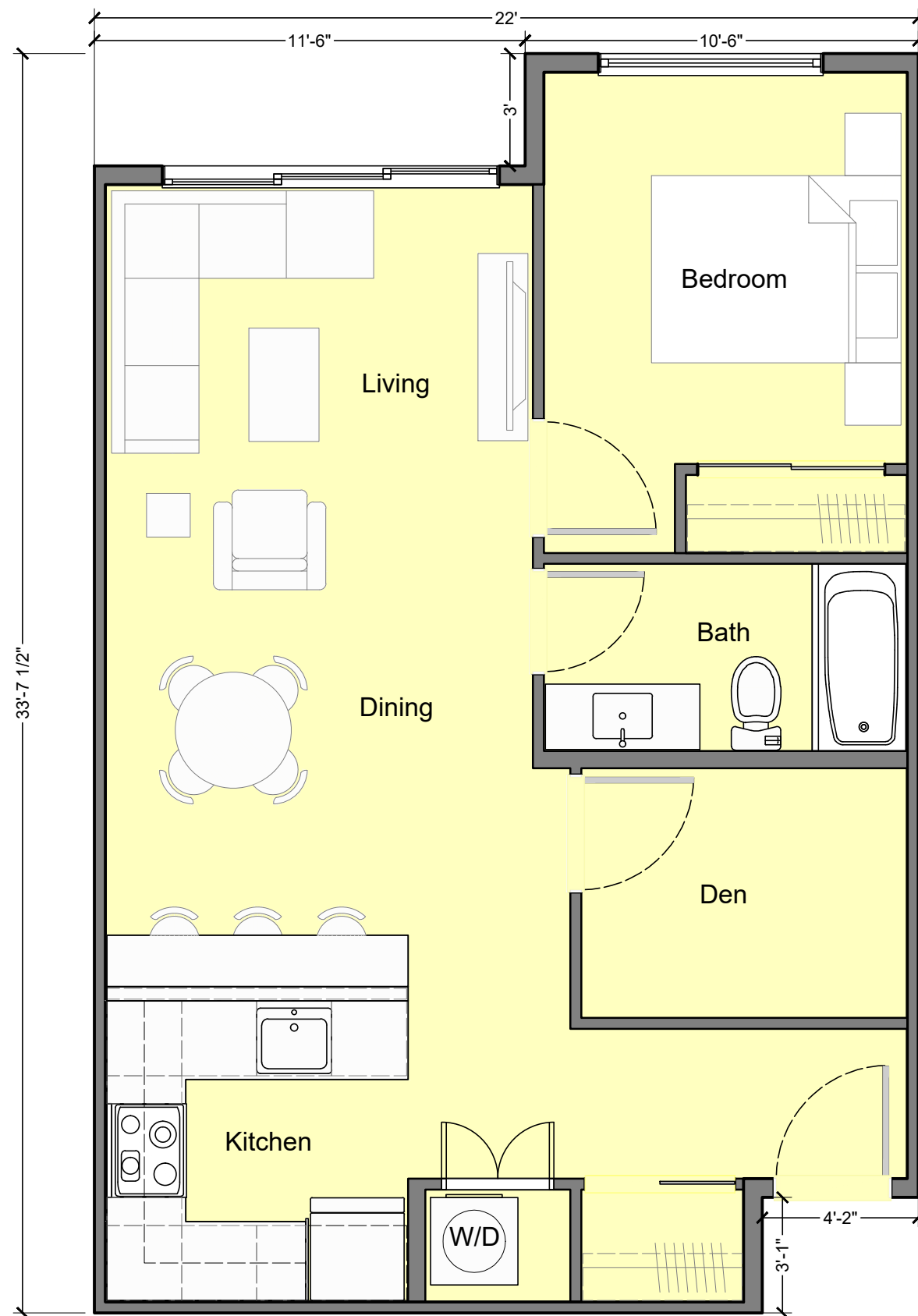
B6 One Bedroom plus Den
(646 sq.ft.)



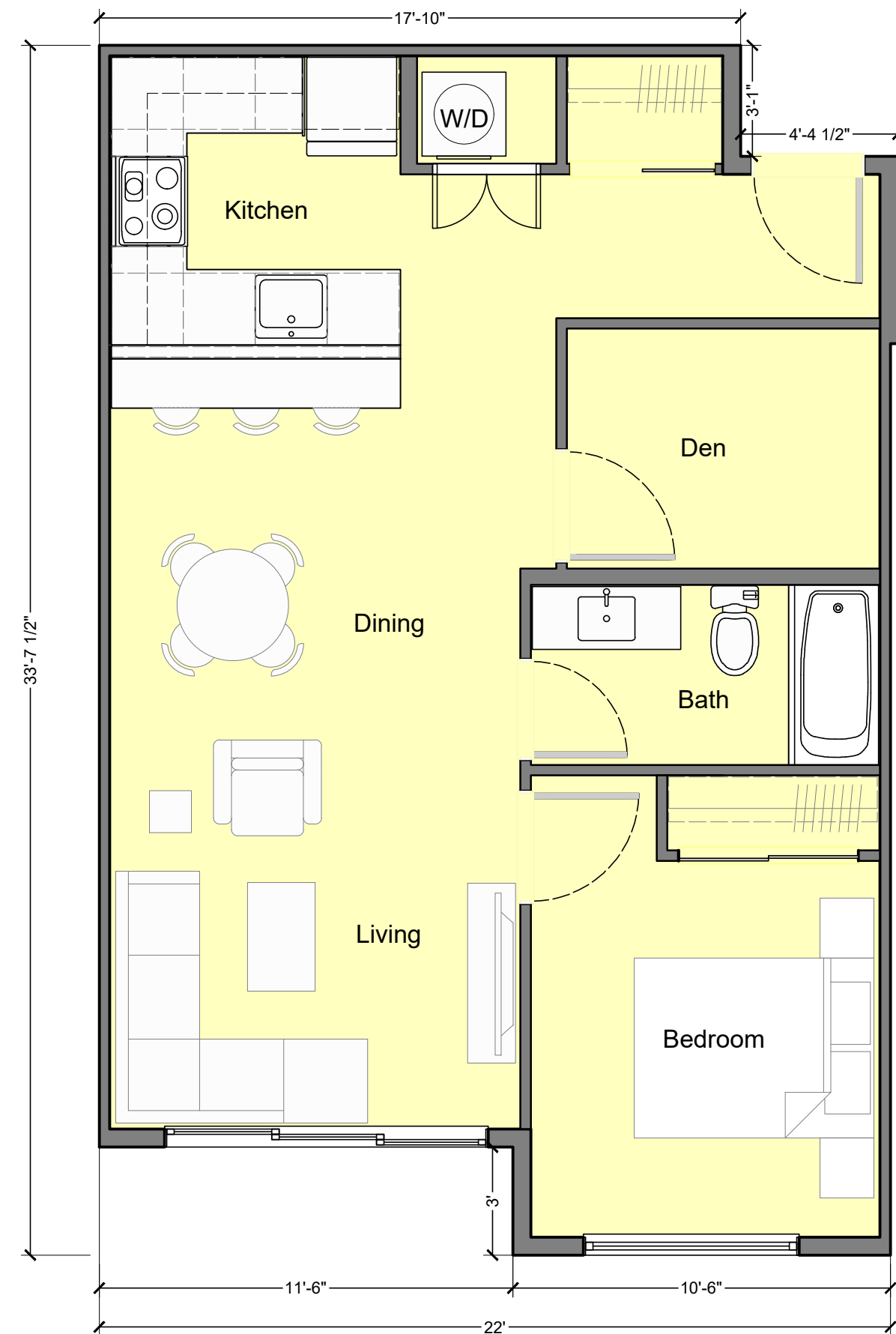
B7 One Bedroom
(556 sq.ft.)



B8 One Bedroom
(623 sq.ft.)



B9 One Bedroom plus Den
(681 sq.ft.)



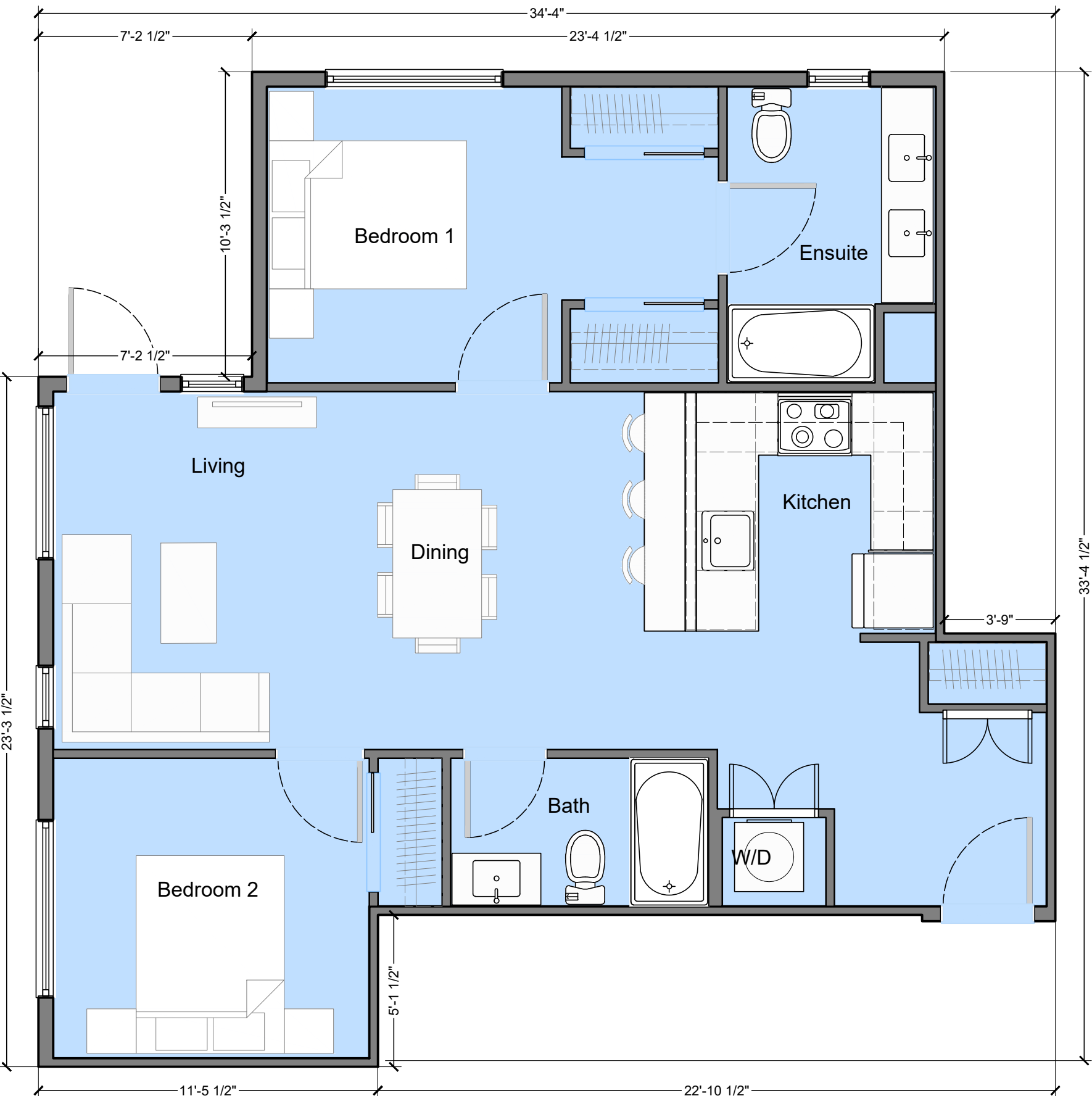
B10 One Bedroom plus Den
(681 sq.ft.)

1057
GIBSONS
WAY

Unit Plans B

Project No.:	2106
Date: September 1, 2023	Issued for: Development Permit
Drawn: CB	
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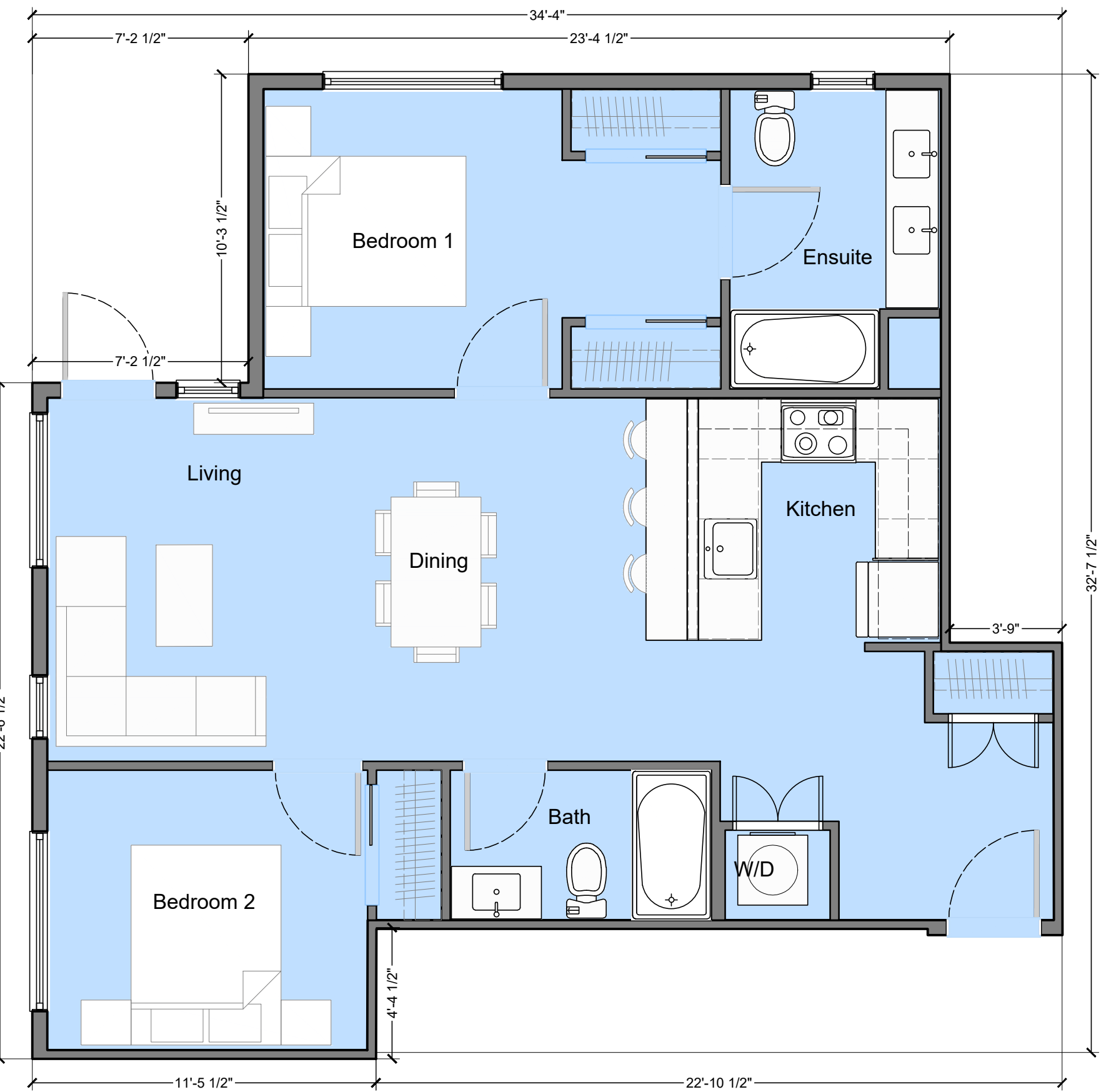
C1 Two Bedroom
(861 sq.ft.)



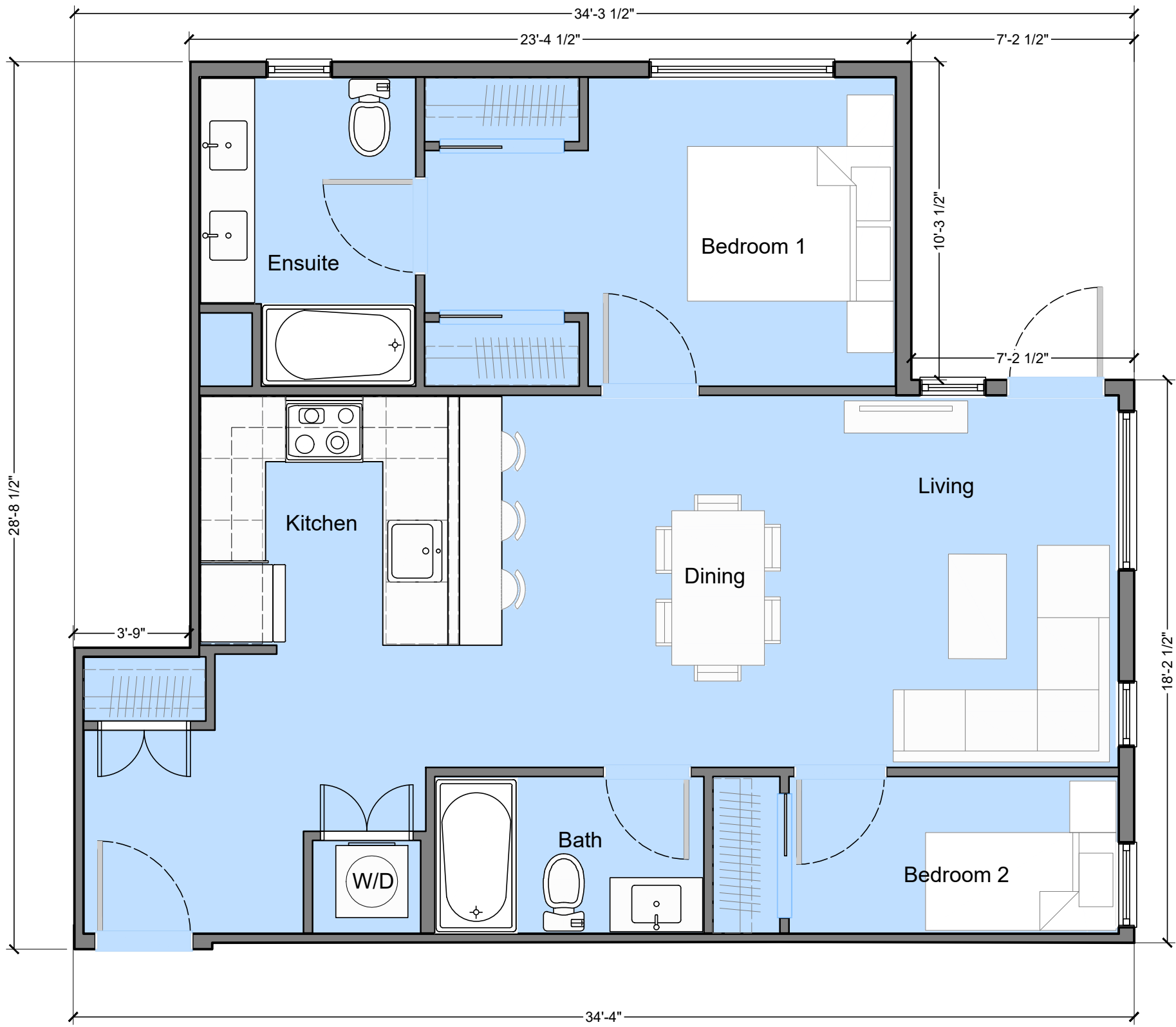
C2 Two Bedroom
(861 sq.ft.)



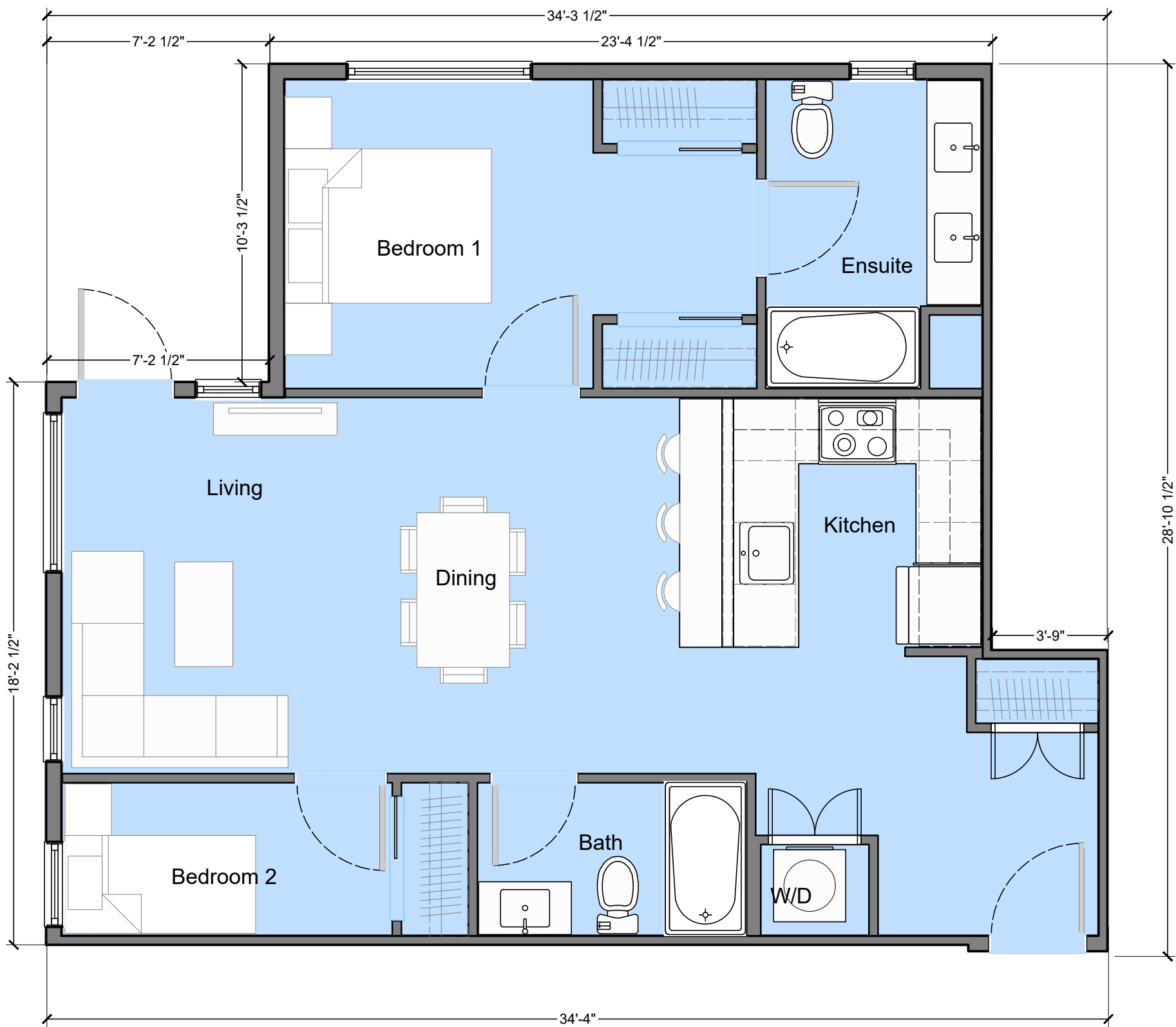
C2.2 Two Bedroom
(854 sq.ft.)



C2.3 Two Bedroom
(856 sq.ft.)



C4 Two Bedroom
(807 sq.ft.)



C4.1 Two Bedroom
(806 sq.ft.)

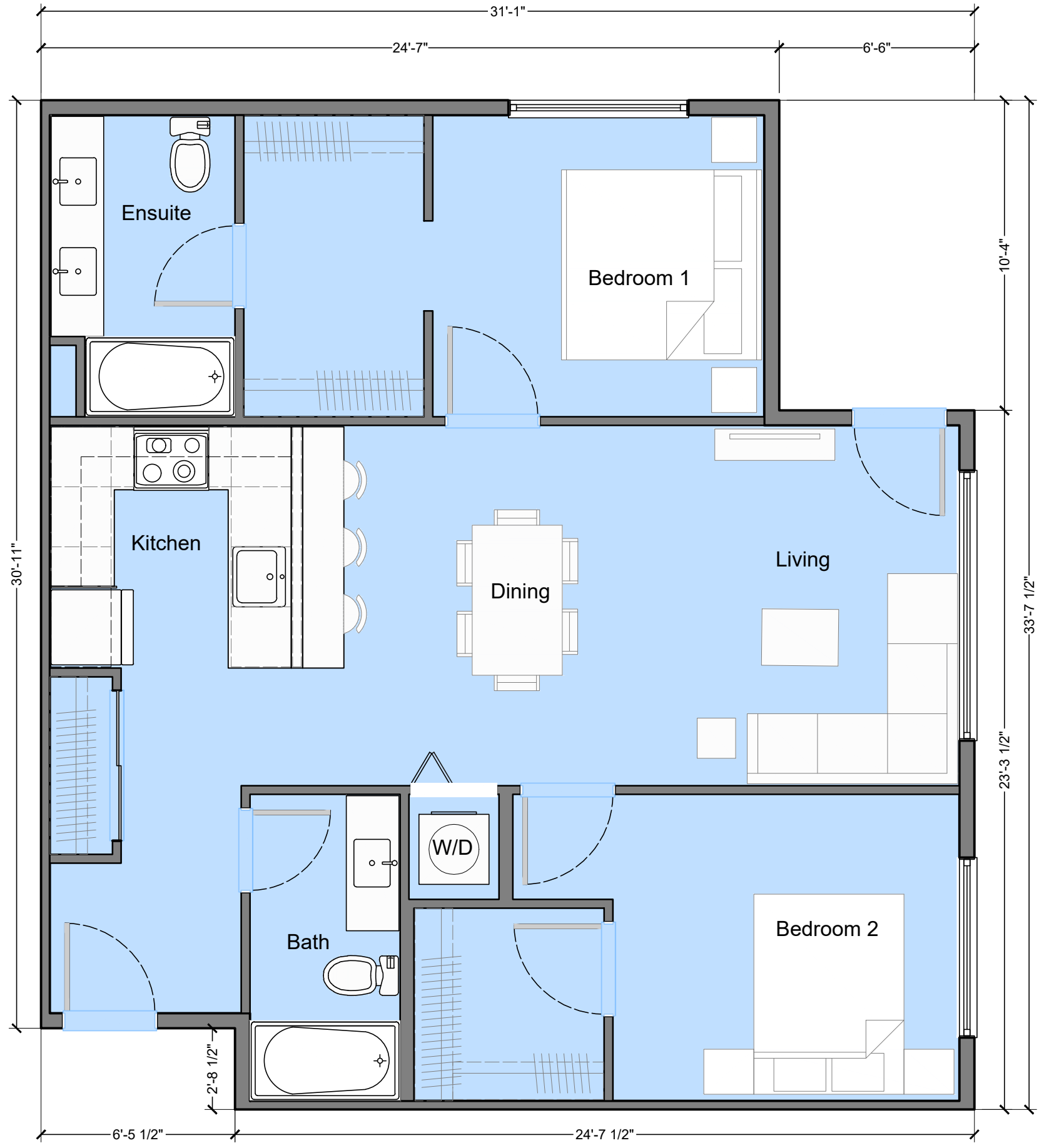
1057
GIBSONS
WAY



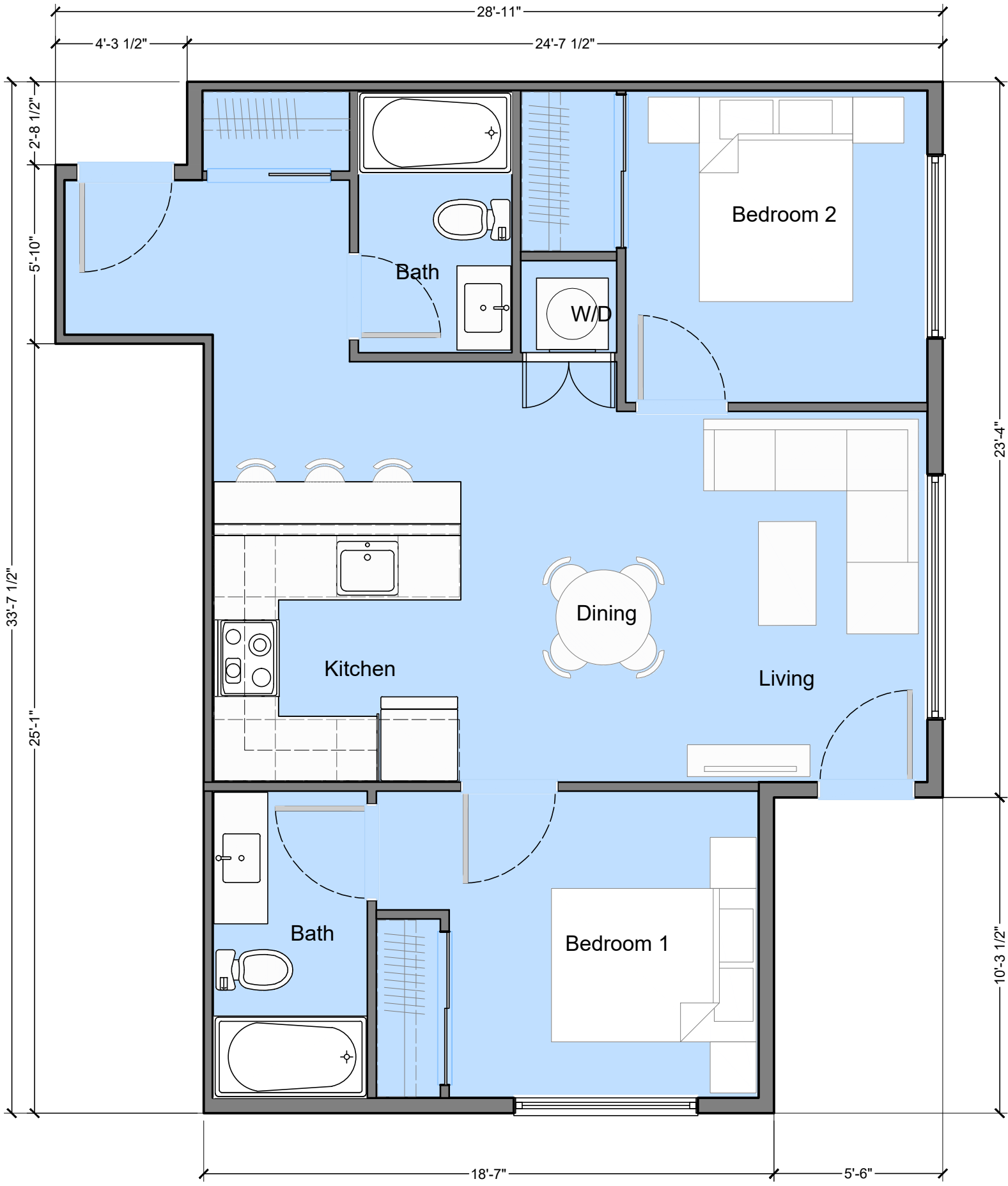
C5 Two Bedroom
815 sq.ft.



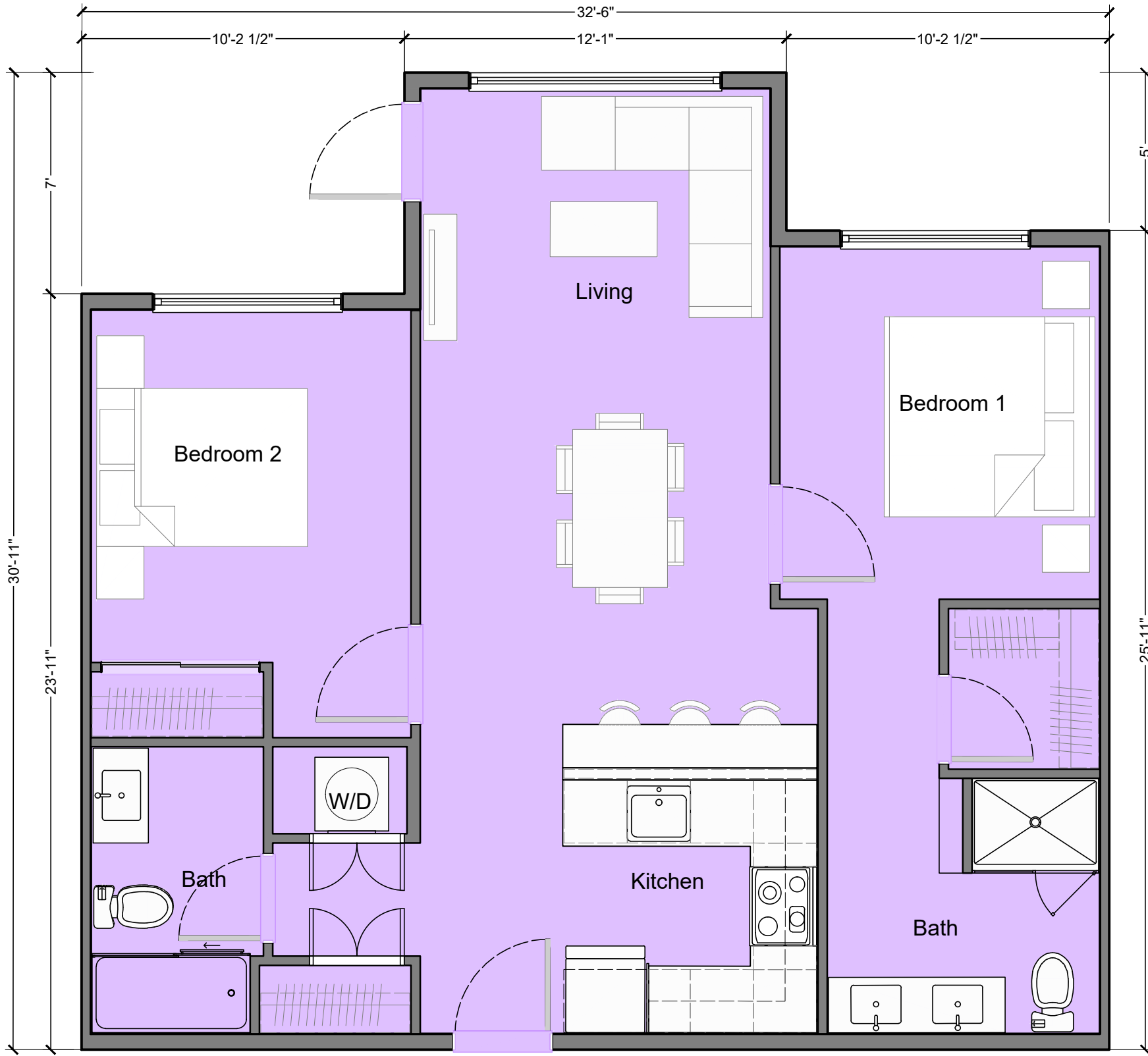
C6 Two Bedroom
835 sq.ft.



C7 Two Bedroom
928 sq.ft.

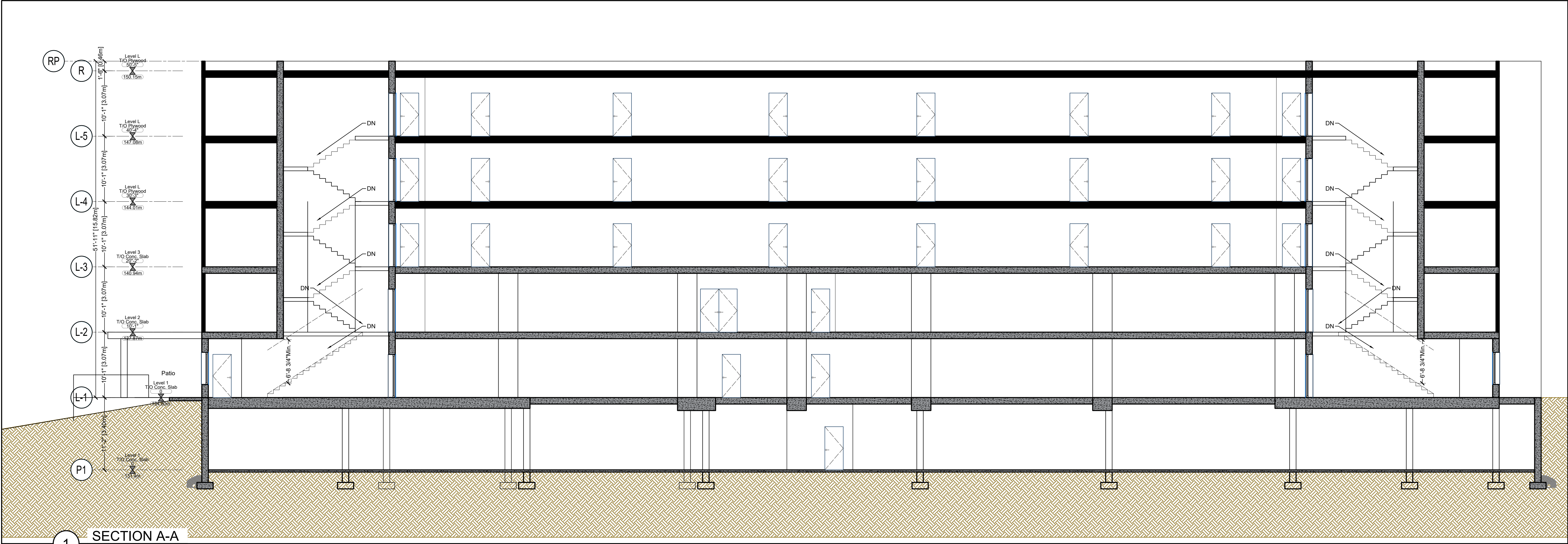


C8 Two Bedroom
755 sq.ft.

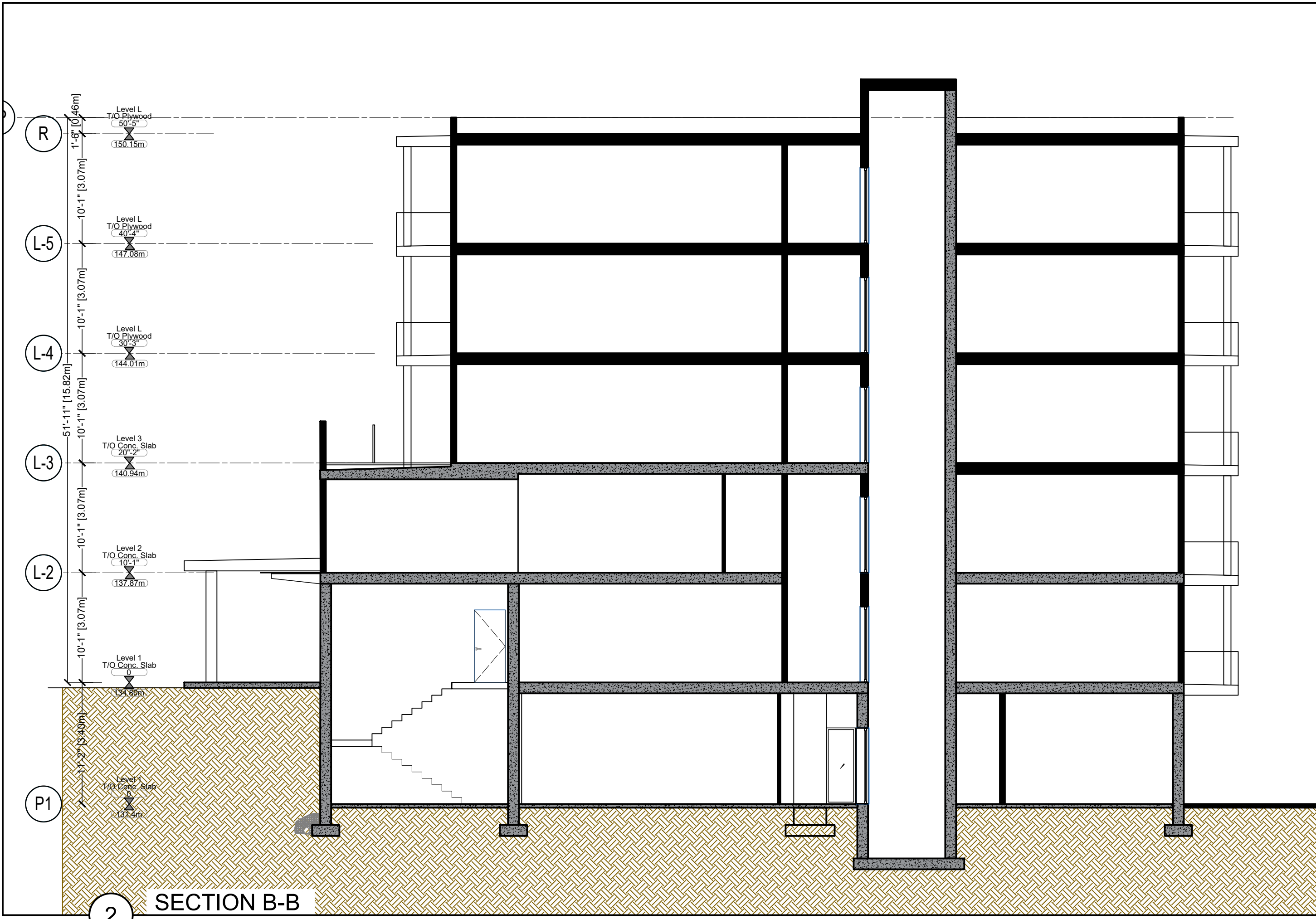


D2 Two Bedroom
854 sq.ft.

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WAY



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WAY



Building A - Section A & B

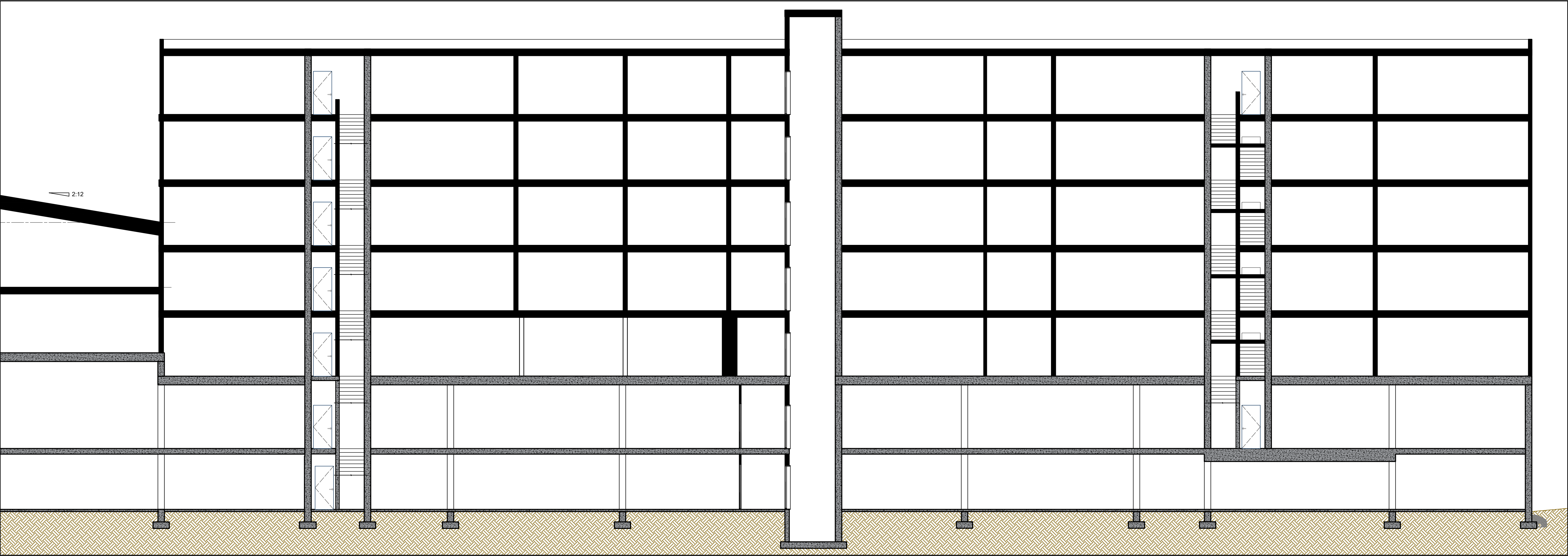
Project No.: 2106

Date: September 1, 2023 Issued for: Development Permit

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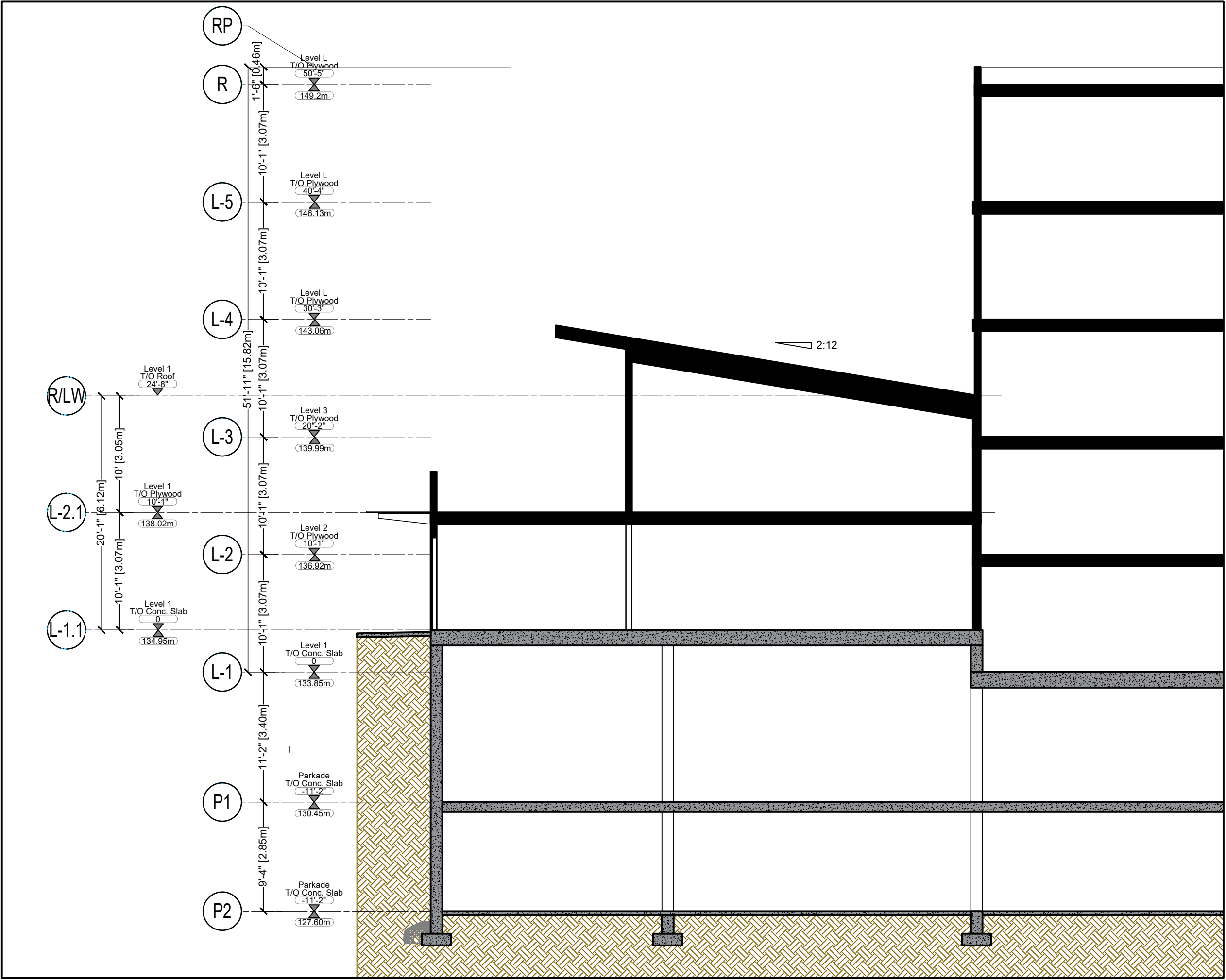
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A-40



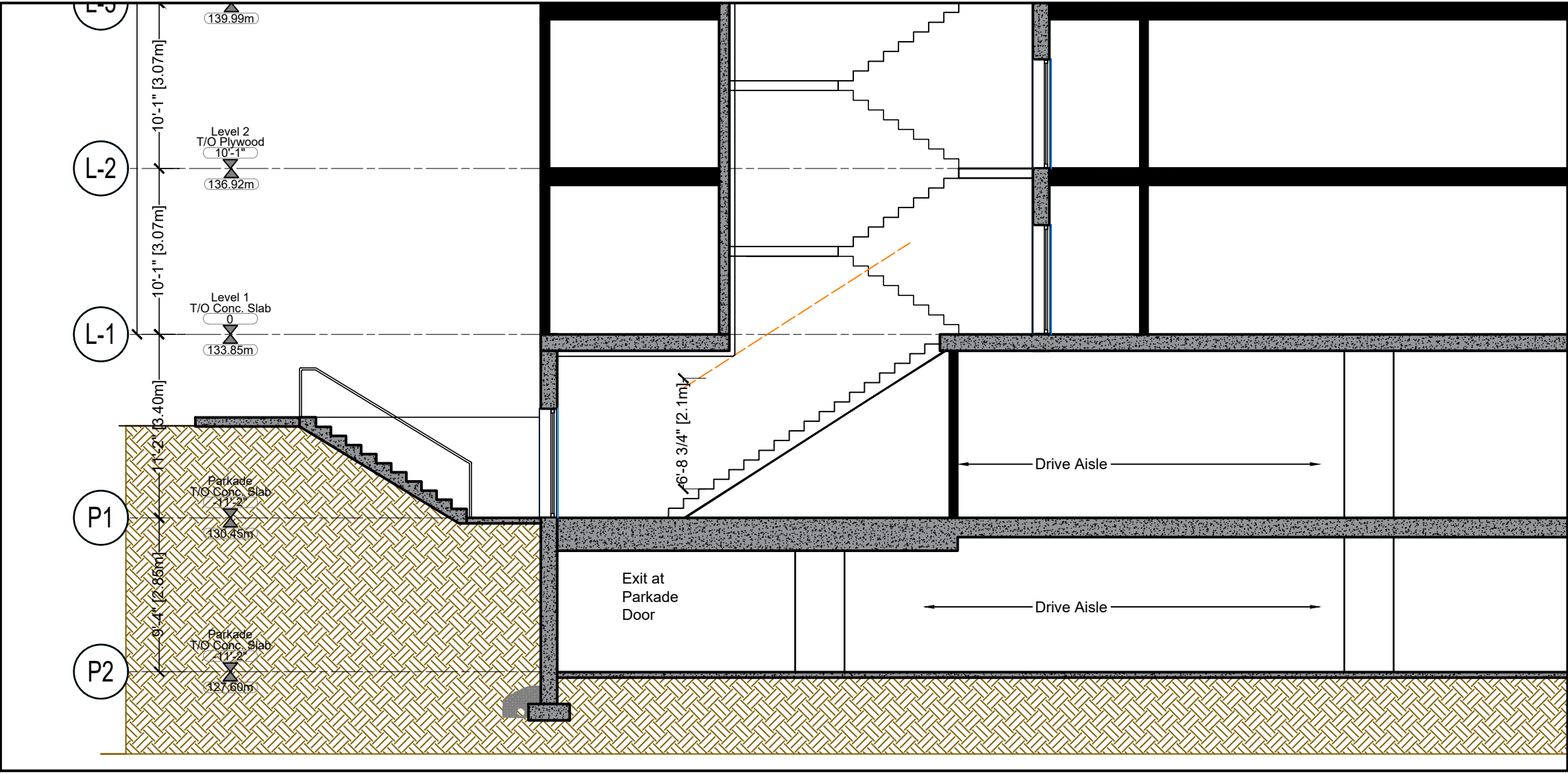
1 SECTION CC (partial)

Scale: 1/8" = 1' - 0"



2 SECTION CC (continued)

Scale: 1/8" = 1' - 0"



3 PARTIAL SECTION DD

Scale: 1/8" = 1' - 0"

1057
GIBSONS
WAY

Building B - Section C

Project No.: 2106

Date: September 1, 2023 Issued for: Development Permit

Drawn: CB

Sheet No.

A-41



1 Building A North Elevation
Scale: 1/8" = 1' - 0"



2 Building A East Elevation
Scale: 1/8" = 1' - 0"



3 Render of Entry
NTS

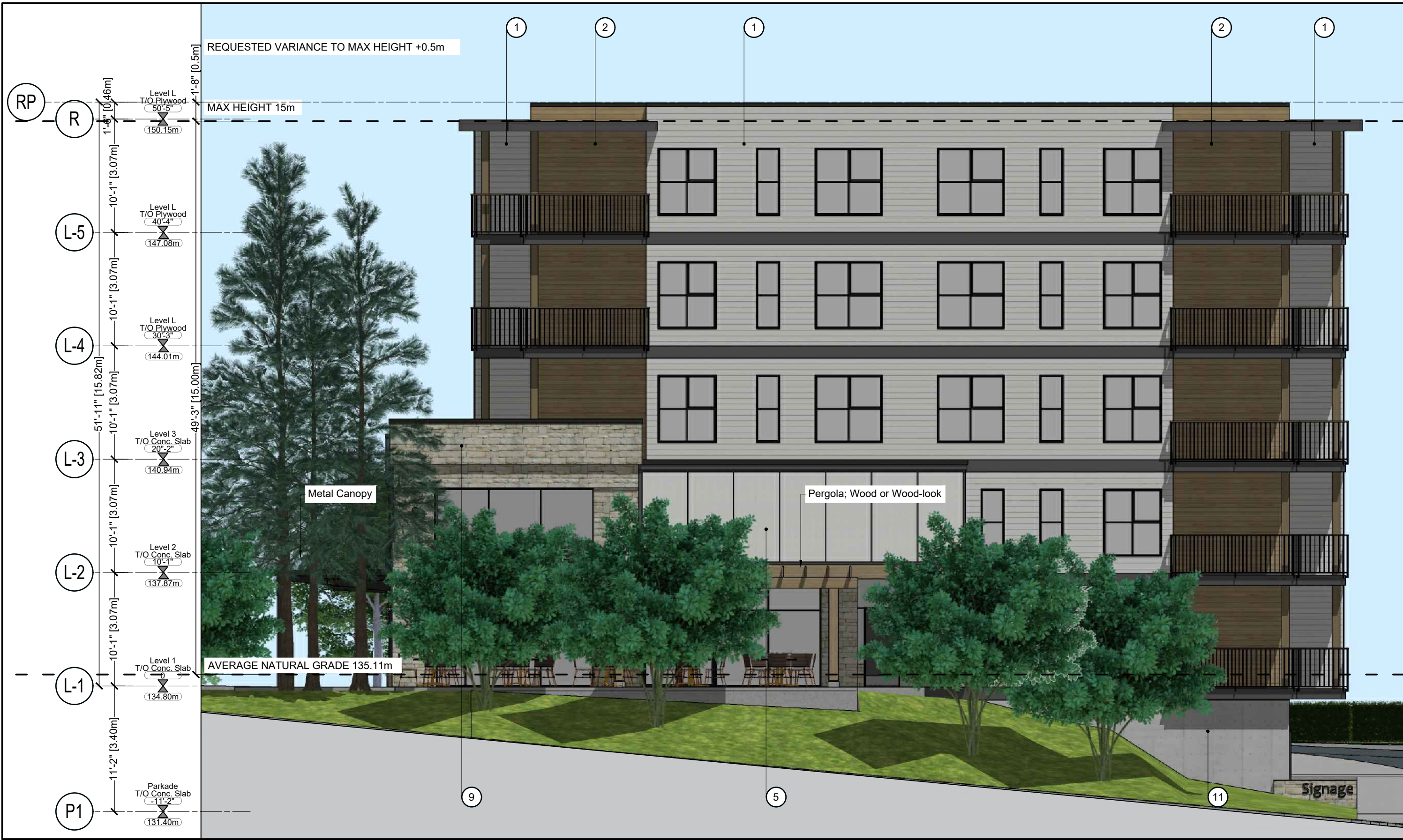
EXTERIOR FINISHING		
SYM	TYPE	COLOUR
1	Cement Board Lap Siding	White
2	Cement Board Lap Siding	Brown
3	Cement Board Lap Siding	Dark Blue
5	Cement Board Paneling	White
6	Cement Board Paneling	Beige
7	Cement Board Paneling	Dark Gray
8	Cement Board Paneling	Dark Blue
9	Stone Veneer	Beige
10	Aluminum Guard Rails	Black
11	Concrete	Grey
12	Storefront Windows	Black Frames
13	Vinyl Windows	Black Frames
14	Privacy Screen Wall	Cement Board Lap Siding
15	Trim	Gray

1057
GIBSONS
WAY

Building A - Elevations

Project No.:	2106
Date: September 1, 2023	Issued for: Development Permit
Drawn: CB	
Sheet No.	

A-50



EXTERIOR FINISHING		
SYM	TYPE	COLOUR
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3	Cement Board Lap Siding	Dark Blue
5	Cement Board Paneling	White
6	Cement Board Paneling	Beige
7	Cement Board Paneling	Dark Gray
8	Cement Board Paneling	Dark Blue
9	Stone Veneer	Beige
10	Aluminum Guard Rails	Black
11	Concrete	Grey
12	Storefront Windows	Black Frames
13	Vinyl Windows	Black Frames
14	Privacy Screen Wall	Cement Board Lap Siding
15	Trim	Gray

1057
GIBSONS
WAY

Building A - Elevations

Project No.: 2106

Date: September 1, 2023 Issued for: Development Permit

Drawn: CB

Sheet No.

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EXTERIOR FINISHING		
SYM	TYPE	COLOUR
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5	Cement Board Paneling	White
6	Cement Board Paneling	Beige
7	Cement Board Paneling	Dark Gray
8	Cement Board Paneling	Dark Blue
9	Stone Veneer	Beige
10	Aluminum Guard Rails	Black
11	Concrete	Grey
12	Storefront Windows	Black Frames
13	Vinyl Windows	Black Frames
14	Privacy Screen Wall	Cement Board Lap Siding
15	Trim	Gray

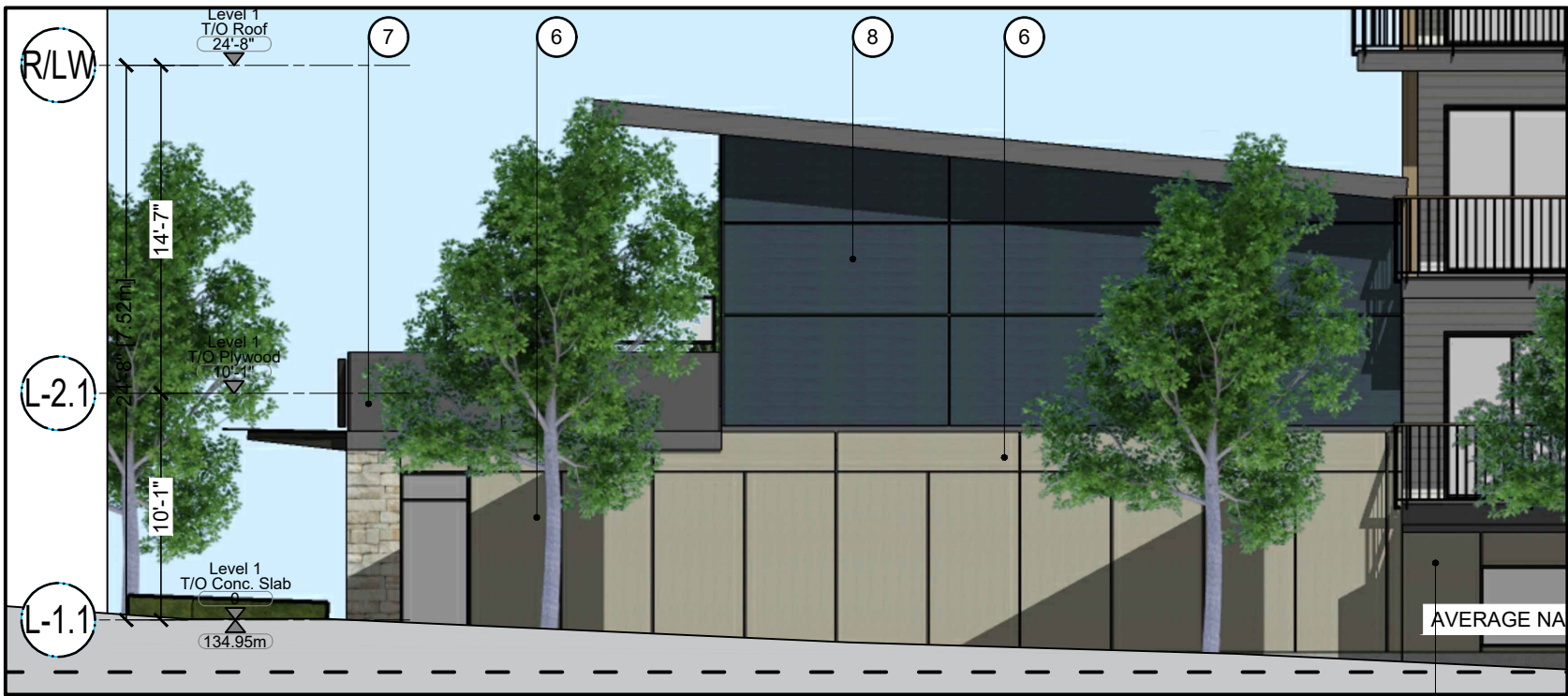
1057
GIBSONS
WAY



1 Building B West Elevation
Scale: 1/8" = 1' - 0"



2 Building B North Elevation
Scale: 1/8" = 1' - 0"



3 Building B Live/Work West Elevation
Scale: 1/8" = 1' - 0"



4 Render Residential Entry
Scale: 1/8" = 1' - 0"

Building B - Elevations

Project No.:	2106
Date: September 1, 2023	Issued for: Development Permit
Drawn: CB	
Sheet No.	

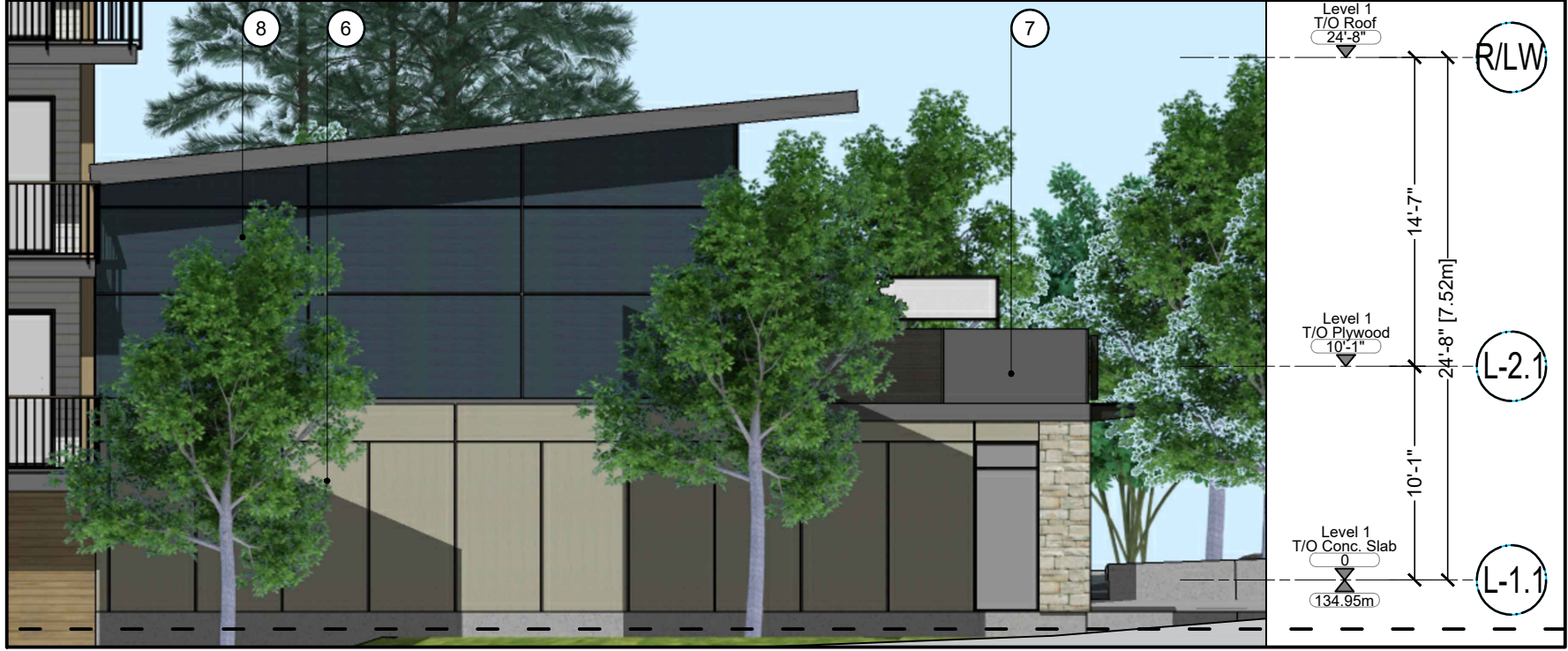
A-52



1 Building B East Elevation
Scale: 1/8" = 1' - 0"



2 Building B South Elevation
Scale: 1/8" = 1' - 0"



3 Building B Live/Work East Elevation
Scale: 1/8" = 1' - 0"



4 Render Live/WorkEntry
NTS

EXTERIOR FINISHING		
SYM	TYPE	COLOUR
1	Cement Board Lap Siding	White
2	Cement Board Lap Siding	Brown
3	Cement Board Lap Siding	Dark Blue
5	Cement Board Paneling	White
6	Cement Board Paneling	Beige
7	Cement Board Paneling	Dark Gray
8	Cement Board Paneling	Dark Blue
9	Stone Veneer	Beige
10	Aluminum Guard Rails	Black
11	Concrete	Grey
12	Storefront Windows	Black Frames
13	Vinyl Windows	Black Frames
14	Privacy Screen Wall	Cement Board Lap Siding
15	Trim	Gray

1057
GIBSONS
WAY

Building B - Elevations

Project No.:	2106
Date: September 1, 2023	Issued for: Development Permit
Drawn: CB	
Sheet No.	

A-53



CLIENT:

PROJECT:

DRAWING TITLE:

DATE: 23.FEB.10

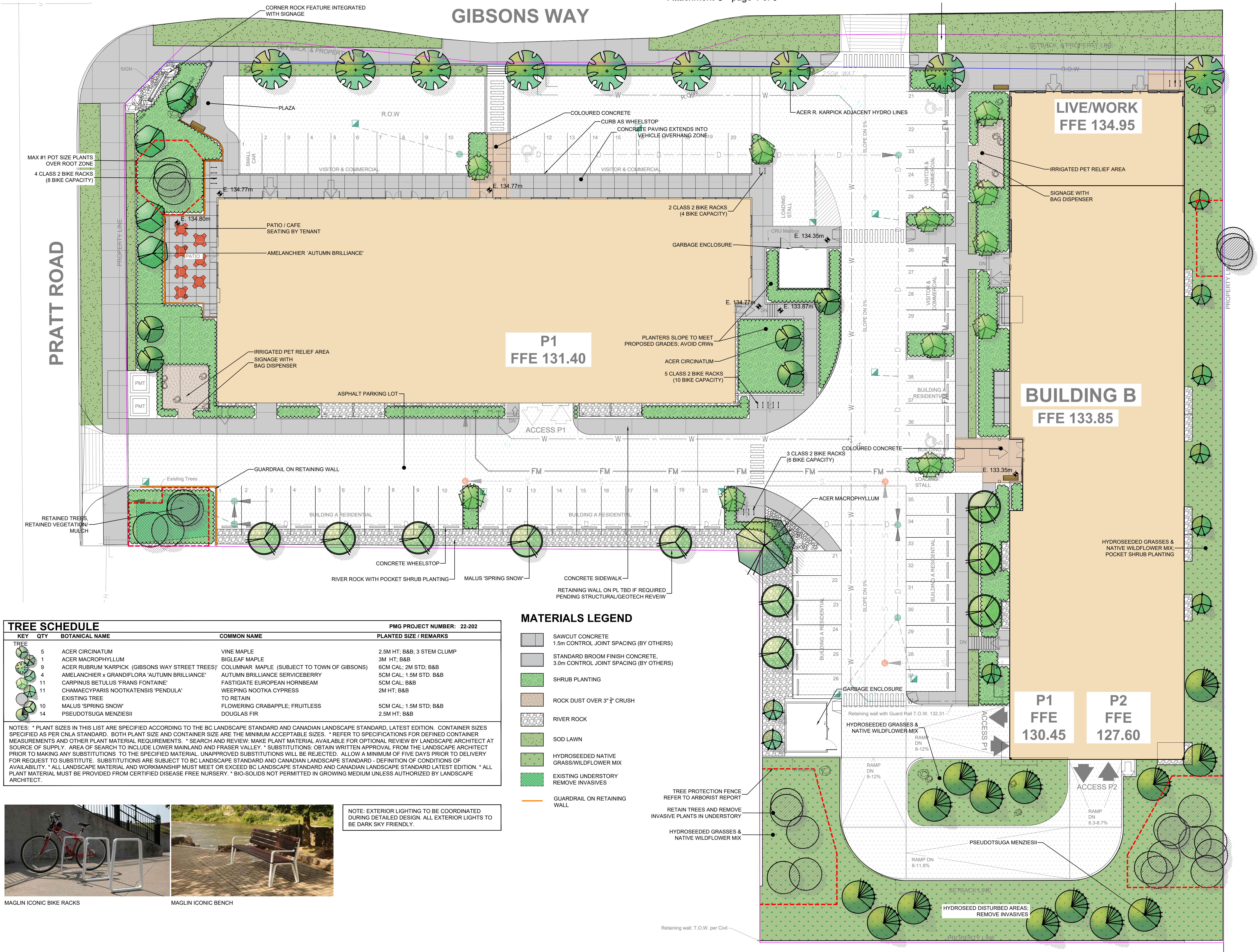
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DESIGN: SA/ PC

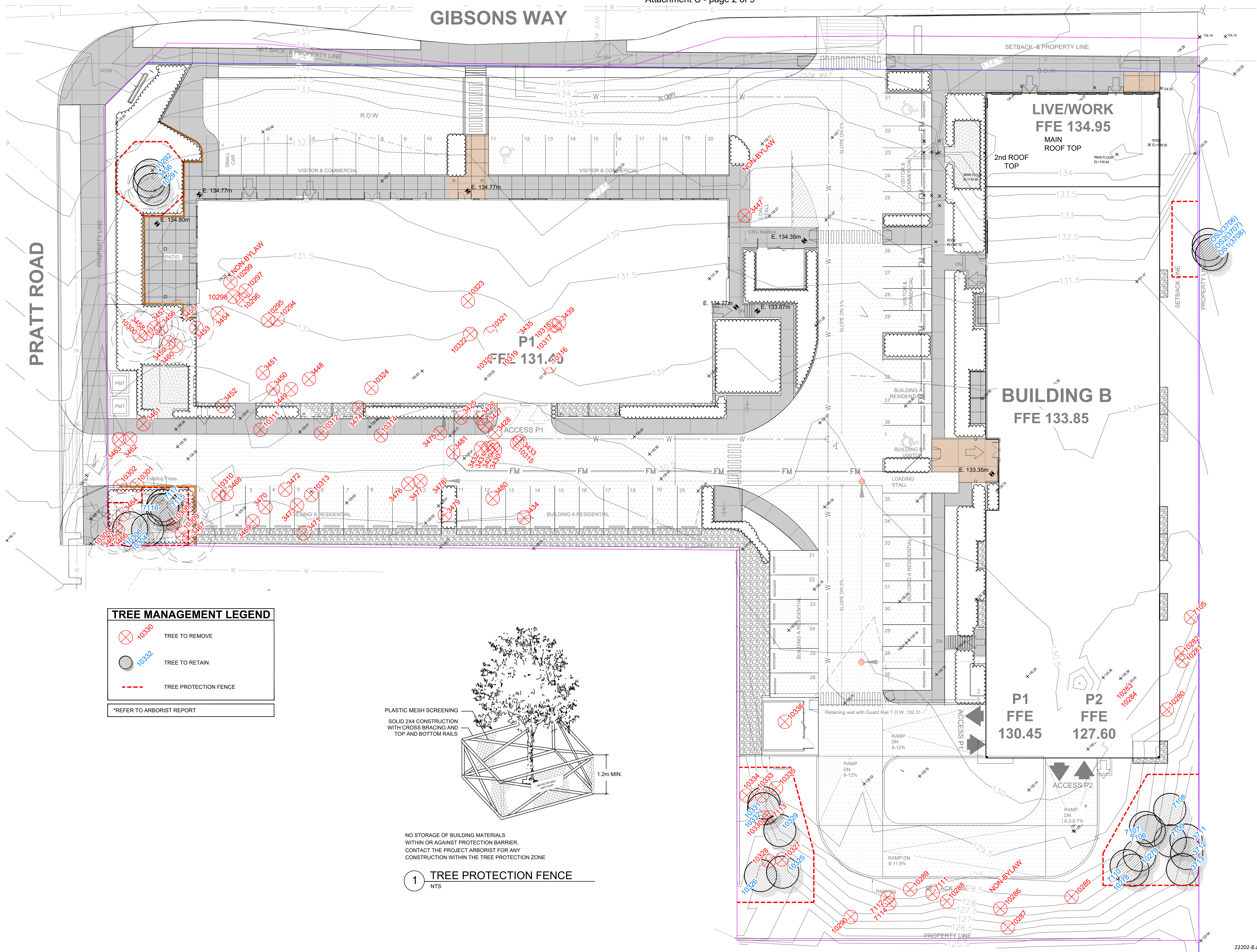
OF 9

22-202



GIBSONS WAY

PRATT ROAD



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p: 604 294-0011 ; f: 604 294-0022

SEAL:



7	23.AUG.16	DP SUBMISSION	SA
6	23.AUG.01	REVISE PER COMMENTS	SA
5	23.JUL.20	REVISE PER CLIENT COMMENTS	SA
4	23.JUN.26	TREE RETENTION REVISIONS	SA
3	23.JUN.02	DP SUBMISSION	SA
2	23.MAY.29	REVISE PER CLIENT COMMENTS	SA
1	23.MAY.02	DESIGN	PC

NO. DATE REVISION DESCRIPTION DR.

CLIENT:

PCREgroup

PROJECT:

MIXED USE DEVELOPMENT

**1057 GIBSONS WAY
GIBSONS, BC**

DRAWING TITLE:

**TREE MANAGEMENT
PLAN**

DATE: 23.FEB.10 DRAWING NUMBER:
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CHK'D: PC

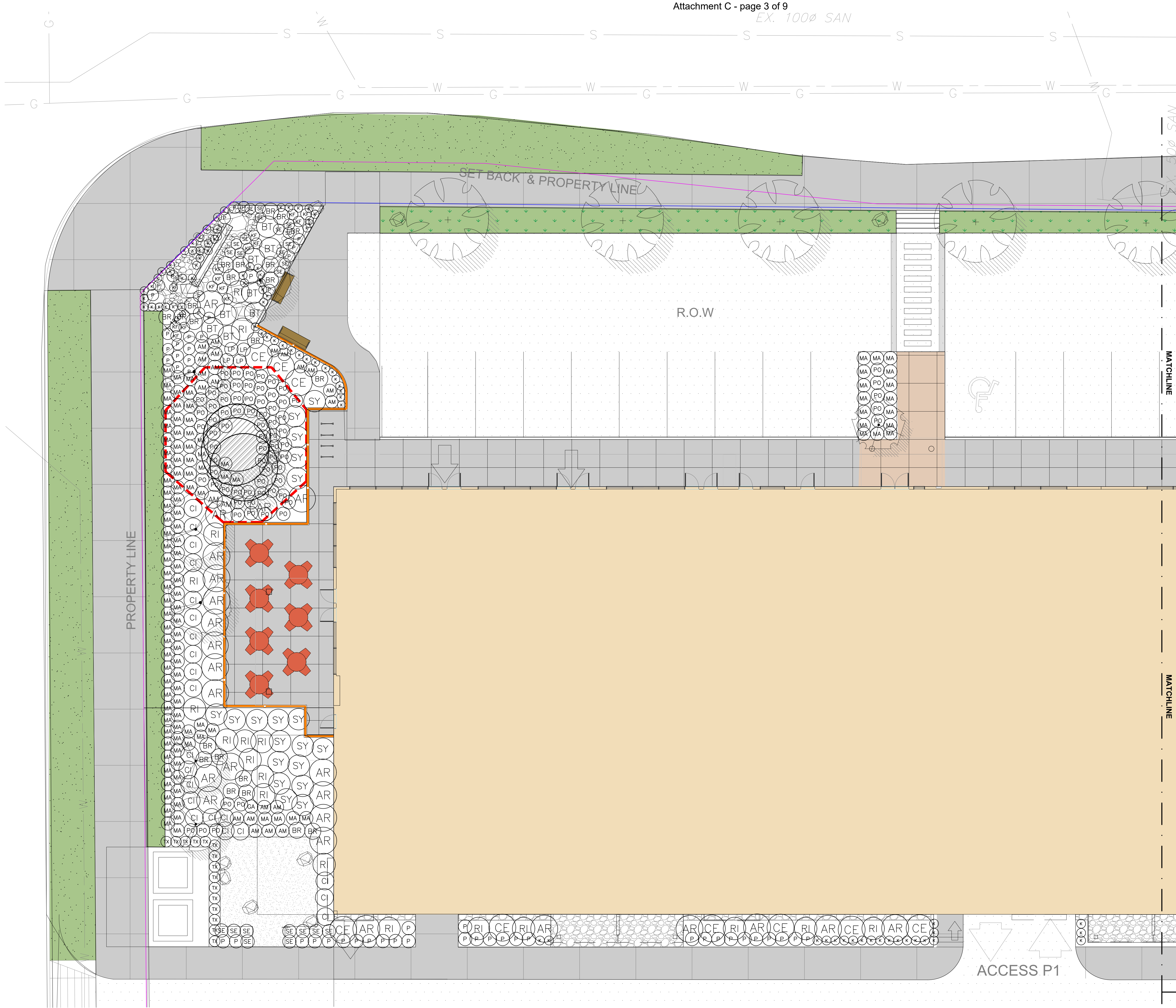
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OF 9

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22-202

22202-8.zip



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5	23.JUL.20	REVISE PER CLIENT COMMENTS	SA
4	23.JUN.26	TREE RETENTION REVISIONS	SA
3	23.JUN.02	DP SUBMISSION	SA
2	23.MAY.29	REVISE PER CLIENT COMMENTS	SA
1	23.MAY.02	DESIGN	PC
NO.	DATE	REVISION DESCRIPTION	DR.

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PCREgroup

PROJECT:

MIXED USE DEVELOPMENT

1057 GIBSONS WAY
GIBSONS, BC

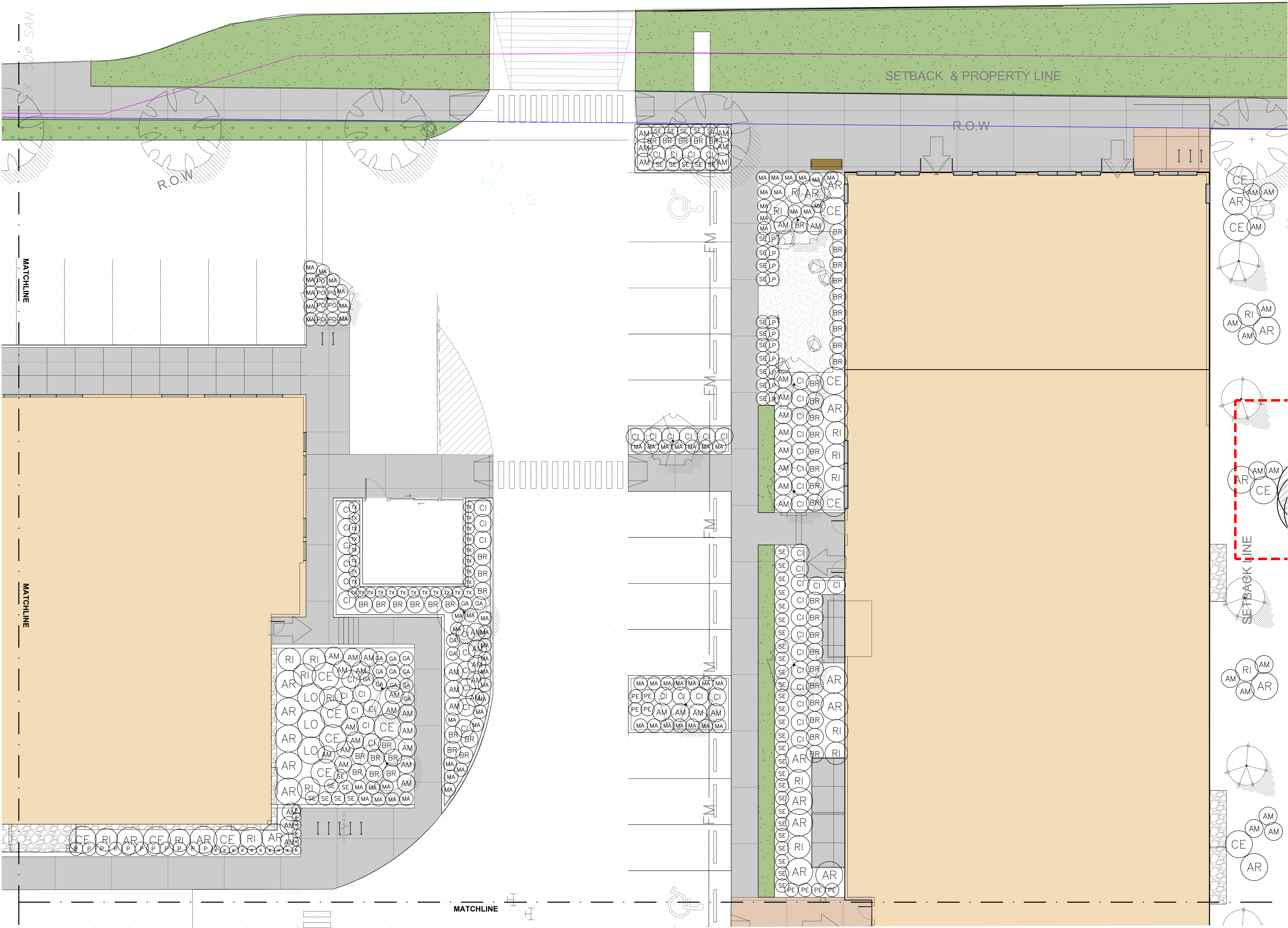
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SHRUB PLAN
WEST

DATE:	23.FEB.10	DRAWING NUMBER:
SCALE:	1/8" = 1'-0"	L3
DRAWN:	SA	OF 9
DESIGN:	SA/PC	
CHK'D:	PC	

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6	23.AUG.01	REVISE PER COMMENTS	SA
5	23.JUL.20	REVISE PER CLIENT COMMENTS	SA
4	23.JUN.26	TREE RETENTION REVISIONS	SA
3	23.JUN.02	DP SUBMISSION	SA
2	23.MAY.29	REVISE PER CLIENT COMMENTS	SA
1	23.MAY.02	DESIGN	PC
NO.	DATE	REVISION DESCRIPTION	DR.

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PROJECT:

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**1057 GIBSONS WAY
GIBSONS, BC**

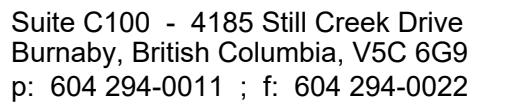
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NORTH-EAST**

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DRAWN:	SA	OF 9
DESIGN:	SA/PC	
CHK'D:	PC	

PMG PROJECT NUMBER:

22-202



SEAL:



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6	23.AUG.01	REVISE PER COMMENTS	SA
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4	23.JUN.26	TREE RETENTION REVISIONS	SA
3	23.JUN.02	DP SUBMISSION	SA
2	23.MAY.29	REVISE PER CLIENT COMMENTS	SA
1	23.MAY.02	DESIGN	PC
NO.	DATE	REVISION DESCRIPTION	DR.

CLIENT:

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1057 GIBSONS WAY
GIBSONS, BC

DRAWING TITLE:

SHRUB PLAN SOUTH-EAST

DATE: 23.FEB.10
SCALE: 1/8" = 1'-0"
DRAWN: SA
DESIGN: SA/PC
CHK'D: PC

L5

OF 9

PMG PROJECT NUMBER:

22-202

CLIENT:

PROJECT:

1057 GIBSONS WAY
GIBSONS, BC

SHRUB PLAN SOUTH-WEST

L6

OF 9

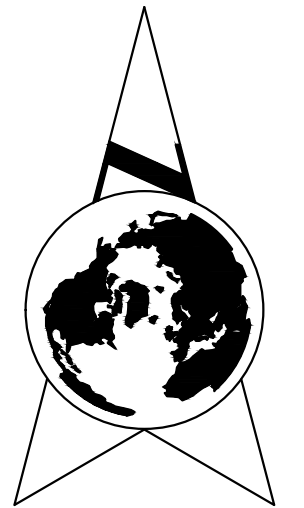
22-202



NOTES: * PLANT SIZES IN THIS LIST ARE SPECIFIED ACCORDING TO THE BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD, LATEST EDITION. CONTAINER SIZES SPECIFIED AS PER CNLA STANDARD. BOTH PLANT SIZE AND CONTAINER SIZE ARE THE MINIMUM ACCEPTABLE SIZES. * REFER TO SPECIFICATIONS FOR DEFINED CONTAINER MEASUREMENTS AND OTHER PLANT MATERIAL REQUIREMENTS. * SEARCH AND REVIEW: MAKE PLANT MATERIAL AVAILABLE FOR OPTIONAL REVIEW BY LANDSCAPE ARCHITECT AT SOURCE OF SUPPLY. AREA OF SEARCH TO INCLUDE LOWER MAINLAND AND FRASER VALLEY. * SUBSTITUTIONS: OBTAIN WRITTEN APPROVAL FROM THE LANDSCAPE ARCHITECT PRIOR TO MAKING ANY SUBSTITUTIONS TO THE SPECIFIED MATERIAL. UNAPPROVED SUBSTITUTIONS WILL BE REJECTED. ALLOW A MINIMUM OF FIVE DAYS PRIOR TO DELIVERY OF MATERIAL. SUBSTITUTIONS ARE SUBJECT TO FREE LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD. * DEFINITION OF CONDITIONS OF AVAILABILITY: * ALL LANDSCAPE MATERIAL AND WORKSMANSHIP MUST MEET OR EXCEED BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD LATEST EDITION. * ALL PLANT MATERIAL MUST BE PROVIDED FROM CERTIFIED DISEASE FREE NURSERY. * BIO-SOLIDS NOT PERMITTED IN GROWING MEDIUM UNLESS AUTHORIZED BY LANDSCAPE ARCHITECT.



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7	23.AUG.16	DP SUBMISSION	SA
6	23.AUG.01	REVISE PER COMMENTS	SA
5	23.JUL.20	REVISE PER CLIENT COMMENTS	SA
4	23.JUN.26	TREE RETENTION REVISIONS	SA
3	23.JUN.02	DP SUBMISSION	SA
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1	23.MAY.02	DESIGN	PC

CLIENT:

PCREgroup

PROJECT:

MIXED USE DEVELOPMENT

1057 GIBSONS WAY
GIBSONS, BC

DRAWING TITLE:

GRADING PLAN WEST

DATE: 23.FEB.10
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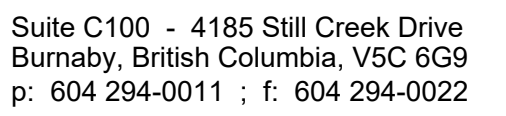
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OF 9

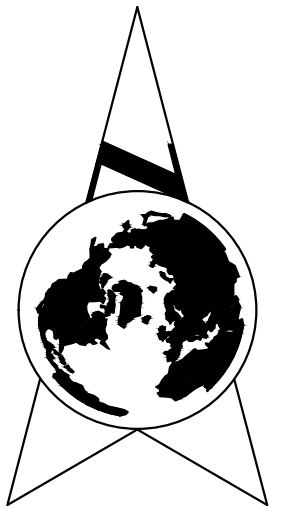
22202-8.zip

PMG PROJECT NUMBER:

22-202



SEAL:



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6	23.AUG.01	REVISE PER COMMENTS	SA
5	23.JUL.20	REVISE PER CLIENT COMMENTS	SA
4	23.JUN.26	TREE RETENTION REVISIONS	SA
3	23.JUN.02	DP SUBMISSION	SA
2	23.MAY.29	REVISE PER CLIENT COMMENTS	SA
1	23.MAY.02	DESIGN	PC

CLIENT:

• PCRE group

PROJECT:

MIXED USE DEVELOPMENT

1057 GIBSONS WAY
GIBSONS, BC

DRAWING TITLE:

GRADING PLAN EAST

DATE: 23.FEB.10

SCALE: $3/32" = 1'-0"$

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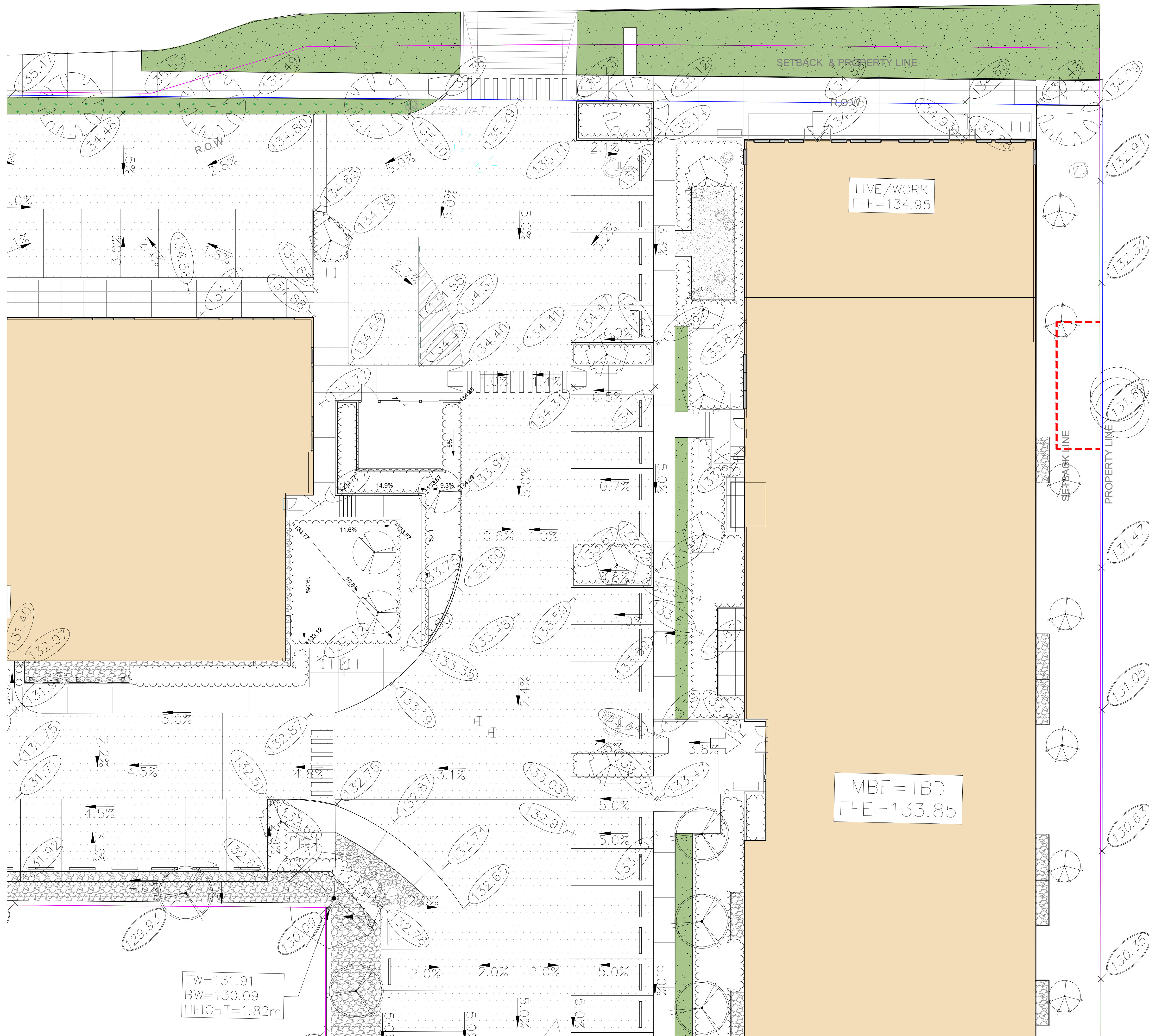
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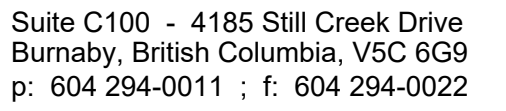
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OF 9

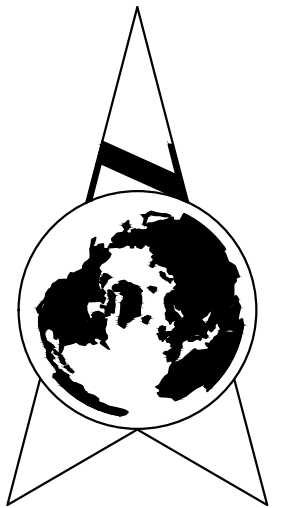
PMG PROJECT NUMBER:

22-202





SEAL:



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4	23.JUN.26	TREE RETENTION REVISIONS	SA
3	23.JUN.02	DP SUBMISSION	SA
2	23.MAY.29	REVISE PER CLIENT COMMENTS	SA
1	23.MAY.02	DESIGN	PC

CLIENT:

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PROJECT:

MIXED USE DEVELOPMENT

1057 GIBSONS WAY
GIBSONS, BC

DRAWING TITLE:

GRADING PLAN SOUTH-EAST

DATE: 23.FEB.10 DRAWING NUMBER:
SCALE: 1/16" = 1'-0"
DRAWN: SA
DESIGN:
CHK'D: PC

L9

OF 9

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22202-8.zip

PMG PROJECT NUMBER:

22-202

16.4 Upper Gibsons Commercial Development Permit Area No. 3

Purpose

The Upper Gibsons Development Permit is designated under Section 919.1 (1)(f) of the Local Government Act for the form and character of commercial development.

Area

The Upper Gibsons Development Permit Area is shown as Development Permit Area No. 3 on Schedule E: Form & Character DPAs. These guidelines shall also apply to any currently zoned commercial sites on Gibsons Way or North Road.

Justification

A significant amount of commercial development in Gibsons is located along the highway and is highly visible to motorists. The general appearance of this commercial development is important for the economic well-being of Gibsons. Council has designated this Development Permit Area in order to ensure a high quality of development along the highway.

The main objective of the Development Permit Area designation is to improve the commercial area and enhance the appearance of private developments for the benefit of visitors, residents and businesses.

Guidelines

Development permits issued in this area shall be in accordance with the following guidelines:

General Form and Character of Development

To create a high quality, visually appealing development with a safe, comfortable pedestrian environment, the following guidelines shall apply:

- *Innovative building design and configuration which introduces variety and detail to the buildings.*
- *Siting of buildings near the front of a parcel with the building's front face and main access facing the street.*
- *Parking at the rear or side of buildings rather than the front.*
- *Significant landscaping adjacent to public roadways and integrated within the site, use of both architectural and landscape features to provide a "gateway" or distinct entrance.*
- *The form and character should support and enhance the small town character.*
- *West Coast design features should be included in the design.*
- *The use of natural colours is encouraged, and the use of a variety of complementary colours as accents is also encouraged to promote visual interest.*
- *Design lighting to minimize light spill, glare and sky glow by using non-glare full cutoff fixtures.*

Building Form, Scale and Massing

To encourage varied building forms and to avoid creation of a commercial strip image, the following guidelines respecting massing and scale shall apply:

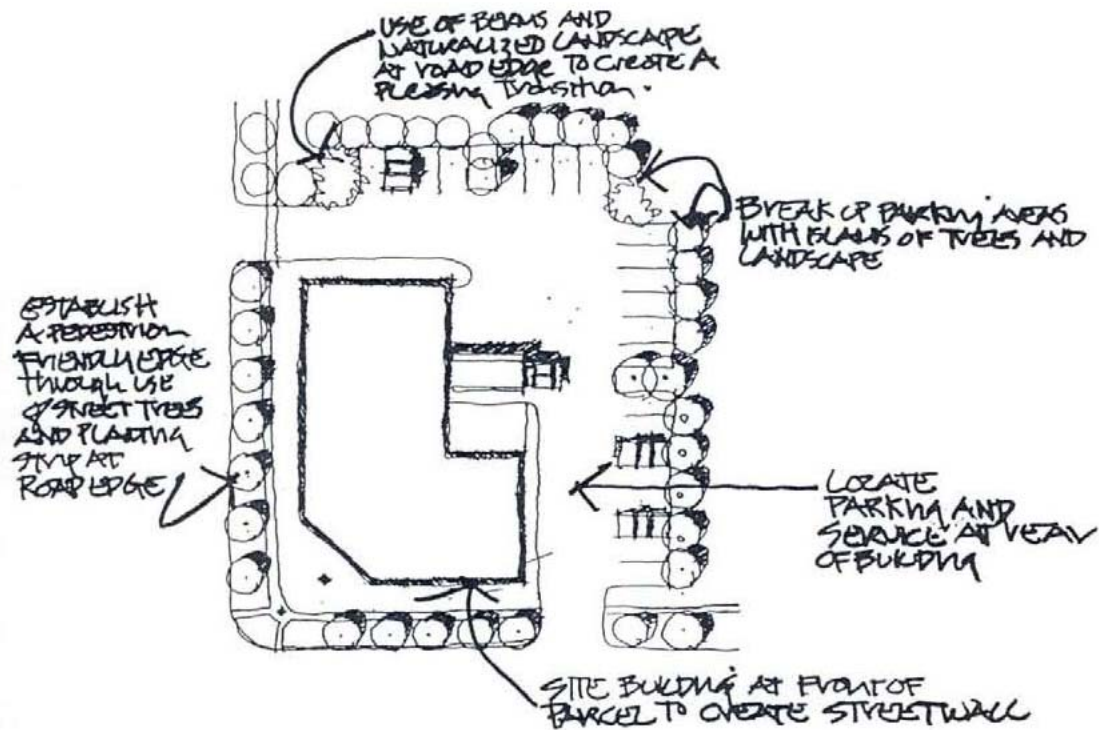
- *Varied building forms will be encouraged. Long, single story buildings should incorporate elements that add vertical definition such as sloped roofs or façade treatments such as fascia or awnings.*
- *Large areas of blank wall are not acceptable on a face with a pedestrian or residential area orientation.*

- Wall lines should be off-set and modulated along the building elevation to create visual interest along the building section.
- Pitching and stepping down of rooflines should be incorporated to vary height in the roofscapes of buildings.
- On sloped sites, building forms should step gradually step down to follow the slope of the site.
- Where commercial development incorporates a residential or multi-unit use on upper floors, a graded transition in the building height is desired to ensure adjacent properties are not faced with a massive wall.
- General modification of standardized corporate franchise building designs or features may be required in the event of conflict with these design guidelines.

Siting of Building and Structures

To reduce the impact of the large parking areas associated with commercial areas, and to reduce the apparent width of Gibsons Way, the following guidelines respecting siting of buildings shall apply:

- Buildings should be sited with the entrance to the buildings facing the street (Gibsons Way, North Road or the access road) to encourage creation of an interest and access for pedestrians. Buildings on corner sites should have façade interest facing both streets.
- Parking should occur at the rear or side of buildings rather than the front and shall be buffered from view by significant landscape islands.
- The buildings or structures should be used to reinforce the definition of street corners.
- Natural landscape which includes significant tree stands should be retained and incorporated into site development plans when feasible.



Siting of building/structures and parking and use of buffering space and landscaping

Residential/Commercial Buffers

Effective transitions between commercial areas and adjacent residential properties can be achieved by:

- *Fencing, combined with a broad area of landscape plantings (tree, shrubs).*
- *Dense shrubbery or hedges capable of impeding travel through to adjacent properties, and which is not visually transparent to adjacent properties.*
- *Creating a transition zone to complement the scale of the development, but not be less than 2.0 metres in width.*
- *Careful positioning of lighting, parking and access points to minimize impacts on adjacent properties.*

Pedestrian Environment

The transition from Gibsons Way edge to the building or parking edge should attempt to reduce the apparent width of Gibsons Way, encourage a “pedestrian friendly” environment and create a “green area” to reduce impacts of traffic. This can be achieved by application of the following guidelines:

- *Buildings and structures should be pedestrian oriented at the ground level. This can be achieved by:*
 - *an emphasis on the fenestration (the arrangement and positioning of windows)*
 - *inclusion of weather protection along outside pedestrian routes through the use of awnings, arcades and canopies*
 - *weather protection must be integral with the building form, and the materials and colors used should be consistent with the façade*
- *Developing pedestrian routes, including sidewalks on each side of Gibsons Way. The pedestrian routes should be well separated from the highway edge by a planting strip of not less than 1.5 metres, wherever possible.*
- *A minimum 3.0 metre planted edge between the sidewalk and the building or parking edge should be established (see illustration below).*
- *Providing pedestrian amenities within the 3.0 metre transition zone between the sidewalk and the building or parking edge such as benches, shelters, alcoves, seating, walls, trellises and arbors and pathways to the adjacent businesses.*

Landscaping

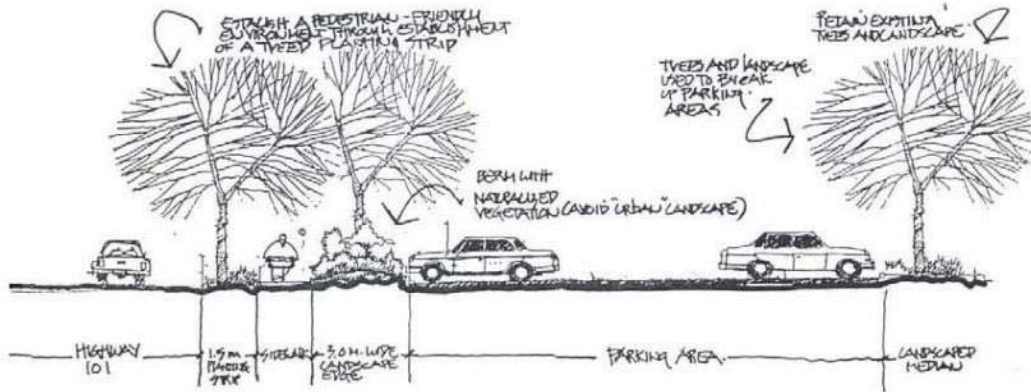
Landscaping of commercial and multifamily areas in Upper Gibsons is a key to improving the image and character of this area. For all developments:

- *A detailed professional landscaping plan shall be provided.*
- *Emphasis on major road edges is required through a combination of “street trees” and lower plantings.*
- *Native plant materials are preferred, and shall include a mix of coniferous and deciduous species.*
- *The retention of natural tree stands is encouraged.*

Parking

- *Parking should not visually dominate a development. Parking areas should be integrated into developments by such means as incorporating significant landscaping, coordination of outdoor elements and linking of buildings with parking by distinctively paved walkways.*
- *Parking lots should be paved and shall include landscaped areas within the lot. No more than 10 parking stalls in a row should be allowed without a landscaping break.*
- *Low, dense screening of street fronting stalls is required. This can be achieved through the use of landscape materials, or, a combination of landscape features such as arbors or trellises which run the length of the parking area.*
- *Support service facilities and structures such as loading bays, refuse containers, storage areas, and utility services should be located and screened with walls, fencing, hedging, planting, other screening materials or a combination of these materials to minimize visibility from public areas.*

Use of landscaping around parking area



Signage

Signage in the Upper Gibsons Development Permit Area shall comply with the following guidelines:

- All signs should be architecturally coordinated with the overall design of buildings and landscaping and may require modification of corporate or franchise design elements.
- Multi-unit buildings are encouraged to have an attractive, simple, single entry sign rather than multi-tenant signs which create a cluttered appearance.
- Freestanding signs should be restricted to a maximum height of 4.5 metres above grade.
- Freestanding signs should reflect a West Coast character by using elements of wood and / or stone.
- Changeable illuminated copy signs shall not be permitted on properties in Development Permit Area No. 3, except where such signage is clearly a requirement of the business activity (i.e. movie theatres, gas stations).
- Internally illuminated (backlit box) signs shall not be permitted, with the exception of internally illuminated channel lettering.
- External lighting for fascia and wall signs should be directed downward and use goose neck style lighting fixtures. An illumination design needs to ensure all wiring and conduits are concealed.

DPA3 Guidelines	ADP Evaluation Notes
<i>General Building Form and Character</i>	
Innovative building design and configuration which introduces variety and detail to the buildings.	
Siting of buildings near the front of a parcel with the building's front face and main access facing the street.	
Parking at the rear or side of buildings rather than the front.	
Significant landscaping adjacent to public roadways and integrated within the site, use of both architectural and landscape features to provide a "gateway" or distinct entrance.	
The form and character should support and enhance the small town character.	
West Coast design features should be included in the design.	
The use of natural colours is encouraged, and the use of a variety of complementary colours as accents is also encouraged to promote visual interest.	
Design lighting to minimize light spill, glare and sky glow by using non-glare full cutoff fixtures	
Building Form and Character	
Varied building forms will be encouraged. Long, single story buildings should incorporate elements that add vertical definition such as sloped roofs or façade treatments such as fascia or awnings.	
Large areas of blank wall are not acceptable on a face with a pedestrian or residential area orientation.	
Wall lines should be off-set and modulated along the building elevation to create visual interest along the building section.	
Pitching and stepping down of rooflines should be incorporated to vary height in the roofscapes of buildings.	
Siting of Building and Structures	
Buildings should be sited with the entrance to the buildings facing the street (Gibsons Way, or the access road) to encourage creation of an interest and access for pedestrians.	

Parking should occur at the rear or side of buildings rather than the front and shall be buffered from view by significant landscape islands.	
Pedestrian Environment	
<p>Buildings and structures should be pedestrian oriented at the ground level. This can be achieved by:</p> <ul style="list-style-type: none"> • an emphasis on the fenestration (the arrangement and positioning of windows) • inclusion of weather protection along outside pedestrian routes through the use of awnings, arcades and canopies <p>weather protection must be integral with the building form, and the materials and colors used should be consistent with the façade</p>	
Developing pedestrian routes, including sidewalks on each side of Gibsons Way. The pedestrian routes should be well separated from the highway edge by a planting strip of not less than 1.5 metres, wherever possible.	
A minimum 3.0 metre planted edge between the sidewalk and the building or parking edge should be established.	
Providing pedestrian amenities within the 3.0 metre transition zone between the sidewalk and the building or parking edge such as benches, shelters, alcoves, seating, walls, trellises and arbors and pathways to the adjacent businesses.	
Signage	
All signs should be architecturally coordinated with the overall design of buildings and landscaping and may require modification of corporate or franchise design elements.	
Freestanding signs should reflect a West Coast character by using elements of wood and / or stone.	
External lighting for fascia and wall signs should be directed downward and use goose neck style lighting fixtures. An illumination design needs to ensure all wiring and conduits are concealed.	